

CITY OF TECUMSEH COMPREHENSIVE PLAN



ADOPTED BY PLANNING COMMISSION ON DECEMBER 7, 2004
ADOPTED BY CITY COUNCIL ON JANUARY 4, 2005

PREPARED BY CITY OF TECUMSEH PLANNING COMMISSION
ASSISTED BY BIRCHLER ARROYO ASSOCIATES, INC.

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INTRODUCTION

The City's Comprehensive Plan is the community's "vision" for the future. The City's Zoning Ordinance contains the rules that govern the path to that vision.



Comprehensive Plan is a public policy document that establishes goals, objectives, strategies, and plans for future land use, housing, transportation, and community development. It portrays a clear statement of community goals and objectives. It establishes a vision of the future, and it includes plans to achieve the vision.

If followed carefully, a Comprehensive Plan will have a lasting impact on the built and natural environment. Decisions made when the Plan is developed will likely be implemented over many years.

The Comprehensive Plan is long-range in its view and intended to guide development in the City over a period of 10 to 20 years. The information and concepts presented in the Comprehensive Plan are used to guide local decisions on public and private uses of land and the provision of public facilities and services. A sound Comprehensive Plan promotes a land use pattern that is consistent with a community's goals.

WHY DO WE PREPARE A COMPREHENSIVE PLAN?

The Municipal Planning Act of 1931 states that "the planning commission shall make and approve a Comprehensive Plan for the physical development of the municipality." In addition, the City and Village Zoning Act of 1921 requires that the zoning ordinance be in accordance with an adopted plan designed to promote and accomplish the following objectives:

- ❖ Meet the needs of residents for food, fiber, energy and other natural resources, places of residence, recreation, industry, trade, service, and other uses of land.
- ❖ Insure that uses of land be situated in appropriate locations and relationships.
- ❖ Limit the overcrowding of land and congestion of population, transportation systems, and other public facilities.
- ❖ Facilitate provision for a system of transportation, sewage disposal, water supply, recreation, and other public improvements.
- ❖ Promote public health, safety and general welfare.

RELATIONSHIP BETWEEN THE COMPREHENSIVE PLAN AND ZONING ORDINANCE

Zoning is a regulatory mechanism for controlling the classification and regulation of land use. It is law. The Zoning Ordinance controls land uses based on today's conditions.

Unlike the Zoning Ordinance, the Comprehensive Plan is a set of policies, not laws. While the Zoning Ordinance and Zoning Map regulate current and proposed land use, the Comprehensive Plan and its maps and policy statements are intended to guide land use decision-making over the long term. The Comprehensive Plan is a community's "vision", while the zoning ordinance governs the path to that vision. **State law requires that the zoning ordinance be based on a plan.** Therefore, the Comprehensive Plan forms the basis upon which zoning decisions are made. With a Comprehensive Plan in place, zoning decisions consistent with the plan and ordinance are presumed by the courts to be valid.



Comprehensive Plan
= Guide

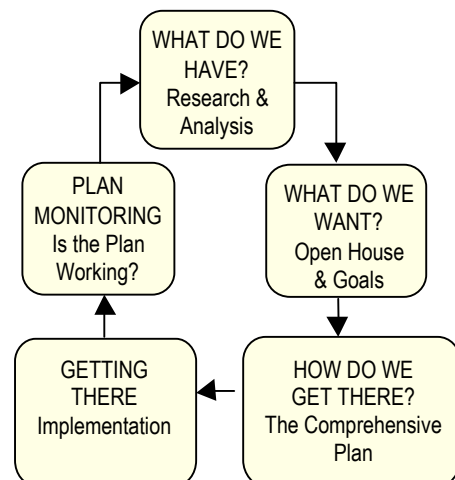


Zoning Ordinance
= Law

THE PLANNING PROCESS

The Comprehensive Plan process began with an inventory and analysis of existing conditions. The Planning Commission reviewed the City's regional setting, development history, existing land use, natural resources and features, and population characteristics. Problems, opportunities, and community assets were identified. The complete findings of the existing conditions analysis are provided in the Appendix of this document.

Upon completion of the existing conditions analysis, the Planning Commission reached out to the public for input through a *Community Open House* and *Visioning Session*.



Introduction

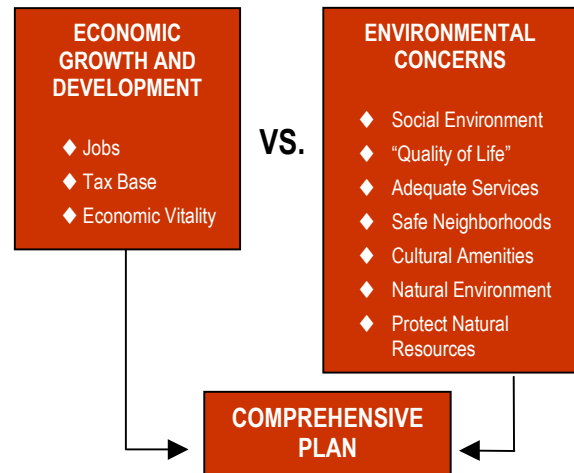
Representatives from a variety of interests within the community came together on November 11, 2003 to discuss planning issues in the City.

Using public opinion from the Open House and Visioning and the experience of City officials, goals and objectives were formulated to guide the “Plan” elements of the Comprehensive Plan.

Finally, the Planning Commission developed a plan for Land Use, Thoroughfares, Housing, Economic Development, Downtown Tecumseh, Capital Improvements & Infrastructure, and Natural Features & Environmental Protection. Recommendations for plan implementation are included in each of these final chapters. The Future Land Use Plan Map shows generalized land use and does not indicate precise size, shape or dimension of parcels of land. In addition, the recommendations of the Land Use Plan have a long-range planning horizon and do not necessarily imply that short-range rezoning is appropriate.

The final task in the planning process was to obtain additional public opinion through a public hearing, which is required by the Municipal Planning Act, prior to the adoption of the plan.

By working closely with the residents, business owners, planning experts, and surrounding communities, the City has developed a plan that attempts to balance the competing interests that affect land use decisions. These include jobs and tax base on one side and protection of quality of life and natural resources on the other. Through careful implementation of the plan, the City can build on its tax base and provide for high-quality new growth, while preserving important environmental assets and protecting the overall health, safety and welfare of its citizens.

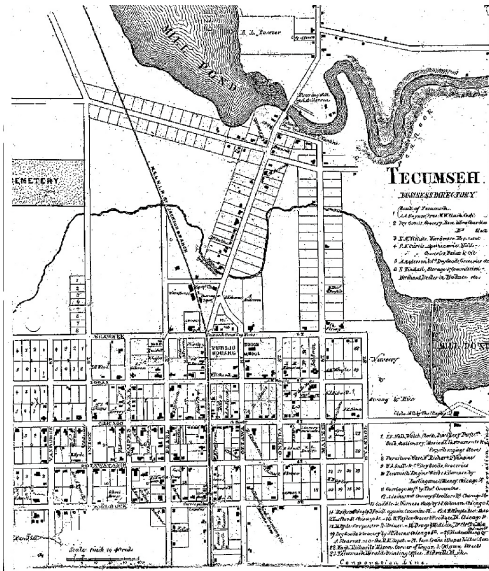


A Comprehensive Plan can help a community strike a balance between the often-competing interests of the public, individual property owners, and the natural environment.

LOCAL HISTORY¹

The City of Tecumseh has a proud history as Lenawee County’s first settlement. It was founded in 1824 by Musgrove Evans, a Quaker from New York and a civil engineer and surveyor. Evans laid out the arrangement of streets and became one of Tecumseh’s first citizens. The settlement was sited to take advantage of the existing Native American trails that passed through the area, the water power of the Raisin River, and the rich soils and extensive forests of the surrounding areas. The town was named for Tecumseh, the Shawnee chief, and began as the Lenawee County seat.

¹ Sources: Proctor, Diane E. *Beyond the Boulevard*. Tecumseh; Tecumseh Area Historical Society, Inc. 1996. State of Michigan Historic Preservation Office website. August 22, 2003. <http://michsite.state.mi.us/detlsite.cfm?ObjectID=11605>.



Tecumseh 1857

Photo: Tecumseh the CD Vol II "2003"

Tecumseh grew steadily throughout the next decade, its economy based on the surrounding farms and products and services required by those farms. In 1838 the County seat was moved to Adrian, an event that had a profound effect on Tecumseh's development from that point on. While Adrian grew into a busy city of over 20,000 people, Tecumseh remained a prosperous but quiet farming community with little development pressure until well into the Twentieth Century. Grain mills and a foundry supplemented the agricultural economy.

While the original plat of Tecumseh only encompassed the sixteen blocks bordered by Shawnee, Wyandotte, Kilbuck, and Evans Streets, additional plats in the 1830s, 1860s, and later years extended the town in all directions.

In 1934 Ray W. Herrick founded the Tecumseh Products Company, maker of compressors for refrigeration units. The company created a large number of jobs and with them a need for housing. Other manufacturing businesses further expanded Tecumseh's industrial and employment base. Several new subdivisions were built in the 1950s and 1960s, and City and school facilities were expanded to keep up with the growing population. The City of Tecumseh was incorporated in 1954, 130 years after its inception as a settlement.



Tecumseh 1869

Photo: Tecumseh the CD Vol II "2003"

RECENT DEVELOPMENT TRENDS

Since 1990, the City of Tecumseh has experienced fairly significant population growth. Its population increased by nearly fifteen percent from 7,462 persons in 1990 to 8,574 persons in 2000. The City's growth can also be seen in the 21.7 percent increase in the number of housing units during that same time period. According to the 2002 Lenawee County Comprehensive Land Use Plan, 691 residential building permits were issued in the City of Tecumseh between 1990 and 2000. Much of this residential development has taken the shape of typical single family subdivisions and site condominium projects that can be found on the north and west edges of the City, such as the Red Maple and Wind Dancer developments. Tecumseh has also seen the addition of several attached condominium and duplex projects in recent years, including Courtland Place, Carriage Homes, and Meadowlands of Evans Creek.

Introduction

In addition, there is a 96-unit duplex cluster development project proposed at the southeast corner of Occidental and Brown Streets that is currently going through the City's development review process.

In recent years, the City has witnessed an emerging trend toward the restoration and conversion of some of its historical homes back to single family residences, as many larger historic homes had been converted to multiple units in earlier years. This reversal in trends may be due in part by a renewed interest in historic preservation and traditional architecture and housing styles.

While most of the City's recent growth and development has been residential, there has been a limited amount of commercial development since 1990. New commercial has primarily occurred on the City's west end and has taken the form of more suburban-style strip developments. East of downtown, the City has seen the addition of a Taco Bell and bank in more recent years.

There has been very little industrial development in the last several years. This is likely due to the lack of available industrial land. While there are small sites (one to three acres) available in the industrial park, the City has seen a recent demand for five to twenty acre sites that can accommodate larger industrial users.

Little has occurred in terms of office development in the last several years; however, the City may witness a change in this trend in the near future. Currently, there is very little professional and medical office space available, especially near Herrick Memorial Hospital. Aware of the recent interior renovations and helipad construction at the hospital, the City is anticipating an increased need for additional office space as a result of the hospital's continued growth. To accommodate such a need, the area surrounding the hospital has been zoned for office and service uses. So far, there have been only a few conversions of existing homes to office uses in this area. The City has not yet seen any large-scale office redevelopment of this area.

HISTORIC PRESERVATION

HISTORIC PRESERVATION COMMISSION

Chapter 42 of the City of Tecumseh Code of Ordinances establishes the Tecumseh Historic Preservation Commission (HPC) and sets forth as a matter of public policy that "the protection, enhancement, perpetuation, and use of improvements of special character or special historical or aesthetic interest or value is public necessity and is required in the interest of the health, prosperity, safety and welfare of the people." The Code further states the purpose of Chapter 42 is to:

- ❖ Safeguard the heritage of the City by preserving districts and historic landmarks which reflect elements of its cultural, social, economic, political, and architectural history.
- ❖ Safeguard the City's historic, aesthetic and cultural heritage as embodied and reflected in such improvements and districts.

Introduction

- ❖ Stabilize and improve property values.
- ❖ Foster civic pride in the beauty and noble accomplishments of the past.
- ❖ Promote the use of historic landmarks for the education, pleasure and welfare of the people of the City.

In keeping with the above purpose, the City Council, after consideration and recommendation by the Historic Preservation Commission, may designate local historic landmarks and establish historic districts within the City. Currently, the City has designated twenty-seven landmarks and established three historic districts. Prior to the issuance of a building permit, the HPC reviews all plans for the exterior construction, alteration, repair, moving, or demolition of any landmark or landmark site. In addition, the Historic Preservation Commission, upon the request of any property owner, shall render advice and guidance with respect to any proposed work on a designated landmark site or within any designated historic district.

TECUMSEH AREA HISTORICAL SOCIETY

The Tecumseh Area Historical Society was founded in 1974 and maintains the Tecumseh Area Historical Museum which is housed in the historic St. Elizabeth Catholic Church building. Since its inception, the Society has played an active role in the establishment of the City's historic districts and seeks to increase the public's awareness of the area's rich history. The Society has also purchased and restored several historic structures in the City and hosts the annual "Promenade the Past" historic home tour.

2

REGIONAL SETTING

Few communities are self-sufficient. The interaction between communities within a region gives each one the opportunity to grow and develop.



Very few communities are self-sufficient. Rather, the interaction between communities within a region gives each one the opportunity to grow and develop. While much of a Comprehensive Plan focuses on local influences, factors outside a community's borders must be considered in the planning process. Two key regional factors impact the growth and development of a community: regional location and regional influences.

REGIONAL LOCATION

The City of Tecumseh is in the northeast corner of Lenawee County near the southern border of Washtenaw County and the western border of Monroe County. State Highway 50 (M 50) runs east/west through Tecumseh, connecting the City to US 23, Britton, Dundee, and Monroe to the east and Brooklyn, Napoleon, and Jackson to the west. State Highway 52 (M 52) runs north/south just west of the City, providing access Adrian to the south and Manchester and Chelsea to the north. The City also has convenient access to US 12 via Tecumseh-Clinton Highway (Evans Street), which connects Tecumseh to Saline, Ann Arbor, Ypsilanti, US 23, and Interstate 94 to the northeast. The City of Tecumseh is located approximately sixty miles from Detroit, forty miles from Toledo, thirty-five miles from Jackson, thirty miles from Ann Arbor and Monroe, and twelve miles from Adrian.

Map 2-2 identifies the regional transportation network, as well as some of the major employers found in nearby communities. Tecumseh's proximity to these regional employment centers make it an attractive location for residents willing to commute.

The City is also conveniently located near several airports, including Meyers Airport in Tecumseh Township, the Lenawee County Airport near Adrian, as well as international airports in Detroit and Toledo.

Map 2-1
Regional Location



LOCAL INFLUENCES

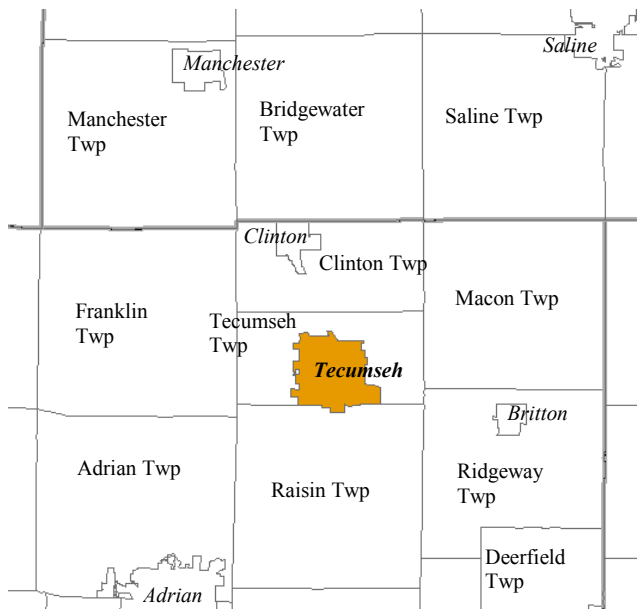
The City of Tecumseh is approximately 5 square miles. The City is bordered by Tecumseh Township to the north, west, and east, and Raisin Charter Township to the south. As depicted on Map 2-3, the City is also in close proximity to the Villages of Clinton and Britton, as well as Clinton, Macon, Franklin, Adrian, and Ridgeway Townships.

Planning for a municipality cannot take place in a vacuum. Rather, it is critical for a community to be aware of its neighbors' long-range goals and objectives. As mentioned above, the City has common borders with Tecumseh Township and Raisin Charter Township. It is important to know what plans these adjacent communities have adopted, and if they will have any impact on the City. Map 2-4 illustrates the future land use designations for the communities that share a common border with the City. This information will be useful as Tecumseh determines its future land use arrangement.

RAISIN TOWNSHIP

The Raisin Township Land Use Plan was adopted in December of 2002. As depicted on Map 2-4, Tecumseh's shared boundary with Raisin Township along Russell Road is primarily planned for Low Density Residential uses. The Township's draft Master Plan describes Low Density Residential areas as suitable for two to three dwelling units per acre, depending on soil conditions.

Map 2-3
Surrounding Communities



Regional Setting

The Plan designates the area east and west of Rogers Highway for Rural Residential uses. According to the Plan, Rural Residential areas are those with soils suitable for septic systems but not as favorable for cultivation. These areas are intended to accommodate residential development at a maximum density of one unit per acre.

The existing Lenawee County Road Commission facility on the west side of Evans Street is designated as Public / Quasi-Public. The Plan also includes a Parks and Recreation category that encompasses all of the Township's recreation facilities, as well as a proposed greenway or nature corridor along the Rasin River.

TECUMSEH TOWNSHIP

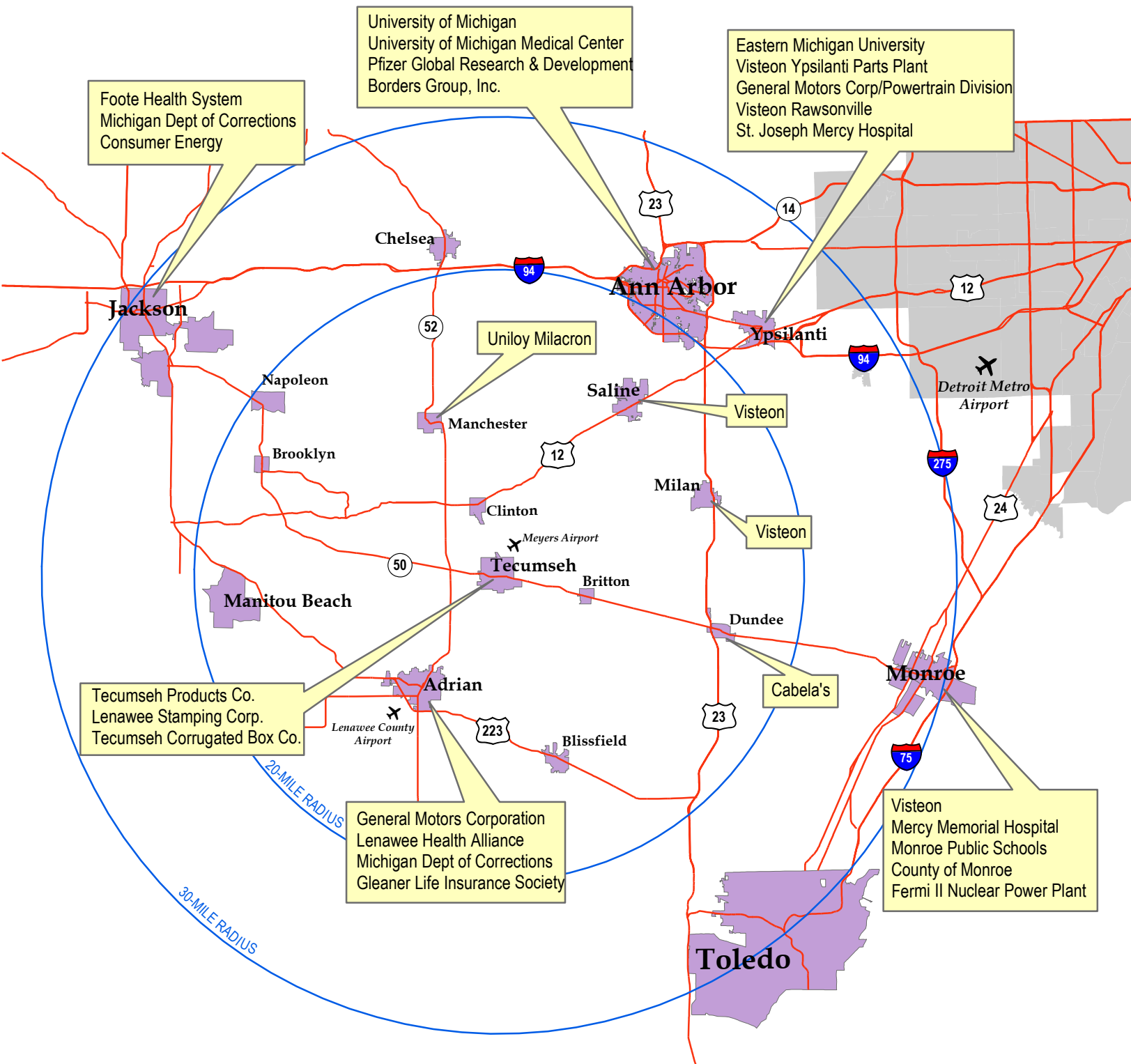
The Tecumseh Township Master Plan was adopted in January of 1997. The Plan primarily recommends Suburban Residential uses adjacent to the City of Tecumseh. These areas are intended to accommodate a range of housing types, including detached single family homes, townhouses, apartments, and manufactured housing. A one-half acre minimum lot size is recommended for Suburban Residential areas.

The Plan designates several areas abutting the City for Commercial use, including all of the frontage along M-50 on the west side of the City. There are two areas adjacent to the City that are planned for Rural Estate Residential. These areas are intended for single family residential development without public utilities at a maximum density of one dwelling unit per acre.

OTHER INFLUENCES

At the county level, the Lenawee County Planning Commission with staff assistance from the Region II Planning Commission provides land use-planning services including the advisory review of certain Township zoning and planning activities. In addition, the Region II Planning Commission provides staff resources, technical support, and land use and transportation planning services to municipalities within the County. Transportation services include the preparation of the Transportation Improvement Program (TIP), which is the schedule and budget for proposed federally funded and regionally significant transportation improvements within Lenawee County. The Lenawee County Planning Commission also assists in the preparation and implementation of the County's Solid Waste Management Plan, and provides an array of aerial photography, mapping and GIS digital data products.

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REGIONAL TRANSPORTATION & EMPLOYMENT

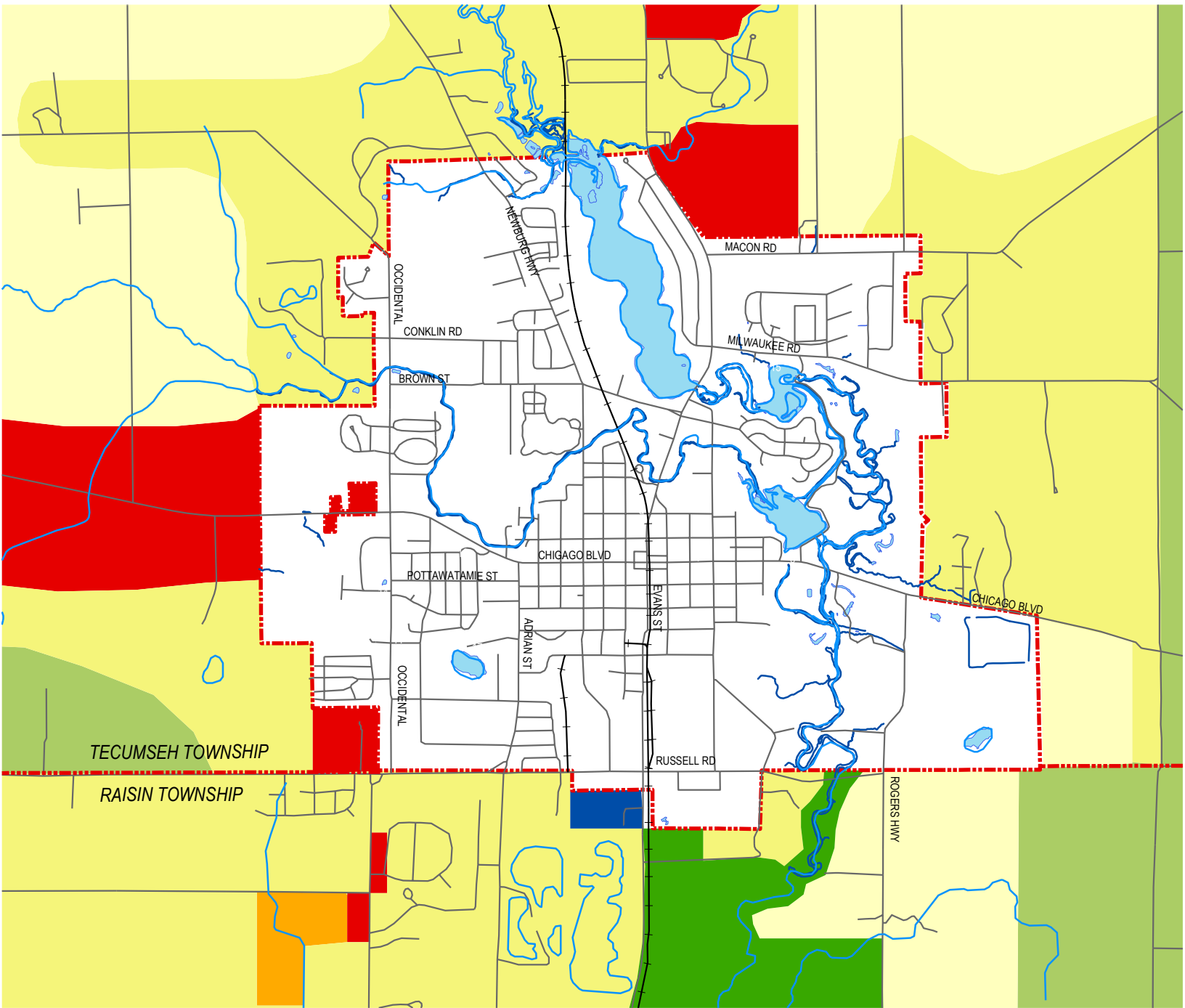
CITY OF TECUMSEH
 LENAWEE COUNTY, MICHIGAN

MAP 2-2

Sources: MONROE COUNTY CHAMBER OF COMMERCE;
 WASHTENAW DEVELOPMENT COUNCIL;
 MICHIGAN ECONOMIC DEVELOPMENT CORPORATION;
 THE ENTERPRISE GROUP OF JACKSON, INC.



BIRCHLER ARROYO
 ASSOCIATES, INC.



RAISIN TOWNSHIP

- AGRICULTURAL
- RURAL RESIDENTIAL
- LOW DENSITY RESIDENTIAL
- HIGH DENSITY RESIDENTIAL
- COMMERCIAL
- PUBLIC / QUASI PUBLIC
- PARKS & RECREATION

TECUMSEH TOWNSHIP

- AGRICULTURAL
- RURAL ESTATE
- SUBURBAN RESIDENTIAL
- COMMERCIAL

**FUTURE LAND USE
OF ADJACENT COMMUNITIES
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP 2-4**

Sources: TECUMSEH TOWNSHIP;
RAISIN CHARTER TOWNSHIP



**BIRCHLER ARROYO
ASSOCIATES, INC.**

3

SUMMARY OF EXISTING CONDITIONS



An evaluation of the City's characteristics is a basic ingredient in planning for its future.

T

he planning process begins with an evaluation of a community's characteristics, such as population, housing, workforce, income, local market potential,

development patterns, natural resources and other pertinent factors. This information is a basic ingredient in planning for the future. Historical and current population trends can be used in various ways to illustrate problem areas of development, identify opportunities for growth and improvement, and provide an indication of probable future needs. Once a database of existing conditions is compiled, a community can use the findings to help set goals for the future development of the community.

This chapter is intended to summarize and highlight important findings from the Planning Commission's analysis of the community's existing conditions. Complete reports on population, housing, and economic statistics, existing land use, community facilities, natural features, and transportation conditions can be found in the Appendix of this document.

POPULATION, HOUSING, AND ECONOMY

POPULATION

The City of Tecumseh had a 14.9 percent increase in population between 1990 and 2000. This is a major increase from the previous two decades, in which the City experienced increases in population of 1.9 and 2.8 percent. It is also significantly higher than Lenawee County's 8.1 percent

Summary of Existing Conditions

rate of growth between 1990 and 2000. Since 1970, the City's share of the County's population has remained steady, in the range of 8.1 to 8.7 percent. However, the City's share of the County's growth has been increasing, from 2.4 percent in 1980 to 9.3 percent in 1990 to 15 percent in 2000.

For planning purposes, it will be assumed that the City of Tecumseh can expect a population ranging between 9,072 and 9,283 people in 2010, and 9,403 to 10,050 people in 2020.

HOUSING

Between 1990 and 2000, the number of households in the City increased by 20.5 percent. The number of households in the community increased at a rate that was higher than the rate of population increase. This is consistent with a nationwide trend of decreasing household size. The number of housing units in the City of Tecumseh increased by 21.7 percent during the last decade, which is consistent with the 20.5 percent increase in households.

INCOME, EMPLOYMENT, AND EDUCATION

From 1989 to 1999, the City of Tecumseh's per capita income increased by 54.2 percent from \$14,781 to \$22,797. During this same time period, median household income increased by 37.4 percent from \$33,545 to \$46,106. According to the Census, 3.5 percent of Tecumseh families were living below the poverty line. This is a lower percentage than that of Lenawee County as a whole, which had a 4.4 percent family poverty rate.

Per the 2000 Census, "management and professional" and "sales and office" occupations make up the majority of the total occupations of City of Tecumseh residents. The employment industry distribution of Tecumseh residents is dominated by the manufacturing; educational, health and social services; and retail trade industries.

Tecumseh residents generally have a higher level of educational attainment than the County as a whole. For example, 88 percent of City adults have a high school degree or higher, while 83.4 percent of the County's adult population falls into this category.

EXISTING LAND USE

RESIDENTIAL USES

According to the 2003 Existing Land Use Survey, approximately 37 percent of the City's land area is developed as residential. Single family development dominates the residential land use in the City with 33 percent of the total land area and 88 percent of residentially developed land. Less than one percent of the City's total land area and approximately one percent of residentially developed land is within two-family developments; three percent of the total land area and nearly nine percent of residentially developed land is comprised of multiple family developments; and one percent of the City's total land area and two percent of residentially developed land is within a mobile home park.

COMMERCIAL, OFFICE, AND INDUSTRIAL USES

Commercial and office uses in the City comprise approximately 2.5 percent of Tecumseh's total land area. About 77 percent of these uses are categorized as commercial, 16 percent as

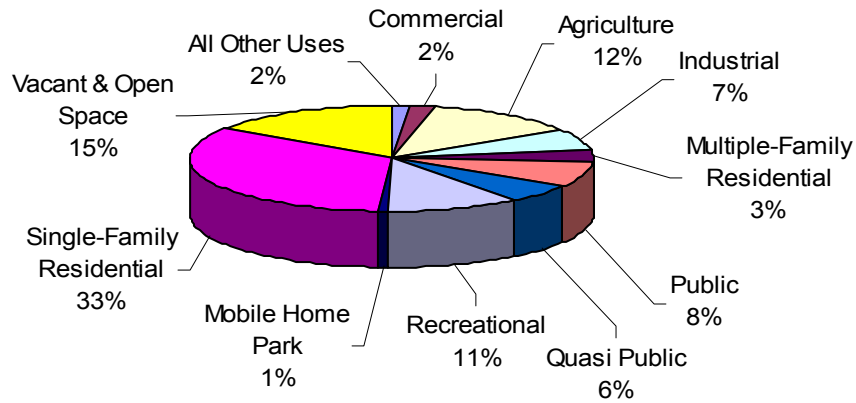
Summary of Existing Conditions

office, and seven percent as downtown mixed use. Industrial uses comprise approximately seven percent of the City's total land area.

OTHER LAND USES

Public uses, including government buildings, public schools, public parking areas, and public cemeteries, make up approximately eight percent of the City's land. Quasi-public uses, such as churches and service organizations, comprise about six percent of the City's total land area. Approximately 11 percent of the City's land is used for recreational purposes, 12 percent is used for agriculture, and another 15 percent remains open or vacant.

**Figure 3-1
Percent of Total Land Use
City of Tecumseh, 2003**



COMMUNITY FACILITIES

CITY FACILITIES

In addition to City Hall, the Community Center, and the Economic Development Office, the City of Tecumseh owns the Civic Auditorium and the former St. Elizabeth's Catholic Church building which now houses the Tecumseh Historical Museum. The Auditorium is operated and maintained through a combination of private donors, commercial sponsors, and volunteers who donate their time to build sets, usher, and run the box office. The Museum is maintained and operated by the Tecumseh Area Historical Society.

SCHOOLS

The Tecumseh Public School District serves students from an 80-square mile area including the City of Tecumseh and parts of the Townships of Adrian, Clinton, Franklin, Macon, Raisin, Ridgeway and Tecumseh. The district includes four elementary schools (Herrick Park, Patterson, Sutton, and Tecumseh Acres), Tecumseh Middle School, Tecumseh High School, and Options High School (an alternative high school). A major recent development in the Tecumseh Public Schools was the opening of the new thirty-three million dollar high school for the 2001/2002 school year.

LIBRARY

The Tecumseh District Library serves a population of over 17,000 people, with over 11,000 active borrowers. The library is an independent entity; that is, it does not have any branches nor does it belong to any larger system. The library collection contains over 50,000 print, audio, video, and electronic resources.

PARKS AND RECREATION

In addition to the Community Center, the City of Tecumseh Parks and Recreation Department currently operates a system of 15 public recreation sites and facilities. Year round recreation programming includes activities and sports for children of all ages, adults, and families.

PUBLIC SERVICES AND UTILITIES

The City of Tecumseh Police Department includes 15 sworn officers, one animal control and enforcement officer, two full-time and several part-time dispatchers. The police station is open from 8 a.m. to midnight, but officers provide service to residents 24 hours a day. The City's Emergency Services Department consists of about 20 paid on-call firefighters and paramedics and three full time Firefighter/EMTs. The Department is equipped with two fire engines, one fire engine ladder truck, one tanker, one light rescue vehicle, one brush truck, and a chief's car. In addition to fire and emergency medical services, the Department maintains and operates severe weather warning sirens. In 2001, the Emergency Services Department answered 597 service calls.

Tecumseh's municipal water comes from seven wells around the City. The Utilities Department is responsible for drinking water treatment, distribution and wastewater treatment. The City maintains 49.5 miles of pipe and the water system can process 5 million gallons of drinking water per day. The City's sewer system essentially provides service to the entire city, except for a few isolated areas. It also provides sewer service to some businesses outside City limits. The plant currently processes about a million gallons per day.

NATURAL FEATURES

TOPOGRAPHY

The City of Tecumseh's topography varies by about 120 feet from the lowest point to the highest point. The lowest areas of 750 feet are located at the bottom of the River Raisin riverbed, and the highest point of 868 feet is located in the very northwest corner of the City near Occidental Road.

GROUNDWATER RECHARGE

A majority of the City can be categorized as a high groundwater recharge area, while some small pockets of moderate groundwater recharge areas can be found at the western and eastern edges of the City. Existing and planned development within recharge areas should include safeguards against negative impacts, as pollution of these areas could have a significant detrimental effect on the area's groundwater.

WATERSHEDS

All runoff water in the City of Tecumseh eventually enters the River Raisin. In the north, east, and south-central parts of the City, water flows into the river directly. Water in the west-central part of the City flows first into Evans Creek, which empties into Mill Pond, and then into the River Raisin. In the southwest corner of the City, water flows into the South Branch of the River Raisin.

WOODLANDS AND WETLANDS

The City of Tecumseh has only a few wetland areas. Nevertheless, wetlands are valuable natural resources within the City. They may serve as storm water holding areas to reduce flooding; provide for the settling of sediments and pollutants from surface water runoff; reduce stream bank erosion caused by storm water runoff; and provide unique habitat for fish and wildlife.



Prior to settlement of southeast Michigan, the area that is now the City of Tecumseh was almost entirely covered by forests; however, after more than two centuries of agricultural and urban development the woodlands in the City cover a much smaller area. Still, the remaining woodlands are valuable natural features which serve as windbreaks, aid in the absorption of rainwater, replenish oxygen, create natural beauty and character, and provide wildlife habitat. Often, woodlands and wetlands are found in conjunction with each other, forming an even more diverse habitat for plants and animals.

TRANSPORTATION CONDITIONS

FUNCTIONAL CLASSIFICATION

Although there is some variation in classification, roadways are typically divided into those that carry through traffic and those that carry local traffic. In order to function successfully, the overall traffic circulation system must be carefully integrated. In the City of Tecumseh, the four basic types of roads are arterials, minor arterials, collectors, and local streets. Those classified as arterials are Chicago Boulevard (M 50), Evans, and Macon. Those classified as minor arterials are Russell, Occidental, Burt, and Rogers Highway. The City's collector streets are Brown, Maumee, Adrian, Union, and Patterson. All other streets in the City function as local streets with the primary function of providing access to adjacent properties.

TRAFFIC VOLUMES AND CRASHES

The City experiences its highest traffic volumes along its two main streets: Chicago Boulevard (M 50) and Evans Street. Occidental also carries a relatively significant volume of traffic south of Chicago Boulevard. Not surprisingly, the intersections with the highest number of crashes during the two-year time period of 2001 through 2002 are primarily located along these same three roadways.

Summary of Existing Conditions

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4

VISION STATEMENT, GOALS & OBJECTIVES

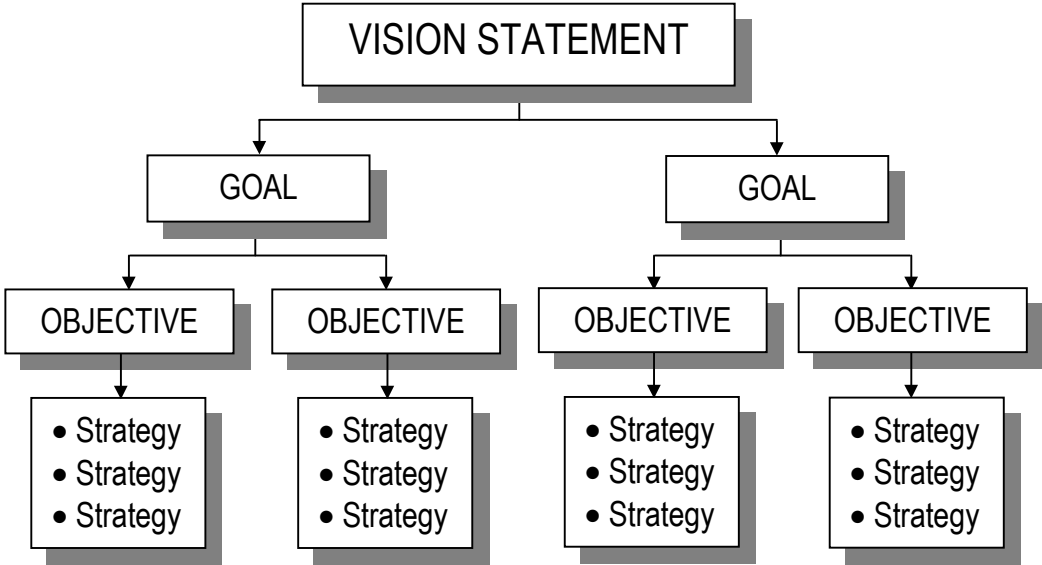
The Vision Statement, Goals & Objectives describe the community's intent for the future. They guide all other aspects of the Comprehensive Plan.



On November 11, 2003, the City of Tecumseh held a Community Open House and Vision Session. Both events were opportunities for the Planning Commission to seek input from residents, community leaders, and neighboring communities. This input, along with the foundation of the Goals and Objectives from the City's 1990 Master Plan, contributes to the policy basis from which long-range land use decisions can be made. Please see Appendix Chapter A6 for more detailed information about the Vision Session.

The **Vision Statement** is a guide to provide direction to the Planning Commission in the formulation of the goals and objectives of the Master Plan. **Goals** are typically very general statements about the quality and character of the community that are not very easily quantified or measured. Goals must be translated into measurable **objectives** that can be prioritized and pursued by instituting specific **strategies** that will be followed. The goals and objectives proposed are intended to result in a specific quality and character for the community. This will be achieved by following strategies designed to permit measurable progress toward achieving the City's objectives. These strategies will have to balance the public purposes inherent in planning for the long-range future of the community with the rights of private property owners to develop an appropriate use of their land.

The following illustration provides a graphic depiction of the relationship between the vision statement, goals, objectives, and strategies.



VISION STATEMENT

The City of Tecumseh intends to promote a vision of its future where:

- clean air, water, and soils are the rule and are not taken for granted;
- residents and visitors continue to enjoy the City’s vibrant downtown, historic architecture, and natural surroundings;
- new development is a positive contribution to the community;
- growth and development occur in a manner that contributes to the community’s unique historic character;
- residential development contributes to and accommodates the variety and complexity of the City’s population;
- the local transportation network affords proper motorized and non-motorized access
- land uses are properly located and regulated to prevent adverse impacts on neighboring uses;
- the environment receives attention and protection from adverse influences;
- public services are provided consistent with the community’s needs, in a fiscally responsible manner; and
- planning and land use regulatory efforts balance the long-term goals and objectives of the community and the individual resident.

GOALS AND OBJECTIVES

COMMUNITY CHARACTER

GOAL: Maintain and enhance the unique, historic character of Tecumseh.

OBJECTIVE: Maintain the close-knit community atmosphere in the City of Tecumseh.

Strategies:

- Encourage uses that are compatible with existing developments.
- Promote pedestrian-friendly designs and uses throughout the city.
- Promote a focus on the residential neighborhoods to maintain the community-minded feel.
- Improve and expand wayfinding signage system to aid in navigation and draw attention to desirable features of the community.

OBJECTIVE: Protect and promote the historic character of the City of Tecumseh.

Strategies:

- Implement historic preservation guidelines to maintain the integrity of existing, historic buildings.
- Encourage use of historically sensitive styles and features in new building construction.
- Emphasize entrances to the City to create an inviting atmosphere for residents and visitors alike.

NATURAL FEATURES

GOAL: Protect and enhance the environmental assets in the City, including clean air, water, and soils, as well as woodlands, wetlands, and lakes.

OBJECTIVE: Carefully consider methods to preserve natural features for the enjoyment of the current and future population.

Strategies:

- Reevaluate zoning ordinance standards to protect woodlands and wetlands.
- Require tree replacement when existing resources are impacted by development. Provide incentives for new tree plantings.
- Consider preserving open space, water bodies, and natural features through a land conservancy or other non-profit



organization.

- Protect the City's water bodies, including rivers, streams, and creeks, from degradation and destruction.
- Enhance the viewsheds of woodlands, wetlands, and other water bodies.
- Consider zoning ordinance provisions which encourage development designs which minimize lot coverage, maximize open space, and allow flexibility in accommodating and protecting environmentally sensitive areas.

RESIDENTIAL

GOAL: Provide a variety of housing options for City residents in various stages of their lives, while maintaining the historic character and sense of community in the City of Tecumseh.

OBJECTIVE: Preserve and strengthen existing residential neighborhoods in the City of Tecumseh.

Strategies:

- Continue to encourage the conversion of homes in historic neighborhoods back to single-family residential.
- Consider instituting a City inspection program for rental properties in order to maintain a healthy and safe housing stock.
- Ensure that nonconforming commercial property in residential neighborhoods is redeveloped as residential when available.
- Continue to protect residential neighborhoods from encroaching non-residential uses through the establishment of appropriate zoning districts, performance standards, and screening requirements.
- Assist City residents in applying for grants and loans to rehabilitate, restore, and refurbish historic properties.
- Implement the Secretary of the Interior's Historic Preservation Guidelines to ensure historically appropriate renovations and additions to historic properties.
- Consider developing more stringent controls regarding maximum heights and maximum footprint of new residential structures to anticipate "bigfoot" development.
- Continue to require developers to provide connected, pedestrian-friendly streets with sidewalks in new subdivisions, in order to maintain the sense of community in Tecumseh.

OBJECTIVE: Provide housing opportunities for individuals and families of all types and income levels.

Strategies:

- Promote a variety of housing choices which include both rental and owner-occupied options.
- Encourage continued variety of single-family dwellings to provide options for households with varying incomes and needs.

Vision Statement, Goals & Objectives

- Provide opportunities through zoning for affordable housing developments, including attached and detached products.
- Provide housing for seniors in all stages of independence so that they may remain residents of the City as long as they wish.
- Continue to encourage upper-story residential in the Downtown area.
- Plan for historically compatible new townhouse construction in the Downtown area.

COMMERCIAL

GOAL: Encourage an appropriate mix of new commercial development that is compatible with Tecumseh's character and needs.

OBJECTIVE: Promote the development of a variety of goods and services establishments within the community to accommodate the needs of Tecumseh residents.

Strategies:

- Address the relationship between adjacent uses by developing site plan review standards that encourage service drives, interconnected parking and shared drives, landscaping, screening and signage which is cohesive and compatible with the existing development patterns.
- Review sign standards to insure appropriate scale and lighting.
- Encourage the development of commercial uses that are underrepresented or not currently located in Tecumseh, such as a hotel or motel and men's and women's clothing stores.
- Implement controls, such as buffering requirements and other zoning ordinance standards, to ensure that business development does not negatively impact surrounding land uses, particularly residential dwellings adjacent to the central business district.
- Implement techniques to encourage creative redevelopment and continued maintenance of vacant and blighted commercial properties.
- Encourage office uses in commercial districts along side streets; encourage more retail and restaurant uses along Chicago Boulevard.
- Consider redevelopment of underutilized areas for office and medical uses.
- Enhance existing commercial shopping areas such as the West Chicago shopping area, Logan/Evans, and Evans/Brown.



OBJECTIVE: Ensure that new and rehabilitated commercial structures represent building mass and design that is compatible with the traditional and historic structures in Tecumseh.

Strategies:

- Implement controls which will limit the size and mass of new commercial structures.
- Develop guidelines that encourage new commercial facades to be compatible with existing structures.
- Encourage the use of masonry and other natural materials for new building construction.
- Ensure that existing and future land uses at City entranceways have attractive site and building designs that project a positive image as one enters the City.

INDUSTRIAL

GOAL: Provide for redevelopment and expansion of the City's industrial base, while mitigating the effects on other uses.

OBJECTIVE: Locate industrial uses in those areas of the City which are best equipped to handle their impacts.

Strategies:

- Fill existing industrially-zoned areas before rezoning additional land for industrial.
- Consider a carefully developed high-tech industrial park, perhaps in partnership with Tecumseh Township.
- Consider encouraging the establishment of a community college satellite in the City, to provide jobs and training to residents of the City and nearby communities.
- Designate industrial areas on the Land Use Plan to accommodate larger industrial users.
- Develop strategies and incentives to maintain and enhance existing industrial users, and to attract new users.

OBJECTIVE: Minimize the negative impact of industrial development on roads, adjacent land uses, and the environment.

Strategies:

- Address the relationship between adjacent uses by developing site plan review standards that encourage service drives, combined parking and drives, landscaping, screening and signage which is cohesive and compatible with the existing development patterns.
- Review and enhance the zoning ordinance performance standards for all uses relating to hazardous materials and other negative environmental impacts.
- Review and enhance the zoning ordinance screening and landscaping requirements for industrial sites.
- Review existing code enforcement efforts to insure adequacy of violation response.
- Consider the establishment of a truck route to protect the downtown area from pollution, noise, road damage, and vibration.

TRANSPORTATION

GOAL: Provide a transportation network that facilitates the safe and efficient movement of people and goods.

OBJECTIVE: Maintain and enhance current road network.

Strategies:

- Maintain an appropriate balance between mobility and accessibility.
- Maintain roadways so as to provide safe and efficient access throughout the community.
- Continue to work cooperatively with MDOT to provide an efficient and safe system of arterial roadways.
- Encourage public-private partnerships that result in development paying its fair share of major roadway improvements.
- Address the relationship between adjacent uses by developing site plan review standards that encourage service drives, combined parking and drives, landscaping, screening and signage which is cohesive and compatible with the existing development patterns.
- Implement good access management practices to reduce the number of driveways for individual sites.
- Determine which road improvements, such as signals and left turn lanes, may be appropriate for problem intersections.
- Eliminate vestigial one-way streets.
- Consider the establishment of a truck route to protect the downtown area from pollution, noise, road damage, and vibration.
- Require acceleration and deceleration lanes for large developments that will impact traffic conditions in the immediate area.



OBJECTIVE: Promote the use of non-motorized facilities throughout the City

Strategies:

- Promote a pedestrian-friendly atmosphere throughout the City.
- Extend sidewalks and/or bike paths to the west end of the City.
- Make sure that sidewalks and bike paths are developed with sensitivity to the surrounding neighborhood.
- Connect parks and schools with a pathway system.
- Connect pathway system to other trails in the area to expand recreational opportunities for residents.
- Require sidewalks in new developments.
- Encourage and support rails-to-trails pathways.

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5

LAND USE PLAN



The Land Use Plan is a representation of the preferred land use arrangement for the community.

The City of Tecumseh's land use plan is a representation of the preferred land use arrangement for the community. It identifies and defines the major future land use categories as well as the approximate locations for various uses envisioned by the Planning Commission. The boundaries reflected on the map are not intended to indicate precise size, shape or dimension; rather they portray a general land use arrangement, which may be refined as the community develops. In addition, the recommendations of the Land Use Plan have a long-range planning horizon and do not necessarily imply that short-range rezoning is appropriate. The general recommendations of the Land Use Plan are further detailed through the Housing, Thoroughfare, Economic Development, Downtown Tecumseh, Capital Improvements, and Natural Resources plan elements.


Each of the plan elements includes an analysis of current conditions, recommendations for future development, and action items intended to assist with implementation of the plan.


FUTURE LAND USE CATEGORIES


The following describes the land use categories reflected on the Land Use Plan Map 5-1:


RESIDENTIAL USES


There are several residential land use categories including on the Land Use Plan Map, in order to accommodate the needs of all the City's residents. It is important to note that certain other uses such as parks and schools are appropriate in residential districts, if there is a need for expansion of the Parks and Recreation or School systems in the future. The Housing Plan describes the location and preferred densities of the following categories of residential uses in detail.

-  **Low Density Single Family Residential**
Most of the large undeveloped parcels in the City are planned for Low Density Single Family Residential, in keeping with the City's primarily residential character. This includes remaining undeveloped areas north of Brown Street, along Macon Road near the airport, and on either side of Rogers Highway in the southern part of the City (including the Bonner Farm property).

-  **In Town Residential**
This predominantly single-family residential category is intended to include the existing older and historic homes surrounding the Central Business District. These homes are characterized by nineteenth-century and early twentieth-century American vernacular architecture. They tend to have smaller front yards and detached garages behind the house. Many have porches. Some two-family uses may be appropriate for this district, if the existing character and design of the home is not compromised.

-  **Two Family Residential**
Two family residential uses are planned along Waskin Drive between Russell Road and Tecumseh Acres Elementary School, and along Mill Highway south of Russell Road.

-  **Low Density Multiple Family Residential**
Areas of Low Density Multiple Family Residential are planned along Union Street at Russell Road, at Brown Street and Occidental, and along Occidental between the Commercial district and South Occidental Park. This category could appropriately include senior housing facilities.

-  **Townhouse/ Attached Single Family**
Areas of Townhouse/Attached Single Family have been designated primarily in two places on the Land Use Plan Map. One area is located along both sides of Chicago Boulevard between Maumee Street and the parkland on the north side and the water treatment plant land on the south side. This area also extends south of the intersection of Division Street and East Chicago Boulevard, near the hospital. The second area designated for Townhouse/Attached Single Family is north of the Downtown district along Evans Street between Shawnee and Brown Streets. Townhouse developments may appeal to residents who wish to live close to downtown but may need more space than an upper-story apartment or condo can provide. Townhouses may also provide another option for empty-nesters or seniors who wish to live close to downtown services, shopping, and events.



Flexible Development

This land use category is designed to provide for flexible but primarily residential development in two specific areas of the City. See Chapter 7, Housing Plan, for details.



Mobile Home Park

The Land Use Plan Map shows a potential expansion of mobile home uses adjacent to the Newburg Meadows Mobile Home Park and Red Mill Pond. A well-maintained and attractive mobile home park can be a great asset to the community, as it provides affordable homeownership opportunities.



Downtown Mixed Use Housing

This housing category refers primarily to second and third-floor apartments and condominiums over first-floor retail or office in the Central Business District. These units are accessed by internal or external stairs. This plan encourages residential use of the second and third floors of downtown buildings, since a larger population downtown contributes to the viability of the businesses there and the general vitality of the district. This type of housing provides yet another unique option for people wishing to locate or relocate within the City.

COMMERCIAL USES

The Economic Development element defines the commercial categories introduced below in greater detail and describes the process used to determine the appropriate location and composition of future commercial development.



Downtown Mixed Use

This land use category encompasses the existing two-block traditional downtown along West Chicago Boulevard, and extends the district along the Boulevard on the north side, to Oneida Street and on the south side, to Maumee Street. Areas of Downtown Mixed Use are also planned along Evans Street north of Chicago, to just north of Shawnee Street. Buildings in this district should be two or three stories and include retail or restaurant uses on the first floor, with residential and limited office uses on the upper floors. In addition to restaurants, some examples of appropriate uses are drug stores, florists, beauty salons, bookstores, specialty shops, clothing stores, hardware stores with no outdoor storage, bank branches, and bakeries.



General Commercial

Larger, community commercial centers and highway-oriented business are included within this land use category. Areas designated for this land use are located along West Chicago Boulevard west of the high school.



Office

Uses with this land use designation include professional offices, medical offices, and personal services that are ancillary to an office use. In some cases, office uses act as a transition between higher intense light industrial and/or retail commercial uses and residential areas. The largest concentration of office surrounds the hospital.



Industrial Commercial

This land use category is intended to include light industrial enterprises that conduct some wholesale or custom retail business on the premises. All operations should be enclosed within a building, an outdoor storage would not be permitted.



Industrial

These areas are intended to accommodate office, light industrial, warehousing, and research and development uses, as well as manufacturing, assembly, and fabrication operations. Outdoor storage should be permitted to meet the needs of such uses, yet should also be appropriately screened, especially when adjacent to non-industrial land uses and thoroughfares.

OTHER USES



Public / Semi Public

Public buildings, schools, and other lands owned or used by the public to provide non-commercial cultural, recreational and social services are included in this category. These public/semi-public uses could be allowed as part of the surrounding zoning district as a permitted use or as a special land use.



Recreation

City and school-owned parks and preserves are included in this category, as well as areas planned for future parks. Private recreational facilities such as golf courses are also included.



Greenbelt

Two planned greenbelt areas have been defined on the Land Use Plan Map. The area in the northern part of the City includes an existing large stand of trees that provides an important focal point at the entrance to the City. Any future development should incorporate and protect this wooded area. The greenbelt in the southern part of the City is designed to preserve the character of Rogers Highway despite developments that may occur along it. The greenbelt will also shield proposed residential developments from truck traffic that may be directed along the road.



Waterway Protection Overlay District

The Waterway Preservation Overlay District is designed to limit the impact of development along the River Raisin. For more information on this district, please consult the Natural Resources/Environmental Protection element.

Table 5-1
Land Use Plan Summary

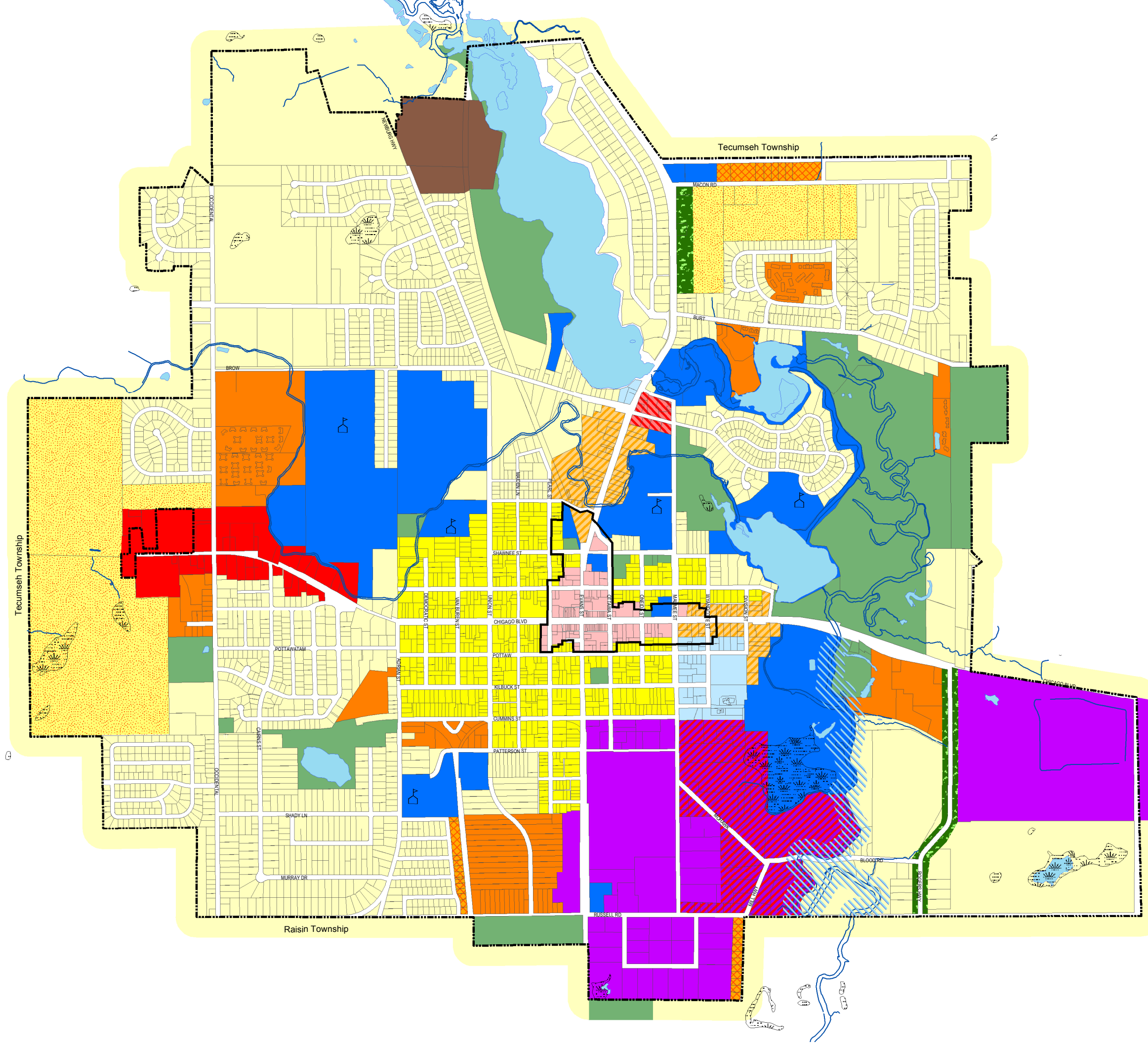
Land Use Category	Acreage	% of Total
Single Family Residential	1238.0	39.4%
In Town Residential	185.8	5.9%
Two Family Residential	17.7	0.6%
Multiple Family Residential	180.1	5.7%
Townhouse/Attached Single Family	46.3	1.5%
Mobile Home Park	40.3	1.3%
Flexible Development	246.9	7.9%
Downtown Mixed Use	24.2	0.8%
Neighborhood Commercial	5.1	0.2%
General Commercial	64.6	2.1%
Office/Medical	24.5	0.8%
Industrial Commercial	93.9	3.0%
Industrial	273.5	8.7%
Public	368.0	11.7%
Recreation	313.2	10.0%
Greenbelt	22.6	0.7%

LAND USE PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing conditions, Goals and Objectives, housing trends, and projected buildout analysis for the City:

- Rezone land as appropriate to eventually achieve the arrangement shown on the Land Use Plan Map.
- Develop Design Guidelines to help builders/developers understand the community's vision for development. Include incentives, such as density and development flexibility, within development options that encourage their use by developers.
- Encourage the maintenance and rehabilitation of existing housing stock by assisting residents with grants and other funding.
- Update standards to ensure that parking is sufficient but not excessive. Allow land-banking of parking spaces.
- Consult the Goals and Objectives chapter on a regular basis to ensure that Planning Commission decisions are in keeping with the intent of the Master Plan.

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- SINGLE FAMILY RESIDENTIAL
- IN TOWN RESIDENTIAL
- TWO FAMILY
- MULTIPLE FAMILY RESIDENTIAL
- MOBILE HOME PARK
- TOWNHOUSE/ATTACHED SINGLE FAMILY
- FLEXIBLE DEVELOPMENT
- DOWNTOWN MIXED USE
- NEIGHBORHOOD COMMERCIAL
- GENERAL COMMERCIAL
- OFFICE/MEDICAL
- INDUSTRIAL COMMERCIAL
- INDUSTRIAL
- PUBLIC / QUASI-PUBLIC
- RECREATION
- GREENBELT
- WETLAND
- DOWNTOWN DISTRICT OUTLINE
- WATERWAY PRESERVATION OVERLAY DISTRICT
- SCHOOLS

NOTES:

1. This map is intended to show generalized land use and is not intended to indicate the precise size, shape, or dimension of areas. These recommendations have a long-range planning horizon and do not necessarily imply that short-term rezoning decisions are appropriate.
2. The City of Tecumseh Comprehensive Plan includes the Land Use Plan Map and all text, maps, charts, tables, and other graphics in the full Comprehensive Plan report.
3. See the Residential Density Plan map for more specific residential density recommendations.

LAND USE PLAN
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN

MAP 5-1
 500 Feet



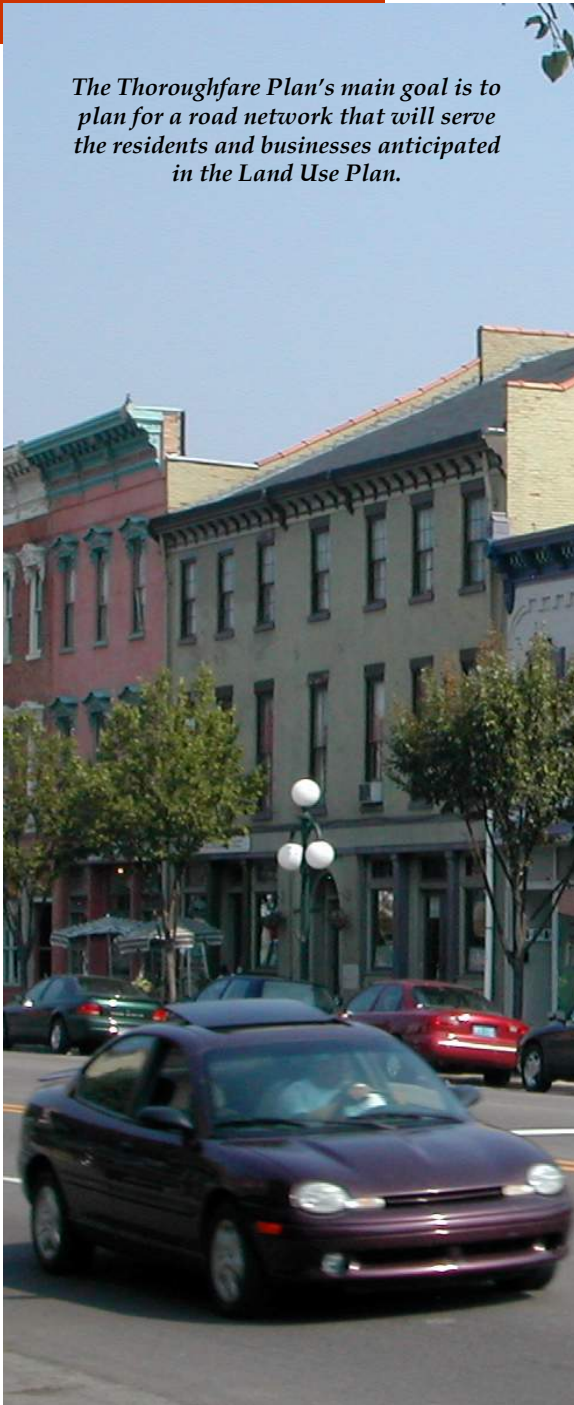
BIRCHLER ARROYO
 ASSOCIATES, INC.

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6

THOROUGHFARE PLAN

The Thoroughfare Plan's main goal is to plan for a road network that will serve the residents and businesses anticipated in the Land Use Plan.



The Thoroughfare Plan's main goal is to plan for a road network that will serve the residents and businesses anticipated in the Land Use Plan chapter. This includes the City of Tecumseh's road network of state, county, and private roadways.

CURRENT ROAD CONDITIONS, TRAFFIC VOLUMES, AND HIGH CRASH LOCATIONS

Like the land use planning process, the process for planning a community's transportation system must begin with a study and analysis of existing conditions. This includes an analysis of current road conditions, traffic volumes, and recent vehicle crash data.

The Existing Road Conditions Map 6-1 identifies the number of lanes on the roadways, traffic signal locations, and locations on the road system that may have a safety concern, such as offset and skewed intersections.

The Traffic Volumes Map 6-2 displays the traffic volumes within the City of Tecumseh from the year 2001 through 2003. As the map indicates, the highest volumes are located on the City's two main streets: Chicago Boulevard (M 50) and Evans Street. Occidental Road also carries a relatively significant volume of traffic south of Chicago Boulevard.

Crash data obtained from the Region II Planning Commission were used to develop the Average Annual Crashes Map 6-3. The map illustrates intersections at which four or more crashes occurred within a two-year time period from January 2001 through December 2002. The intersections with the highest number of crashes are primarily

located along the City’s most heavily traveled roads, including Chicago Boulevard (M 50), Evans, and Occidental.

The intersection crash frequencies shown on Map 6-3 vary primarily with the amount of traffic passing through those intersections. To better assess the actual hazard levels, crash rates – in terms of crashes per million entering vehicles (MEV) – were determined for the nine intersections having both four or more crashes in two years and a means of estimating daily traffic volumes. Relative to a large sample of comparable intersections throughout southeast Michigan, it was found that three Tecumseh intersections have average or above-average crash rates (all others are below average):

- ❖ M 50 / Maumee – 1.26 crashes per MEV (94% of average)
- ❖ Occidental / Russell – 1.15 crashes per MEV (98% of average)
- ❖ M 50 / Occidental – 1.70 crashes per MEV (142% of average)

According to procedures outlined in the *SEMCOG Traffic Safety Manual* (developed for statewide use), only the crash rate for M 50 and Occidental is sufficiently above the sample average to confidently declare it a "high-crash" location.

FUNCTIONAL CLASSIFICATION

Traffic is concentrated on certain roadways due to the roads’ physical condition, level of use, and direction of travel, as well as the overall land-use pattern. About 20 percent of the roads carry approximately 80 percent of vehicle miles traveled.¹ In order to set funding priorities for the roads that carry the highest volumes, transportation planners established a street classification system. Table 6-1 to the left illustrates the street classification system concept used in the City of Tecumseh. This functional classification system is the foundation upon which the road network is designed.

Table 6-1
**Roadway Functional Classification,
 City of Tecumseh**

Road
Arterials
Minor Arterials
Collectors
Local

Although there is some variation in classification, roadways are typically divided into those that carry through traffic and those that carry local traffic (see Figure 6-1 on page 6-3). It is desirable to physically separate these two road types as much as possible to eliminate conflicting traffic movements, traffic congestion, delays, and crashes.

In order to function successfully, the overall traffic circulation system must be carefully integrated. In Tecumseh, the four basic types of roads are arterials, minor arterials, collectors, and local streets. In addition, there are non-motorized pedestrian safety paths. The definitions for safety paths and the roadway types are provided on the following two pages. The role of each road classification in providing access and mobility is illustrated in Figure 6-2 below. Generally, as access increases, mobility decreases – and vice versa.

¹ Coghlan, Gerald. "Opportunities for Low Volume Roads", *Transportation in the New Millenium*, Transportation Research Board, 1999.

ARTERIALS

Arterials are the “highest order” of surface streets, and they typically carry high volumes of traffic. Arterials provide travel routes from one city to another, and can traverse one or more states. They are most often used for longer trips, as higher speeds are allowed. When an arterial passes through a more populated area, the number of intersections increases and speeds decrease. Arterial roads have a dual function: to provide routes for through traffic while providing access to abutting properties and minor intersecting streets. This can lead to congestion and traffic crashes because of turning vehicles conflicting with through traffic. Examples of Tecumseh roads currently functioning as arterials include Chicago Boulevard (M 50), Evans Street, and Occidental Highway.

MINOR ARTERIALS

Minor arterials serve a similar function as arterials; however, these roads typically carry less traffic over shorter distances than arterials. Examples of existing minor arterials within the City of Tecumseh include Rogers Highway, Russell Road, Macon Road, and Burt Street.

COLLECTOR STREETS

The collector streets primarily permit direct access to abutting properties and provide connections to higher order roadways including minor arterials and arterials. Through traffic movement from one part of the City to another is deliberately discouraged on these streets. The collector street, in most cases, is a public roadway serving moderate traffic movement from local streets to arterial streets. Although collectors permit access to abutting property, it is preferable that they do not serve an access function for residential lots. The collectors may accommodate pedestrians and public utility facilities within the right-of-way. Collectors feed the arterials, thus reducing the number of curb cuts onto arterials and ensuring fewer interruptions for arterial traffic. Some collectors are residential collectors and others are nonresidential collectors. The nonresidential collectors accommodate traffic generated by industrial and commercial developments. The residential collectors connect local streets serving residential areas to the arterial system. Examples of existing collector roads within the City of Tecumseh include Adrian Street, N. Union Street, Maumee Street, and Brown Street.

Figure 6-1
Functional Classification
of Streets

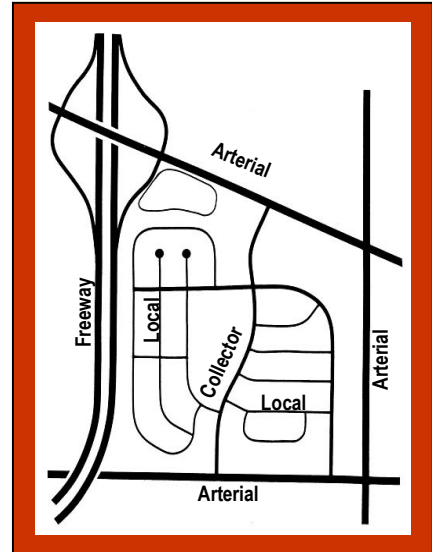
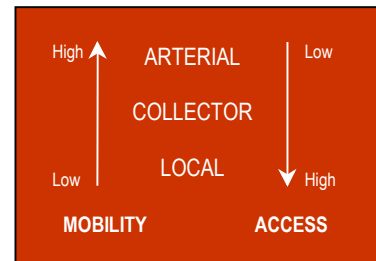


Figure 6-2
Mobility Vs. Access



LOCAL STREETS

Local streets serve the purpose of providing access to abutting land and consist of all facilities that do not belong to one of the higher systems. These streets make up a large percentage of total street mileage in urban areas, but they almost always carry a small portion of vehicle miles traveled. They offer the lowest level of mobility and may carry no through traffic. Examples of this class of roadway are residential subdivision streets and cul-de-sacs.

PROPOSED ROADWAY FUNCTIONAL CLASSIFICATIONS AND CROSS SECTIONS

The Thoroughfare Plan Map 6-4 indicates how the roadways in the City of Tecumseh are classified for future road function, and also includes planned right-of-way requirements. Relative to the existing functional classification of the City's roadways, no changes in designation are proposed. The existing facilities are thought to be adequate to handle anticipated growth.

A 100-foot right-of-way is anticipated for roads classified as arterials, with five lane sections up to 64 feet wide. Minor arterials are planned for 86-foot rights-of-way and a back-to-back width of 40 feet for three or four lanes. For collectors, two or three lane sections up to 36 feet wide are anticipated, with a right-of-way of 70 feet. Local streets are planned for a 66-foot right-of-way, with two lanes up to 28 feet wide. It should be noted that every road will not achieve the maximum cross-section, while others may exceed the cross section width where additional turn lanes are necessary or additional capacity is required and fits within the planning context of the area.

NON-MOTORIZED PEDESTRIAN FACILITIES

Sidewalks and pedestrian safety paths are pedestrian-oriented facilities that are an integral part of the City's transportation network. They are generally located within the street right-of-way. Sidewalks are most prevalent and justified at points of community development such as schools, local businesses, subdivisions, and industrial developments where pedestrian concentrations are high. Sidewalks are typically 4 feet in width and are located 1 foot from the edge of right-of-way. In order to discourage pedestrians from using the traffic lanes, it is preferable for sidewalks to have all-weather surfaces.

Due to the increasing use of the bicycle as a mode of transportation, bicycle facilities have become an important element in the highway design process. Existing road systems may be supplemented with facilities to provide for such traffic. Bike paths generally carry two-way bicycle traffic with a width of 8-10 feet. Two-way bike paths are common in recreation areas connecting different points of interest. When designing such exclusive bike paths, it is necessary that different design factors such as turning radii, grades and sight distance be considered. When located within the right-of-way, bike paths are typically set 1 foot from the edge of the right-of-way.

At the present time (2004), there is one existing pathway in the City, an 8-foot-wide sidewalk that runs along Occidental Highway between Chicago Boulevard and Brown Street.

As a result of resident input gained during the Community Vision Session, the objective of promoting the use of non-motorized facilities throughout the City was established. Strategies to achieve this objective include: connecting parks, schools, and local business with a pedestrian

and bicycle pathway system; connecting pathway systems to regional trails; promoting rails-to-trails pathways; and requiring sidewalks in new developments. At the Vision Session, residents voiced particularly strong support for the establishment of more bike paths.

The City of Tecumseh's Parks and Recreation Master Plan, which was adopted in 2001, has a similar objective of providing linked path systems for pedestrians and bicyclists. The Plan proposes a path system connecting the City's parks, neighborhoods, and local business, noting that while many of the City's neighborhoods have sidewalks, others still lack pedestrian connections. In particular, the Plan identified a number of proposed pathways, including: a pedestrian path through Indian Crossing Trails linking Herrick Park, Standish Dam, Evans Park, and Douglas W. Bird Kiwanis Memorial Park; a pedestrian and bicycle path system linking Clinton to Tecumseh; and a path system linking Tecumseh to Raisin Township.

The River Raisin Greenway Study, a regional plan that was completed in March 2001 and identifies a potential greenway system in the River Raisin watershed, also proposes linking Tecumseh to Raisin Township. This trailway connection would be created by extending an existing trail, the Kiwanis Trail, which runs along an abandoned rail corridor from the center of the City of Adrian through Raisin Township. In the River Raisin Greenway Study, the City of Tecumseh's interest in providing a path system within its own boundaries is also established. Trailway and Bike Path Plan Map 6-5 depicts generalized routes based on those planned in the Greenway Study.

TRUCK ROUTES

Chicago Boulevard (M 50) currently functions as the City's primary truck route and also as the "Main Street" of downtown Tecumseh. As shown on the Traffic Volumes Map 6-2, Chicago Boulevard has the highest traffic volume of any street in Tecumseh, with trucks comprising a considerable amount of the road's traffic. Trucks enter Tecumseh from the north, on Evans Street, and turn onto Chicago Boulevard, and also enter the City from the east on Chicago Boulevard. Truck traffic on this road negatively impacts retail establishments in the downtown, causing noise pollution, air pollution, destruction of the roadway, and damage to historic buildings. Additionally, rumbling trucks create a hostile atmosphere for pedestrians.

During the Community Vision Session held in November 2003, residents expressed strong support for keeping trucks out of downtown Tecumseh. Feedback gained at the Vision Session resulted in the objective of establishing an alternative truck route in order to protect the downtown area. As shown on Map 6-6, the City of Tecumseh has designated a truck route that bypasses downtown Tecumseh, extending south on Rogers Highway from Chicago Boulevard and west on Russell Road to the City limits (eventually connecting to M 52). As federally-funded highways and Class A roads, both Rogers Highway and Russell Road are capable of handling commercial truck traffic. In order to address truck traffic entering downtown from the north, another alternative truck route has been planned, extending east on Macon Road from Evans Street, then south on Billmyer Highway and west on Chicago Boulevard to Rogers Highway.

A number of strategies may be employed to encourage use of the Rogers Highway / Russell Road truck route. The City will need to coordinate with the Michigan Department of Transportation to seek the State's designation of the alternative route. An appropriate signage package should be

developed, diverting traffic off Chicago Boulevard and marking Rogers Highway and Russell Road as designated truck routes. The City may also wish to explore the possibility of an ordinance barring through commercial truck traffic from Chicago Boulevard and redirecting it to Rogers Highway and Russell Road. Measures required to enforce any proposed ordinance should be discussed. For the route diverting truck traffic off Evans Street, some of the aforementioned strategies may be appropriate. Additionally, coordination with the Lenawee County Road Commission may be necessary for roads extending into Tecumseh Township.

ACCESS MANAGEMENT RECOMMENDATIONS

Most streets provide two functions: 1) to move traffic and 2) to provide access to land uses that abut them. However, these functions can often conflict because each access point interrupts traffic movement as vehicles turn off and onto the roadway. In order to balance these two road functions, access management techniques should be used. The access management section describes ways in which the road network’s capacity can be maximized, by reducing the impact of development abutting the major road network.

The access management techniques described below primarily apply to more intensive, non-residential land uses. Access management is usually implemented through the site plan review process, and these techniques are suggested as guidelines in that process. Reference should also be made to applicable sections of the City’s Zoning Ordinance. Each case will require an individual analysis to determine the appropriate action given the characteristics of the site and use.

RESTRICTING THE NUMBER AND SPACING OF ACCESS POINTS

Limiting the number of driveways permitted for each land use can help preserve the traffic movement function of a roadway. Proposed and existing land uses should provide the minimum number of driveways needed to provide access to a development site. If additional driveways are proposed, additional street frontage for the subject site and appropriate spacing between existing driveways should be provided.

Even if only one access point is proposed, the most appropriate location should be selected to preserve the function of the roadway and, more importantly, to assure public safety. Driveways located too close together are safety hazards and they can negatively impact road capacity. Recommended spacing standards for non-residential driveways on the same and opposite sides of the roadway are provided in the following tables.

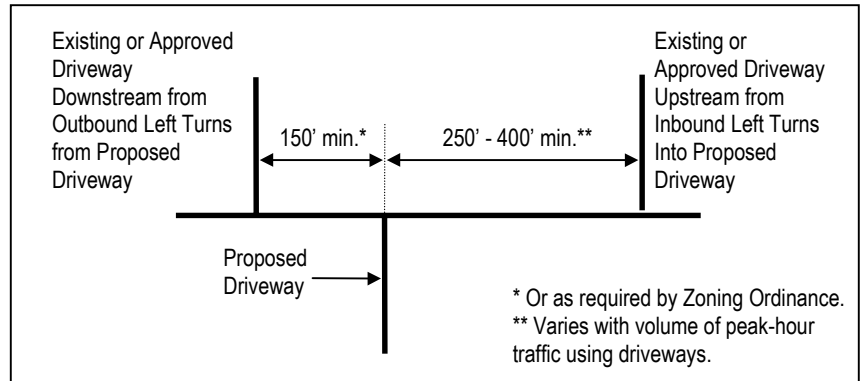
Table 6-2

Spacing Standards for Driveways on Same Side of Street

Speed Limit (Mph)	Minimum Driveway Spacing (Feet)*
25	135
30	155
35	180
40	215
45	260
50 or greater	310

* Center-to-center. Note: Greater separation between driveways and street intersections may be required.

Figure 6-3
Spacing Standards for Driveways on Opposite Side of Street

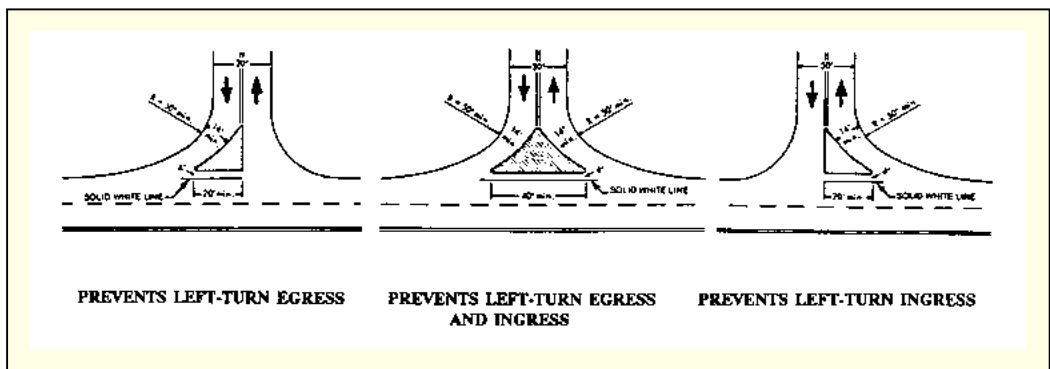


ACCESS / DRIVEWAY DESIGN

Another access management technique is assuring proper driveway and intersection design. Driveways should be designed with adequate width, turning radius, and depth to allow automobiles and large trucks to enter and exit a site safely and efficiently. A clear-vision area at the corners of all driveways and intersections is also needed for safe driver visibility.

In addition, uses that generate high volumes of traffic may warrant the construction of deceleration and acceleration lanes adjacent to driveways and intersections. Left-turn passing lanes or center left-turn lanes may also be necessary. Such improvements are often identified by the completion of traffic impact studies. In general, traffic impact studies are recommended whenever a proposed land use will generate more than 750 vehicle trips per day and/or more than 100 vehicle trips in one direction during the morning (e.g., 7 a.m. - 9 a.m.) or afternoon (4 p.m. - 6 p.m.) peak hour.

Finally, restricting turning movements at a driveway or intersection is often warranted due to traffic volumes or poor spacing of proposed access points relative to existing driveways and/or intersections. For example, when an existing driveway is too close to an intersection, it is possible to improve the access and safety by restricting turning movements to right turns in and out of a proposed or existing development site. The graphic below illustrates ways in which driveways can be “channelized” to restrict turning movements.



ENCOURAGING SHARED ACCESS

Providing shared access to a site reduces the number of access points, preserves the capacity of the road, and can even help to maintain the character of the community. Shared access can be achieved through a variety of techniques including shared driveways, frontage roads, and internal connections between sites. As discussed above, access management is critical for non-residential land uses because of their intensive nature and tendency to demand a higher number of access points. The graphics below illustrate ways in which residential and non-residential uses can utilize access management techniques.

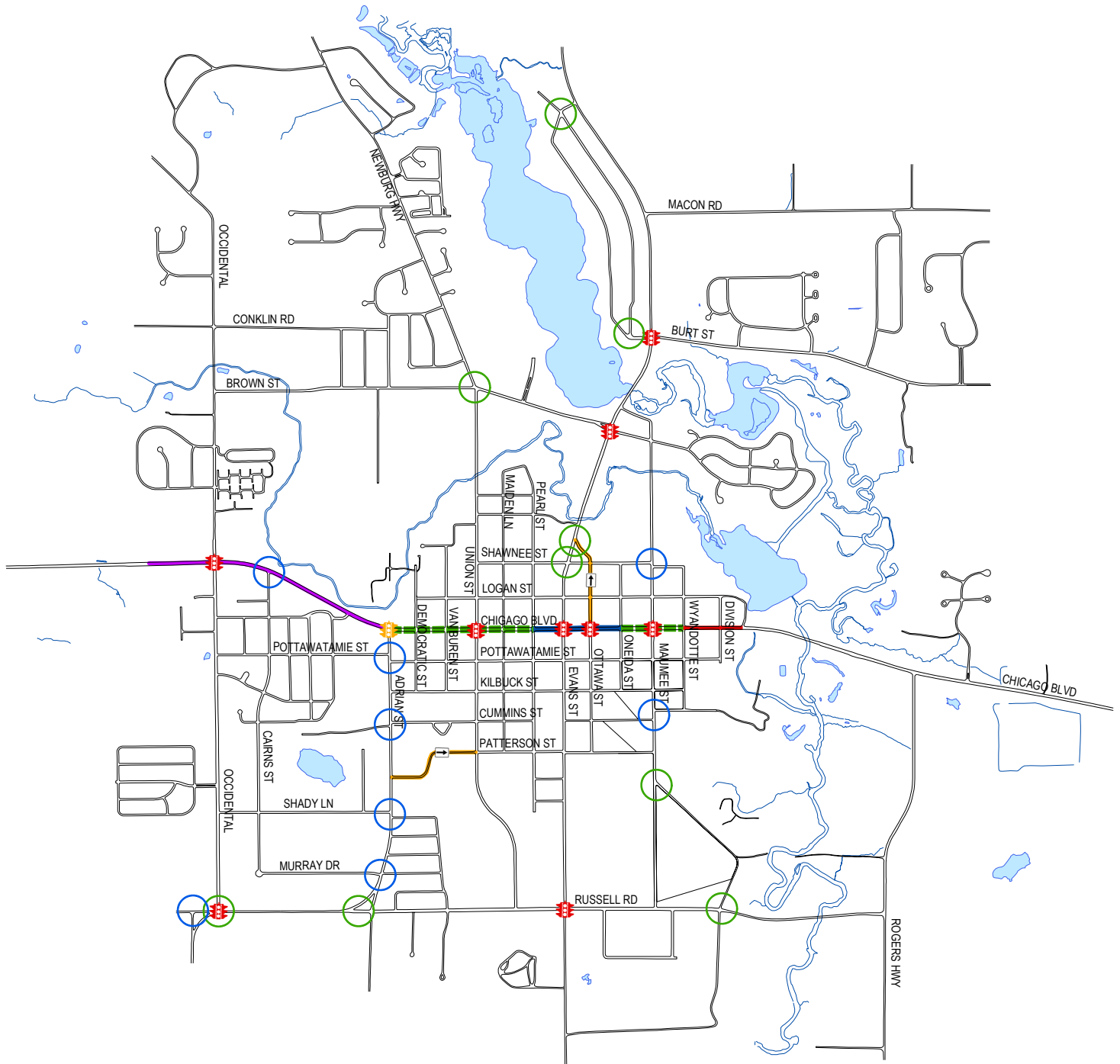
Common access problem created by individual driveways serving homes or businesses on a major roadway (top left). Shared access driveways and frontage roads preserve capacity of the roadway, views from the road, and can provide a buffer area for houses from traffic noise (top right).










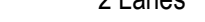
Shared access for a number of non-residential uses preserves the road capacity, which is especially important near intersections (left). Shared parking at the rear of the buildings also helps preserve the aesthetic appearance and character of the community. If shared access drives are not feasible, internal service roads and/or internal parking lot connections between uses should be provided to preserve roadway capacity (above).

THOROUGHFARE PLAN ACTION ITEMS

The following key recommendations were derived from an examination of the Goals and Objectives, existing transportation conditions, and projected build out analysis for the City:

- Develop site plan review standards that encourage service drives and combined parking and drives
- Adopt and implement good access management practices to reduce the number of driveways for individual sites
- Improve problem intersections by means of additional turn lanes, revised traffic controls, and lighting where appropriate
- Request that MDOT study the M 50 / Occidental Highway intersection to determine if improvements can be made to reduce traffic crashes.
- Work cooperatively with MDOT to provide an efficient and safe system of arterial roadways
- Encourage use of alternative truck routes to protect the downtown area from pollution, noise, road damage, and vibration
- Promote the use of non-motorized facilities
- Consult the Goals and Objectives chapter on a regular basis to ensure that Planning Commission decisions are in keeping with the intent of the Master Plan.



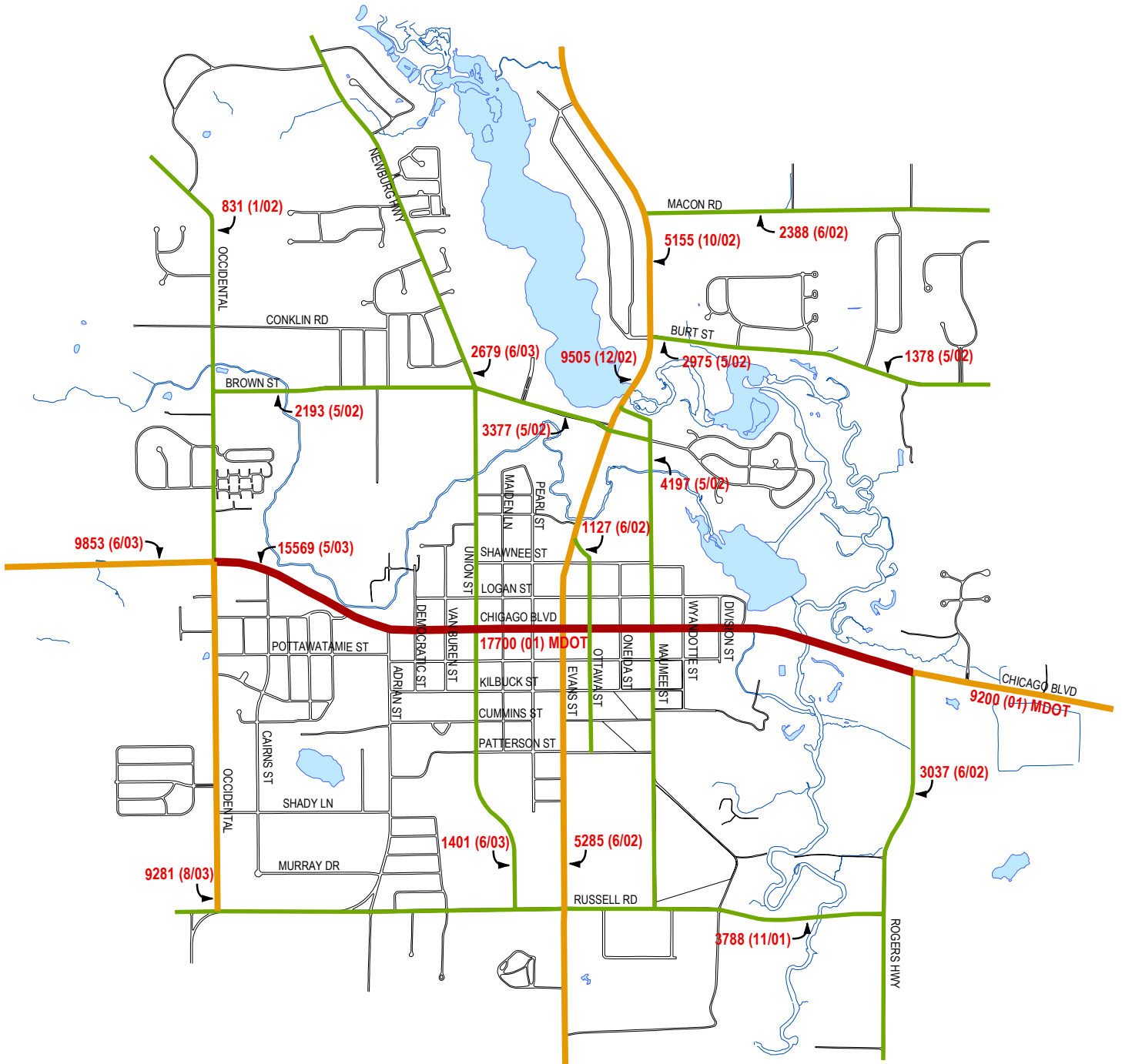
-  Traffic Signal
-  Traffic Signal (Flashing Mode)
-  Skewed Intersection
-  Offset Intersection
-  4 Lane Boulevard
-  4 Lanes
-  3 Lanes (Center Turn Lane)
-  3 Lanes (2 Westbound Lanes)
-  2 Lanes
-  One Way

**EXISTING
ROAD CONDITIONS
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP 6-1**

500
Feet



**BIRCHLER ARROYO
ASSOCIATES, INC.**



DAILY TRAFFIC (DT)

- 0 - 5,000
- 5,000 - 9,999
- 10,000 +

↑ 3788 (11/01)
↑
DT (Month/Year)

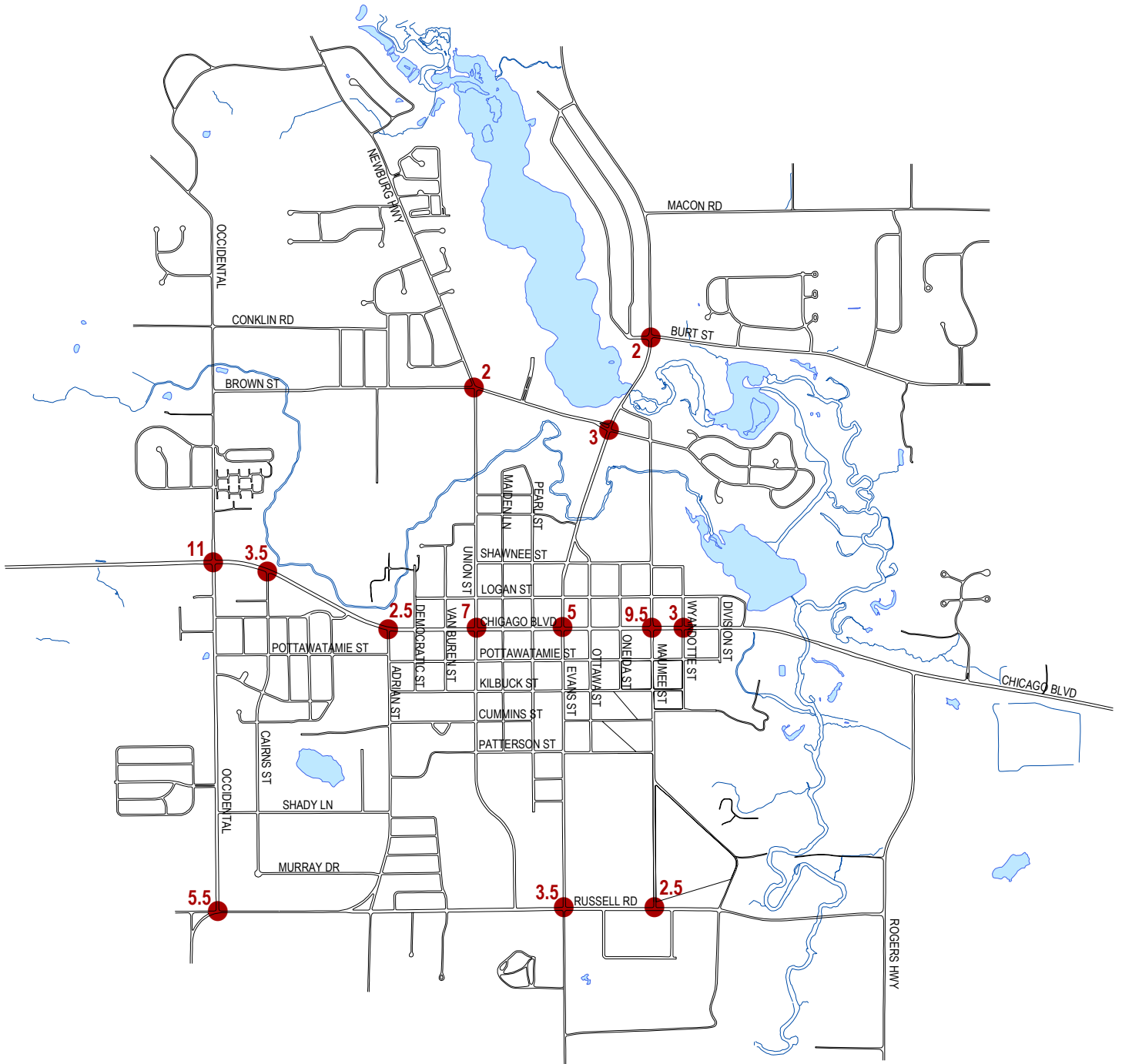
**2001-2003
TRAFFIC VOLUMES
ON MAJOR STREETS
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN**

MAP 6-2

500
□ Feet

Sources: CITY OF TECUMSEH DEPARTMENT OF PUBLIC WORKS;
MICHIGAN DEPARTMENT OF TRANSPORTATION (MDOT)





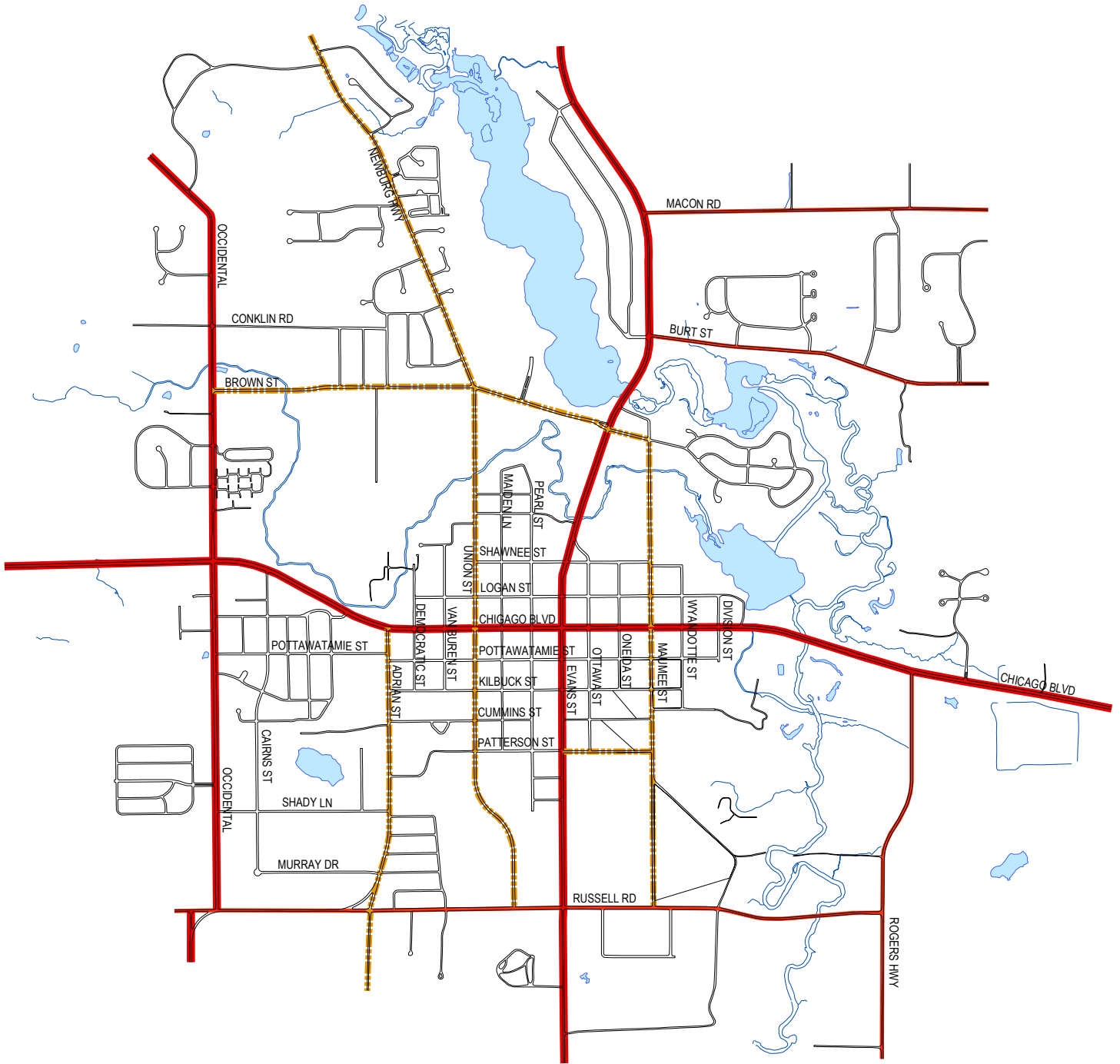
- Intersections with 4 or More Crashes in 2-Year Time Period (2001-2002)
- 7 Average Annual Number of Crashes

AVERAGE ANNUAL CRASHES
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN

MAP 6-3
 500
 Feet

Source: REGION II PLANNING COMMISSION





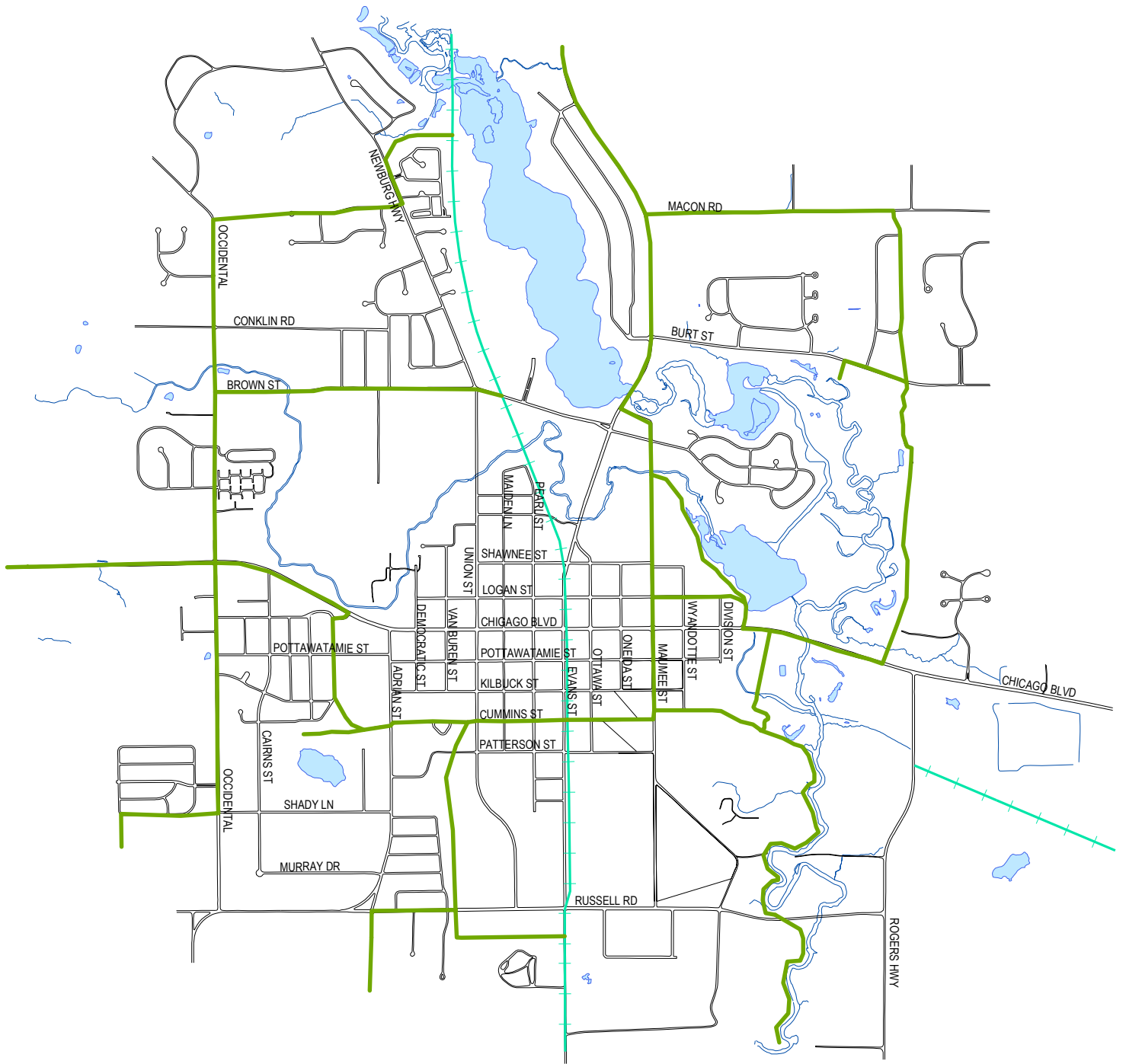
- Arterial (100' ROW)
- Minor Arterial (86' ROW)
- - - Collector Street (70' ROW)
- Local Street (66' ROW)

**THOROUGHFARE
PLAN**
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP 6-4

500
Feet



**BIRCHLER ARROYO
ASSOCIATES, INC.**



- Paved Bike Path
- Potential Rails-to-Trails

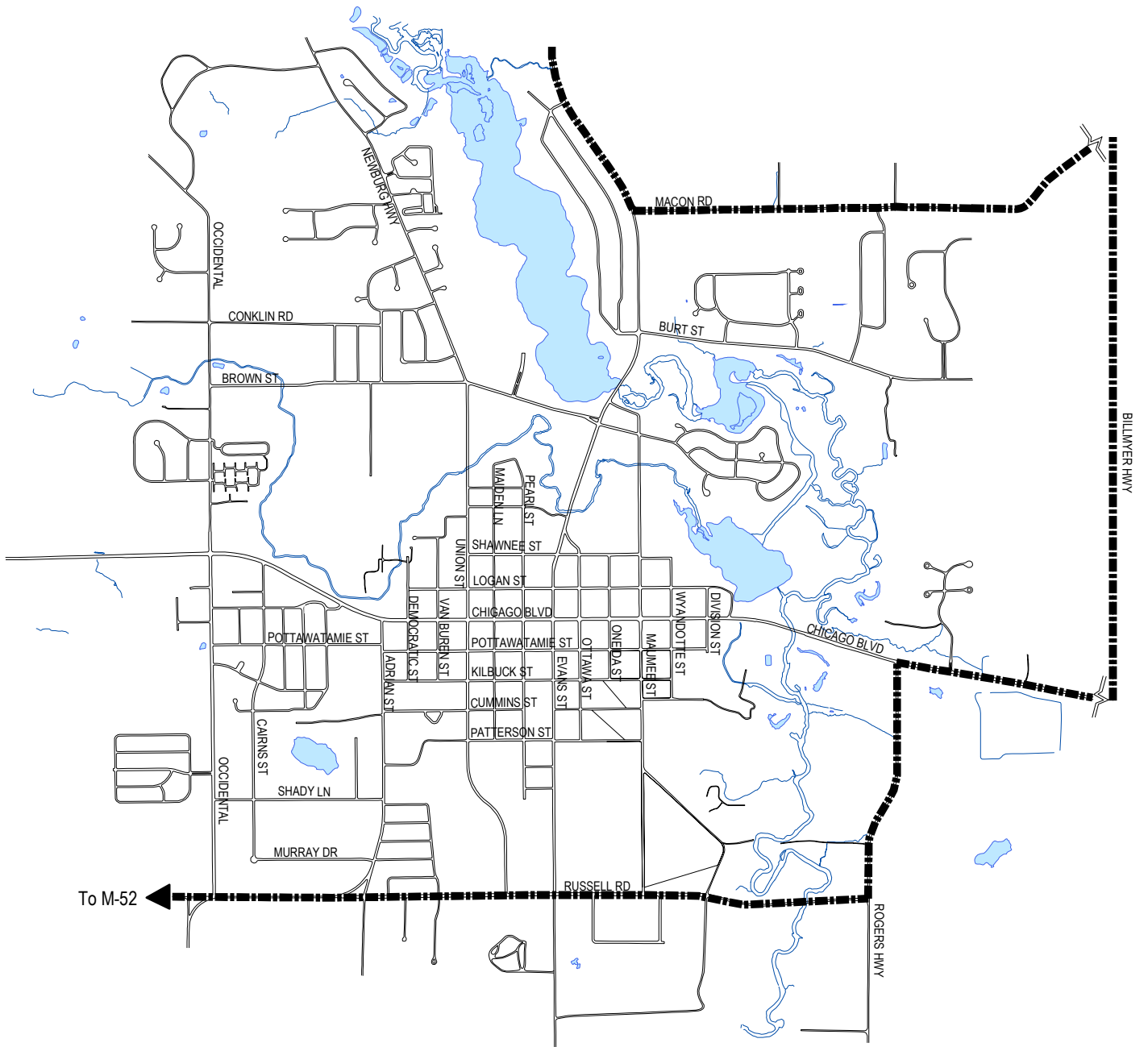
**TRAILWAY AND
BIKE PATH PLAN***
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP 6-5

500
□ Feet



**BIRCHLER ARROYO
ASSOCIATES, INC.**

*This plan depicts generalized routes based on the River Raisin Greenway Study.



To M-52

▬▬▬ Truck Route

TRUCK ROUTES
 CITY OF TECUMSEH
 LENAWEE COUNTY, MICHIGAN
 MAP 6-6

500
 Feet



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7

HOUSING PLAN



The Housing Plan provides analysis and details about how housing will be enhanced, developed, and redeveloped in the City of Tecumseh's future.

The City of Tecumseh is fundamentally defined by the quality and diversity of its housing stock and neighborhoods. The Housing Plan component of the Comprehensive Plan refines the different residential land use designations identified in the City of Tecumseh Land Use Plan. It presents specific recommendations for a wide range of residential housing options, including single and multiple family uses and attached and detached forms of development.

This plan component also provides an analysis of housing trends in the City and adjacent communities to assist the City in adequately planning for the housing needs of its current and future residents.

Development density ranges were applied to all of the City's planned residential areas to develop a forecast of future population including a potential housing unit count. The Housing Plan illustrates the preferred residential development pattern for the City of Tecumseh, supported by the general land use goals and objectives.

HOUSING TRENDS

The Existing Land Use survey completed in September, 2003 indicated that currently approximately 37 percent of all land within the City is developed as residential. As shown in the table below, the City of Tecumseh's total housing units increased by around twenty percent between 1990 and 2000. This is the same growth rate the City

experienced during the 1970s, although the 1980s saw a relatively modest increase in housing units.

Table 7-1
Housing Units
City of Tecumseh
1970-2000

	Total Housing Units 1970	Total Housing Units 1980	Percent Change, 1970-1980	Total Housing Units 1990	Percent Change, 1980-1990	Total Housing Units 2000	Percent Change, 1990-2000
City of Tecumseh	2,270	2,723	20.0%	2,999	10.1%	3,598	20.0%

The age of the City’s housing stock adds more information to the story. Nineteen percent of all the housing units in the City of Tecumseh were built prior to World War II (before 1940). Another 31% were built during the postwar building boom,¹ but only 28% were built during the thirty years between 1960 and 1990. After three decades of modest growth, the 1990s were another building boom decade for the City of Tecumseh: a full twenty percent of the City’s dwelling units were built after 1990. Taking into consideration area-wide housing trends, this pace of building is likely to continue for at least the next several years. Unlike neighboring townships also experiencing rapid growth, however, the City of Tecumseh has a limited amount of vacant land on which homes can be built. Eventually, virtually all City land will be developed or permanently preserved.

Table 7-2
Housing Trends
City of Tecumseh and Nearby Communities
1990-2000

Community	Total Housing Units 1990	Total Housing Units 2000	Percent Change, 1990-2000	Single Family Units, 1990	Single Family Units, 2000	Percent Change, 1990-2000	Multiple Family Units, 1990	Multiple Family Units, 2000	Percent Change, 1990-2000	Mobile Home Units, 1990	Mobile Home Units, 2000	Percent Change, 1990-2000
Adrian	7,842	8,569	9.3%	4,561	4,736	3.8%	2,658	3,212	20.8%	623	621	-0.3%
Britton	250	271	8.4%	194	225	16.0%	51	46	-9.8%	5	0	-100.0%
Clinton	960	969	0.9%	601	634	5.5%	169	167	-1.2%	190	168	-11.6%
Deerfield	342	369	7.9%	289	321	11.1%	44	48	9.1%	9	0	-100.0%
Manchester	675	933	38.2%	509	579	13.8%	152	244	60.5%	14	110	685.7%
Saline	2,588	3,215	24.2%	1,707	2,339	37.0%	775	789	1.8%	106	87	-17.9%
Tecumseh	2,999	3,598	20.0%	2,121	2,551	20.3%	769	950	23.5%	109	97	-11.0%
Lenawee County	35,104	39,769	13.3%	27,319	30,808	12.8%	4,863	5,965	22.7%	2,922	2,991	2.4%

Sources: United States Census Bureau, 1990 and 2000 Census.

¹ This number includes housing units that were built 1940-1959, but in general, the vast majority of building activity took place after the war was over.

Table 7-3 below provides a look at the composition of housing types available in the City by percentage of total units. Multiple family developments make up roughly 26 percent of the total housing units available in the City. This percentage of multiple family units is on the higher end of the nearby villages and cities. All the cities and villages have higher percentages of multiple family residential uses than the County as a whole, since the County includes many rural areas with mostly large lot development.

Table 7-3
Percentage of Housing Types
City of Tecumseh and Nearby Communities, 2000

Community	Single Family	Multiple Family	Mobile Home
Adrian	55.3%	37.5%	7.2%
Britton	83.0%	17.0%	0.0%
Clinton	65.4%	17.2%	17.3%
Deerfield	87.0%	13.0%	0.0%
Manchester	62.1%	26.2%	11.8%
Saline	72.8%	24.5%	2.7%
Tecumseh	70.9%	26.4%	2.7%
Lenawee County	77.5%	15.0%	7.5%

Source: United States Census Bureau, 2000 Census.

Mobile homes (manufactured housing) make up 2.7 percent of all housing units in the City. This percentage is the same as Saline, and higher than Deerfield or Britton (each has no mobile homes), but lower than the other cities and villages in the area. This percentage is also lower than the County as a whole. The City of Tecumseh has an average percentage of single family homes among nearby cities and villages.

Table 6-3 below shows that the City of Tecumseh has a higher median housing value than all the of the nearby cities and villages except Saline and Manchester. These two communities are closer to Ann Arbor, which may drive up demand from people who want to live in a smaller town and are willing to commute back and forth to work. Median housing values in the Township increased approximately 90 percent from \$62,900 in 1990 to \$119,300 in 2000.

Table 7-4
Median Housing Value
City of Tecumseh and Nearby Communities, 1990-2000

Community	Median Housing Value, 1990	Median Housing Value, 2000	Percent Change, 1990-2000
Adrian	\$44,200	\$86,100	94.8%
Britton	\$50,400	\$94,800	88.1%
Clinton	\$64,600	\$117,300	81.6%
Deerfield	\$47,200	\$107,300	127.3%
Manchester	\$72,600	\$140,900	94.1%
Saline	\$95,300	\$169,200	77.5%
Tecumseh	\$62,900	\$119,300	89.7%
Lenawee County	\$54,000	\$109,500	102.8%

Sources: United States Census Bureau, 1990 and 2000 Census.

According to the 2000 US Census, at \$557 a month the median rent in the City of Tecumseh is about average for nearby cities and villages but above the County average. As Table 7-5 below illustrates, the Village of Manchester and the City of Saline command higher rents than does Tecumseh, and the Village of Clinton’s rents are about the same. In contrast to the relatively reasonable rates among most of the communities surveyed, median rent in the City of Ann Arbor is \$742 per month.

Table 7-5
Median Rent
City of Tecumseh and Nearby Communities
1990-2000

Community	Median Rent, 1990	Median Rent, 2000	Percent Change, 1990-2000
Adrian	\$325	\$504	55.1%
Britton	\$289	\$493	70.6%
Clinton	\$318	\$556	74.8%
Deerfield	\$313	\$453	44.7%
Manchester	\$363	\$600	65.3%
Saline	\$452	\$601	33.0%
Tecumseh	\$356	\$557	56.5%
Lenawee County	\$316	\$517	63.6%

Sources: United States Census Bureau, 1990 and 2000 Census.

RESIDENTIAL LAND USE PLAN

Map 7-1 depicts the entire land area planned for residential development within the City. Consistent with existing development trends, single family development remains the principal residential development form provided for in the City’s Residential Plan. A wide range of single and multiple family residential categories is provided to accommodate a variety of lifestyles and living arrangements. The residential land use categories are general and encompass a range of densities. The Residential Density Map, Map 7-2, specifies the recommended number of dwelling units per acre within each planned area. Planned residential land use categories and recommended densities are as follows.

Low Density Single Family Residential


This land use category’s recommended density is 2.5–4.0 units per acre. Most of the large undeveloped parcels in the City are planned for Low Density Single Family Residential, in keeping with the City’s primarily residential character. This includes remaining undeveloped areas north of Brown Street, along Macon Road near the airport, and on either side of Rogers Highway in the southern part of the City (including the Bonner Farm property). An estimated 1238 acres are categorized as Single Family Residential.

 In Town Residential


This predominantly single-family residential category is intended to include the existing older and historic homes surrounding the Central Business District. These homes are characterized by nineteenth-century and early twentieth-century American vernacular architecture, several examples of which are shown below. They tend to have smaller front yards and detached garages behind the house; many have porches. The overall density of this district is between 3.5 and 4 units per acre, although actual lot sizes vary significantly. Some two-family uses may be appropriate for this district, if the existing character and design of the home is not compromised. Any redevelopment or infill housing in this area must be particularly considered for sensitivity to and compatibility with the existing fabric of the neighborhood. Around 186 acres are included in this category.



Distinctive home styles in the In Town Residential District (clockwise from top left): Craftsman Bungalow, Italianate, Cape Cod, Gable-Front-and-Wing Greek Revival Farmhouse, Queen Anne.

 Two Family Residential


Two family residential uses are planned along Waskin Drive between Russell Road and Tecumseh Acres Elementary School, and along Mill Highway south of Russell Road. These uses should be developed or redeveloped at 7.0 units per acre. They account for about 18 acres of land planned within the City.

 Low Density Multiple Family Residential

This multiple family land use designation provides for developments up to 10 units per acre. Areas of Low Density Multiple Family Residential are planned along Union Street at Russell Road, at Brown Street and Occidental, and along Occidental between the Commercial district and South Occidental Park. This category could appropriately include senior housing facilities. Roughly 180 acres of land throughout the City are designated for Low Density Multiple Family.



These brownstones in Dearborn's West Village provide another option for in-town living.

 Townhouse/ Attached Single Family

Several areas of Townhouse/Attached Single Family, totaling about 46 acres, have been designated on the

Land Use Plan Map. They are located along Chicago Boulevard east of Maumee, south of the intersection of Division Street and East Chicago Boulevard, and along North Evans street between the river and Brown Street. This category is intended to accommodate a density of 9.0 units per acre. Townhouse developments may appeal to residents who wish to live close to downtown but may need more space than an upper-story apartment or condo can provide.



Flexible Development

This land use category is designed to provide for flexible use development in two specific areas of the City. It is the intention of the Planning Commission that these areas be developed as a Planned Unit Development (PUD) and Planned Residential Development (PRD). They should incorporate usable open spaces, walkways, pathways, and traditional architectural styles, in keeping with the major architectural themes within the City.



Canton's Cherry Hill Village provides an example of a development with a mix of multiple (shown) and single family uses.

The Flexible Development district at the western edge of the City should be developed as a Planned Unit Development (PUD). It could include mixed uses or designated individual uses limited to residential, office and/or commercial. It is anticipated that development of the three parcels identified for this land use would include a mixture of uses, but predominantly be developed as housing. A portion of the frontage along West Chicago Boulevard could be developed as a mixture of commercial, office, and upper-story multiple-family residential in the context of the PUD, where the City could achieve a recognizable and substantial benefit. Behind these uses, low density multiple-family or attached single-family uses would be appropriate, decreasing in density to low density single family uses toward the back of both sides of the development. If these areas are not developed as a PUD, a variety of residential densities could still be accommodated, but commercial and office uses would not be envisioned.

The Flexible Development district in the northeast part of the City should be developed as a PRD. It would be appropriate for this area to be developed as a primarily residential project of varying residential densities, similar to the development directly to the southeast that combines multiple family and detached single family uses. The overall density for each of these Flexible Development areas should be 4.8 units per acre. Flexible Development is designated for roughly 247 acres within the City.



Mobile Home Park



The Land Use Plan Map shows a 10-acre potential expansion of mobile home uses adjacent to the Newburg Meadows Mobile Home Park and Red Mill Pond. A well-maintained and attractive mobile home park can be a great asset to the community, as it provides affordable home ownership opportunities. Any mobile home park expansion should be developed at no higher density than 5.0 units per acre, which is typical for a modern manufactured housing community.



Downtown Mixed Use Housing

This housing category refers primarily to second and third-floor apartments and condominiums over first-floor retail or office in the Central Business District. These units are accessed by internal or external stairs (shown at right) or an elevator. This plan encourages residential use of the second and third floors of downtown buildings, since a larger population downtown contributes to the viability of the businesses there and the general vitality of the district. This type of housing provides yet another unique option for people wishing to locate or relocate within the City.



SENIOR HOUSING

Vision Session participants and the Planning Commission identified the expansion of housing options for seniors as a top priority for the City.

Options

Housing designed for and marketed toward seniors can occur within many different land use categories. A site condominium of detached single family homes could be designed with ranch-style floorplans, wheelchair accessible bathrooms and kitchens, and a condominium association that would be responsible for outside maintenance. Attached dwellings could exhibit the same features. These types of developments could occur in either the Flexible Development areas, Two-Family, Multiple Family, or Townhouse/Attached Single Family categories. The areas designated for Townhouse/Attached Single Family may prove to be particularly important because they are located within a short distance of the stores and services of Downtown Tecumseh and the Hospital and medical offices. In addition, a progressive senior housing center with options ranging from completely independent attached single family homes to assisted living apartments to 24-hour nursing care would be appropriate for a Multiple Family district.

Next Steps

- A marketing effort aimed at developers that explains the City's desire for senior housing and outlines how the City could streamline the development process for a suitable project could help get development started. It is important to remember that senior-focused housing may occur in any residential land use category.
- In order to encourage new senior housing options, the City could extend an offer to developers to rezone an area designated in the Land Use Plan to Multiple Family if the developer is willing to enter into a development agreement to build the type of facilities the City desires.
- Creating a Townhouse/Attached Single Family Zoning District and rezoning property to this district could encourage the construction of housing suitable for seniors who wish to live close to downtown.
- The City should identify capital facilities and infrastructure improvements that may be necessary to serve areas targeted for senior housing and make these high priorities in the Capital Improvements program.

INFILL HOUSING RECOMMENDATIONS

The term *infill development* refers to the development or redevelopment of underutilized land. In urban centers it can take the form of mixed-use downtown buildings; in suburban centers infill development can occur on what was once an outdated shopping center and now is a lively “downtown.” For a predominately residential city like Tecumseh, opportunities for infill housing are likely to occur on a small scale when there are changes in land use in already-developed areas.

ISSUES

Communities throughout the nation view infill development differently. Some areas may see residential redevelopment as an opportunity for their community to improve and prosper while others perceive it as a potential burden on roads and schools and a loss of privacy for adjoining residents.

Deteriorated urban centers may see infill development as a way to invigorate declining commercial or residential areas by infusing new tax dollars into a community, bringing more people to live and work in the downtown where infrastructure and employment opportunities already exist, or assist with the clean up and mitigation of brownfield sites. Suburban communities may see infill development as a tool to manage population growth within already developed areas while preserving farmland, open space, and natural features in less populated areas. Conversely, communities may perceive negative effects from infill development such as increased density, traffic, and potential loss of environmentally sensitive areas. Whatever the perception, infill development can be a useful tool for many communities faced with sites that due to location, dimensional, or development challenges are in need of creative tools for development or redevelopment.

INFILL IN TECUMSEH

In the case of Tecumseh, infill development could arise out of several sets of circumstances. The owners of a house on a double lot may decide to split the lot and sell off the vacant half. A house that has fallen into major disrepair may be razed or thoroughly redeveloped. A non-conforming commercial use may go out of business and be torn down to make way for a new house. Several underutilized parcels could be combined for a larger development. In all of these cases, one or more new houses will be built in the midst of an existing residential district. The character of this new development is particularly important in a city like Tecumseh, which has such architecturally significant neighborhoods.

INFILL HOUSING GUIDELINES

As land values in the City continue to rise, underutilized parcels may increasingly be targeted for residential redevelopment. This potential requires that the City of Tecumseh establish a set of basic guidelines to help direct public policy decisions. To further the objectives of the City, these guidelines along with a clearly defined description of the City’s definition of what constitutes residential infill / redevelopment, should be incorporated as a section of the City Zoning Ordinance.



This new house in Cherry Hill Village incorporates details that would make it fit into an older neighborhood.

1. Residential Infill / Redevelopment shall be consistent with the predominant character of the existing neighborhood and shall be built in an architectural style that blends in with or complements existing houses on the street.
2. Residential Infill / Redevelopment proposals shall be consistent with the use and density recommendations of the City's Master Plan. To that end, all requests for rezoning will be measured against the adopted Master Plan.
3. Residential Infill / Redevelopment shall not require public expenditure for infrastructure improvements. The cost of all infrastructure upgrades shall be borne by the development sponsor.
4. Residential Infill / Redevelopment shall not receive density credit for unbuildable, natural features including regulated wetlands, floodplains, and fragile, steep slopes in excess of 20 percent.

FUTURE HOUSING ESTIMATES

A general analysis of the City's future housing capacity is useful in ensuring that the Land Use Plan furthers the Planning Commission's goals for the City. This analysis allows the City to closely examine existing and potential residential development in order to plan for the future residential needs of the community as a whole.

Table 7-6 below provides estimates of potential residential development by type. Planned units were calculated based on the net unplatted land area available for development multiplied by the planned density shown on the Residential Density Map 6-2. Net area excludes wetlands and ten (10) percent of the land area to account for site characteristics and potential parcel and lot variations that could present limitations for development. An additional fifteen (15) percent of the unplatted land area designated for single-family uses was excluded to account for internal roads.

Table 7-6
**Future Housing Buildout Estimates
City of Tecumseh**

	Single Family	Multiple Family	Mobile Home Park	Total
2003 Total ¹	2,708	1,044	130	3,882
Additional Units per Density Map ^{2,3}	1,835	395	40	2,270
Buildout Total	4,507	1,439	170	6,152

¹ 2003 Total Units include projects built or approved since the 2000 Census count, as well as projects for which at least a conceptual plan has been filed.

²Planned units were calculated based on the unplatted land area available for development multiplied by the planned density shown on the Residential Density Map 5-2. Net area excludes wetlands and ten (10) percent of the land area to account for site characteristics and potential parcel and lot variations that could present limitations for development.

³ An additional fifteen (15) percent of the unplatted land area designated for single-family uses was excluded to account for internal roads.

The total planned residential capacity including existing, pending and potential development is estimated at 6,152 units. The total dwelling unit capacity is an estimate based on realistic assumptions and available data. It is intended as a planning tool and may be high since some of the land currently planned for residential development may remain as single-family large lot uses, or be used for parks or schools.

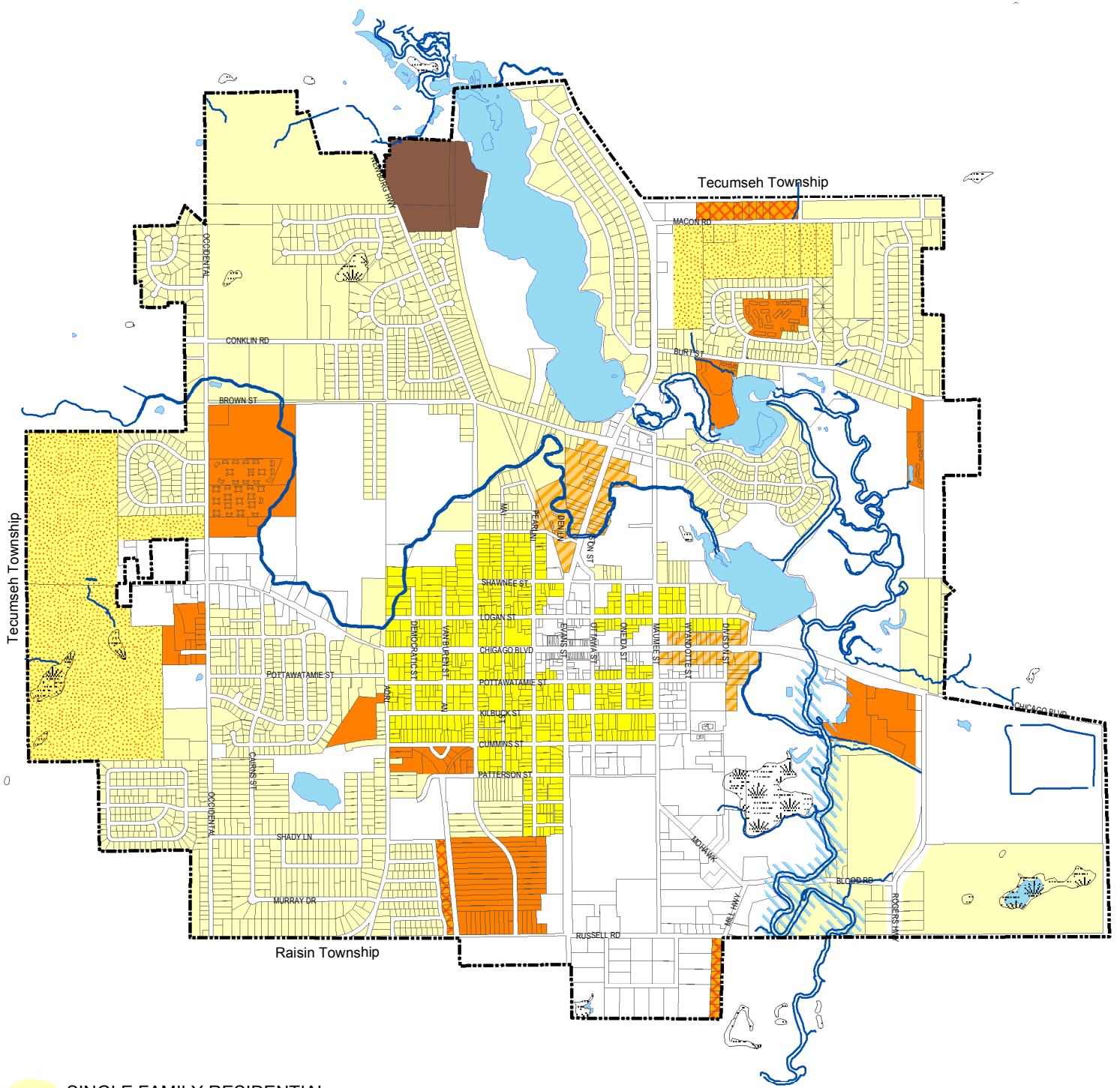
Build-out population for the City is estimated at 14,888 residents. This estimate was calculated using the planned dwelling unit capacity of 6,152 units multiplied by the City of Tecumseh's average household size of 2.42, as reported in the 2000 US Census. This figure is a rough estimate and may be overstated if the trend in decreasing household sizes continues. For instance, applying an average household size of 2.3 persons to the planned dwelling unit capacity results in a forecasted build out population of 14,150 residents.

The build out analysis reveals the potential for significant increase in the City's population. Indeed, the estimated build out population of 14,888 residents represents a 74% percent increase in population from the 8,574 population figure in the 2000 US Census. The residential unit capacity analysis and population estimates are forecasts intended for long-range planning purposes. Many factors beyond the availability of vacant land zoned for residential development will determine the ultimate population growth and residential development capacity of the City.

HOUSING PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing conditions, Goals and Objectives, housing trends, and projected build out analysis for the City:

- Provide for an appropriate mix of housing types to meet the needs and lifestyles of City residents, including all categories described in this chapter.**
- Encourage the maintenance and rehabilitation of existing housing stock by assisting residents with grants and other funding.**
- Rezone land as appropriate to eventually achieve the arrangement shown on the Land Use Plan Map.**
- For new development, encourage high-quality architecture that makes the dwelling the visual focus of the structure from the road and obscures accessory uses such as garages.**
- For infill and redevelopment, ensure that new structures enhance their neighborhoods rather than detracting from them.**
- Encourage the development of new Senior Housing options.**
- Consult the Goals and Objectives chapter on a regular basis to ensure that Planning Commission decisions are in keeping with the intent of the Master Plan.**



-  SINGLE FAMILY RESIDENTIAL
-  IN TOWN RESIDENTIAL
-  TWO FAMILY
-  MULTIPLE FAMILY RESIDENTIAL
-  MOBILE HOME PARK
-  TOWNHOUSE/ATTACHED SINGLE FAMILY
-  FLEXIBLE DEVELOPMENT
-  WETLAND
-  WATERWAY PRESERVATION OVERLAY DISTRICT

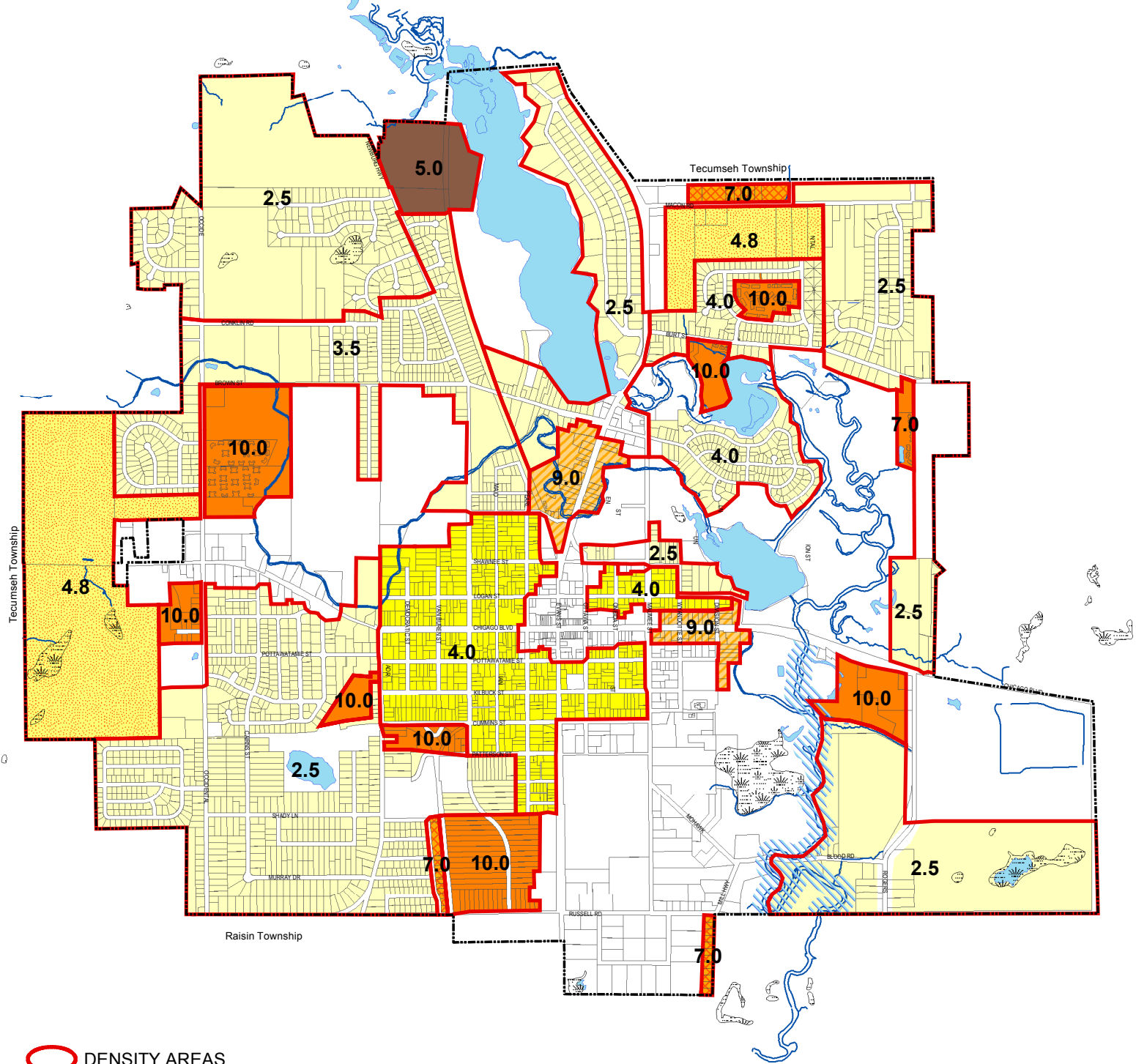
RESIDENTIAL PLAN
CITY OF TECUMSEH
 LENAWEЕ COUNTY, MICHIGAN
 MAP 7-1

500
 Feet



BIRCHLER ARROYO
 ASSOCIATES, INC.

NOTES:
 1. This map is intended to show generalized land use and is not intended to indicate the precise size, shape, or dimension of areas. These recommendations have a long-range planning horizon and do not necessarily imply that short-term rezoning decisions are appropriate.
 2. The City of Tecumseh Comprehensive Plan includes the Land Use Plan Map and all text, maps, charts, tables, and other graphics in the full Comprehensive Plan report.
 3. See the Residential Density Plan map for more specific residential density recommendations.



- DENSITY AREAS
- SINGLE FAMILY RESIDENTIAL
- IN TOWN RESIDENTIAL
- TWO FAMILY
- MULTIPLE FAMILY RESIDENTIAL
- MOBILE HOME PARK
- TOWNHOUSE/ATTACHED SINGLE FAMILY
- FLEXIBLE DEVELOPMENT
- WETLAND
- WATERWAY PRESERVATION OVERLAY DISTRICT

**RESIDENTIAL DENSITY
PLAN
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP 7-2**

500
□ Feet



**BIRCHLER ARROYO
ASSOCIATES, INC.**

NOTES:

1. This Density Map is intended to show the maximum residential density possible under conventional development techniques. It is not intended to represent a guarantee of number of units for any site; rather, it is to be used as a general guide for residential rezoning decisions.
2. The City of Tecumseh Comprehensive Plan includes the Land Use Plan Map and all text, maps, charts, tables, and other graphics in the full Comprehensive Plan report.

8

ECONOMIC DEVELOPMENT PLAN

The Economic Development Plan provides recommendations for strengthening the City's commercial areas.



Future development in the City of Tecumseh will be dependent on a number of factors, some of which include market demand, the carrying capacity of vacant land, property availability for redevelopment, government policies, and the needs of the resident population, to name a few. The Economic Development Plan discusses the benefits of non-residential development and where development opportunities still exist that would enhance the City's commercial development.

Non-residential development provides a community with a number of benefits including:

- ❖ Tax revenue for public services
- ❖ Employment for residents
- ❖ Goods and services for consumers
- ❖ Economic development catalyst for support services.

To further explore these benefits and evaluate how the City can assist with strengthening its commercial areas, this chapter will provide two analyses. The first will look at the fiscal benefits to the community from non-residential development (Tax Base Analysis). This analysis is designed to provide the City with information about different land uses and their effect on tax base, revenues, and expenditures. The Tax Base Analysis provides comparisons of the City of Tecumseh's tax base with Lenawee County and other communities near Tecumseh. There is also a general discussion of the cost of services for different land uses.

The second part of the Economic Development Plan will evaluate the commercial areas of the

community and the opportunities that exist. The analysis will provide recommendations on how these areas can be strengthened through physical enhancement, economic development through business recruitment and development and “niche” building.

TAX BASE ANALYSIS

According to the State Tax Commission, the City of Tecumseh has a 2003 combined State Equalized Value of \$278,018,800. As shown in Figure 8-1, the City of Tecumseh ranks third (out of seven) among nearby cities and villages in terms of taxable value per capita with \$32,426 per capita. This is comparable to the per capita value for Lenawee County, which is \$34,549. Compared with other cities and villages, the City of Tecumseh appears adequately positioned to meet the demands of its current population. The challenge will be to maintain this fiscal capacity as the City grows.

Figure 8-1
Per Capita SEV, 2003
City of Tecumseh and Nearby Communities

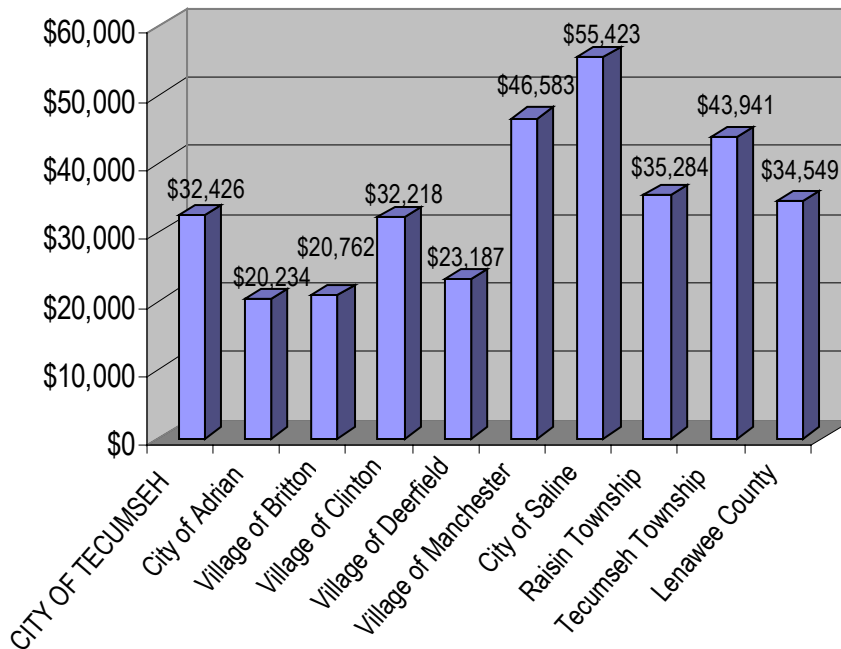


Figure 8-2 shows that nearly 70 percent of the City’s taxable value comes from residential uses and 22 percent from commercial and industrial uses. It should be noted that since multiple-family and mobile home park developments are taxed as commercial businesses, the assessed value for these developments is included in the commercial rather than residential category.

Figure 8-2
SEV Distribution, 2003
City of Tecumseh

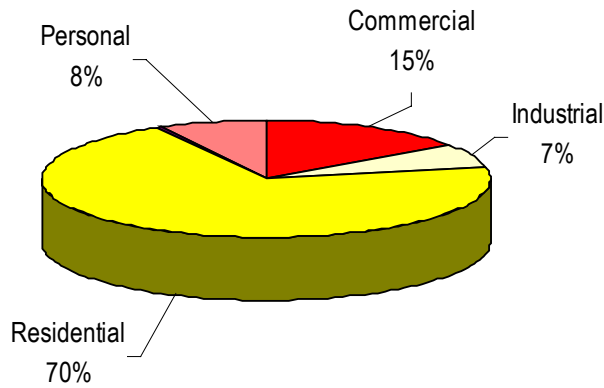
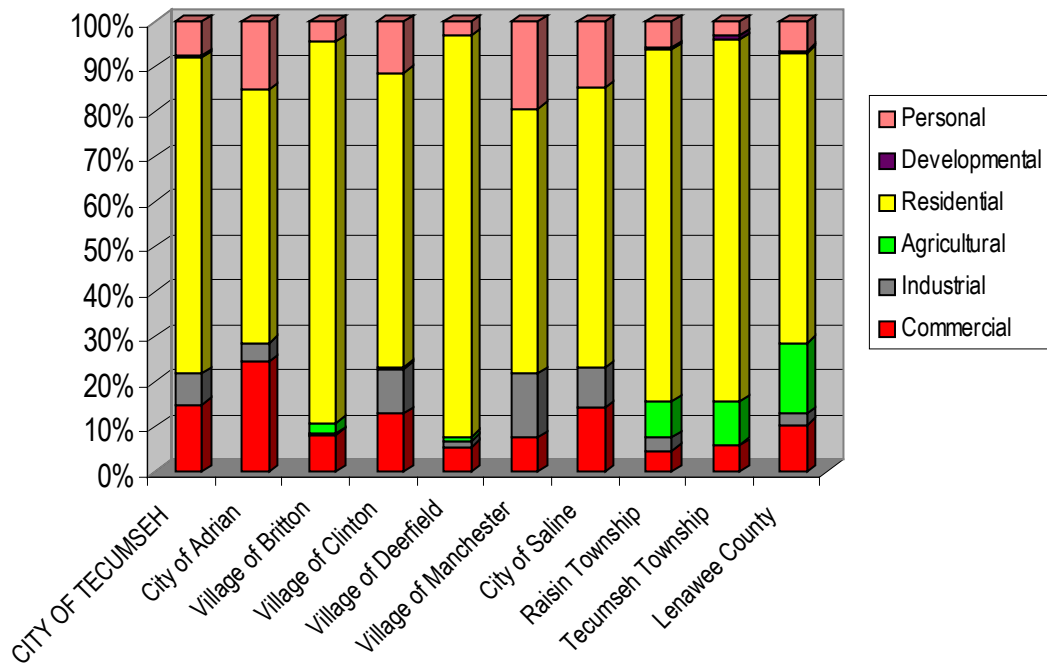


Figure 8-3 shows the percentage of total tax base, by land use category, for the City of Tecumseh and nearby communities. Table 8-1 summarizes all of the tax information. As Figure 8-3 portrays, the City of Tecumseh’s tax base is more balanced than many other communities in the area. For example, the percentage of the City’s SEV derived from commercial uses is only exceeded by the City of Adrian (and nearly equaled by the City of Saline). These uses strengthen the tax base because they produce a high tax dollar value per square foot of development. Industrial uses are also of particular value to a community because of high revenue and low demand for services (see discussion on following page).

Figure 8-3
SEV Distribution, 2003
City of Tecumseh and Nearby Communities



Tecumseh ranks near the middle of the communities surveyed in terms of the percentage of SEV derived from residential uses. However, compared with the other cities surveyed, Tecumseh has the lowest percentage of non-residential SEV. Adrian and Saline are strengthened by a strong personal property tax base. The following discussion on the Cost of Services from various land uses will help clarify why a sufficient supply of non-residential uses enhances public services for the residential support population.

Table 8-1
SEV by Property Class, 2003
City of Tecumseh and Nearby Communities

	Agricultural	% of Total	Commercial	% of Total	Industrial	% of Total	Residential	% of Total	Developmental	% of Total	Personal	% of Total	Total
CITY OF TECUMSEH	\$0	0.00%	\$41,475,600	14.92%	\$18,865,600	6.79%	\$195,112,600	70.18%	\$1,229,600	0.44%	\$21,335,400	7.67%	\$278,018,800
City of Adrian	\$0	0.00%	\$107,306,900	24.58%	\$17,777,500	4.07%	\$245,113,050	56.15%	\$0	0.00%	\$66,333,700	15.20%	\$436,531,150
Village of Britton	\$349,900	2.41%	\$1,196,400	8.24%	\$26,500	0.18%	\$12,289,500	84.68%	\$0	0.00%	\$650,500	4.48%	\$14,512,800
Village of Clinton	\$216,500	0.29%	\$9,541,900	12.92%	\$7,460,800	10.10%	\$48,190,900	65.23%	\$101,100	0.14%	\$8,365,200	11.32%	\$73,876,400
Village of Deerfield	\$216,100	0.93%	\$1,328,000	5.70%	\$269,000	1.15%	\$20,734,500	88.98%	\$0	0.00%	\$755,551	3.24%	\$23,303,151
Village of Manchester	\$0	0.00%	\$7,652,200	7.61%	\$14,330,200	14.24%	\$59,138,200	58.77%	\$0	0.00%	\$19,497,630	19.38%	\$100,618,230
Raisin Township	\$18,562,800	8.09%	\$10,740,400	4.68%	\$6,488,800	2.83%	\$179,578,400	78.22%	\$994,000	0.43%	\$13,229,400	5.76%	\$229,593,800
City of Saline	\$0	0.00%	\$63,845,900	14.34%	\$39,449,300	8.86%	\$276,523,700	62.10%	\$0	0.00%	\$65,447,500	14.70%	\$445,266,400
Tecumseh Township	\$7,901,000	9.56%	\$4,750,000	5.75%	\$191,800	0.23%	\$66,433,700	80.38%	\$728,300	0.88%	\$2,649,000	3.20%	\$82,653,800
Lenawee County	\$539,832,675	15.80%	\$361,861,828	10.59%	\$79,955,700	2.34%	\$2,198,687,498	64.35%	\$12,755,800	0.37%	\$223,481,114	6.54%	\$3,416,574,615

COST OF SERVICES OVERVIEW

A number of studies indicate that different types of land uses demand various levels of municipal services, such as schools, fire/police protection, sewer and water, road infrastructure, etc. Because of this range in demand for services and in turn municipal expenditures, it is important for communities to evaluate the composition of their tax base. The costs of services and infrastructure must be balanced against revenue. There are also issues related to quality of life, including traffic, noise, and other environmental impacts.

Fiscal impact analysis can be used to project the impact of the public costs and revenues associated with residential and non-residential growth. This methodology is beyond the scope of this report. However, there have been various studies that show that residential land uses (especially multiple-family residential with a high number of bedrooms) typically demand more services than they pay for in tax revenues. A study entitled "The Fiscal Impact of Sprawl", prepared by Dr. Robert Burchell of Rutgers University, provided the cost-revenue hierarchy of land uses shown in Table 8-2.

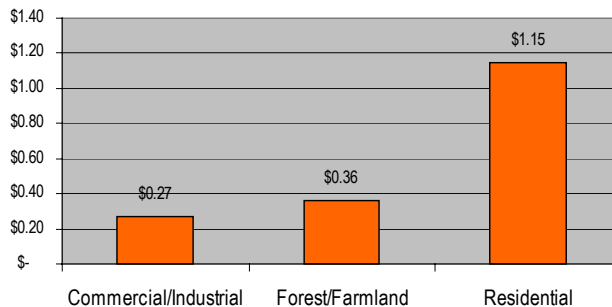
Table 8-2
The Cost-Revenue Hierarchy of Land Uses

↑ Municipal Gain (+)	Research Office Parks	↑ School District Gain (+)	
	Office Parks		
	High-Rise/Garden Apartments (Studio/1 bedroom)		
Age-Restricted Housing	Municipal Break Even		
Garden Condominiums (1-2 bedrooms)			
	Open Space		
↓ Municipal Loss (-)	Retail Facilities		School District Loss (-)
	Townhouses (2-3 bedrooms)		
	Expensive Single-Family Homes (3-4 bedrooms)		
	Townhouses (3-4 bedrooms)		
	Inexpensive Single-family Homes		
	Garden Apartments (3+ bedrooms)		
	Mobile Homes		

Burchell, Dr. Robert. The Growth Equation: Excerpts from a Presentation at the MSU Land Use Forum, February 18, 1997 Entitled "Fiscal Impact of Sprawl". Planning and Zoning News, Vol. 15, No. 10 August 1997.

Another report created by the Farmland Information Center looked at eighty-three different communities throughout the United States. Although the City of Tecumseh has little farmland left, the conclusions are nevertheless instructive. The summary report, shown in Figure 8-4, documented the high cost of residential development compared with agriculture, commercial, or industrial development. On average a residential home required \$1.15 of services for every \$1.00 in revenues generated while commercial/ industrial uses required \$0.27 for every \$1.00 of revenue and agricultural land \$0.36 for every \$1.00 of revenue.¹

Figure 8-3
Cost of Community Services
The cost of providing services for every \$1.00 in generated revenue



Source: American Farmland Trust, Fact Sheet of Cost of Community Services Study, Sept. 2001

¹ American Farmland Trust, Fact Sheet – Cost of Community Services Study, Sept. 2001

Research conducted in two townships in Calhoun County, Michigan indicated that residential uses cost \$1.47 and \$1.20 per \$1.00 of revenue generated, commercial and industrial cost \$0.20 and \$0.25 for every \$1.00 in revenue, and farmland and open space \$0.27 and \$0.24 for every \$1.00 in revenue.² The Farmland Information Center Report and the Calhoun County report focused on the fiscal benefits of farmland preservation for a community compared with residential development.

In summary, these studies conclude that open space, office, industrial, and some commercial facilities generate more in municipal tax revenues than public expenses and that most residential development generally does the opposite. Although these studies are generalized and should be used with caution since every community is different in terms of its fiscal stability, they do provide an additional consideration for land use planning in the City. However, cost is not the only issue the City needs to consider when determining an appropriate mix of land uses; it is just one factor in determining to what extent various land uses should be planned.

Most communities strive to balance the mix of open space, residential, and non-residential land uses to provide a more even flow of revenues and expenditures and to address quality of life issues. It is also important for communities to recognize that many retail land uses can demand a significant level of municipal services. Therefore, from a fiscal perspective, it is preferable that the non-residential tax base be composed of land uses such as office parks and high-tech industrial developments. Of course, fiscal considerations do not serve as the sole catalyst behind development policies.

The variation in generated revenue from different land uses is also important in terms of planning for infrastructure and public service needs of the community. As the City's population grows so will its need for public infrastructure and services. To ensure that residents are not overburdened with the costs of these improvements, additional non-residential may be appropriate. However, given the limited amount of land available for future non-residential sites, development will be limited to office uses and infill commercial development.

CONCLUSIONS

The preceding discussion identifies the following issues that the City of Tecumseh will need to consider while preparing their Master Plan:

- ❖ The City has a great share of its tax base coming from residential land uses, which in terms of public services typically require more costs to provide services than the tax revenue generated.
- ❖ Non-residential SEV is the lowest compared with the other cities surveyed. This indicates that residents in the City of Tecumseh bear the cost of public services more than in other areas.
- ❖ From a value per capita standpoint, the City of Tecumseh currently appears to be adequately positioned to provide needed services for its current residents. However, given that the City has a large portion of its tax base coming from residential development, the challenge will be to maintain this fiscal capacity as the City grows.

² Cost of Community Services, www.mqtinfo.org/planningeduc0087.asp/

- ❖ Due to limitations on land available for future development, increasing the non-residential share of the tax base could be accomplished through development of more office uses near the hospital and infill development within existing commercial areas.

MARKET ANALYSIS

The City of Tecumseh has three distinct commercial centers, which include the North End, West End and Downtown. All of these centers have very different market characteristics and business mixes that provide a variety of goods and services for Tecumseh residents and the outlying market area.

The North End is predominately composed of small center, neighborhood retail users that cater to the needs of residents in the immediate area. Development there is in the form of auto-oriented shopping centers and modest-sized individual businesses. The West End is a larger retail core and predominate users are strip retail uses, auto-oriented services and community-wide commercial. West

End businesses draw their customers from the larger Primary Trade Area. The Downtown is the historic commercial and governmental center of the community. All of these commercial areas play a vital role in the economic vitality of the City. The City and its residents would be well served by planning for uses that are appropriate for and enhance these three areas while meeting customer expectations and demand.

The following will discuss how a balanced commercial environment can be achieved within these three distinct centers. Factors that will be discussed include:

- ❖ Trade area and market potential
- ❖ Physical and Economic Characteristics
- ❖ Competition from other centers and “Niche” identification

The following analysis uses recent market data for the City’s primary trade area and builds on previous studies prepared for the City including the Economic Enhancement Strategy 1995: “Our Hometown Tecumseh”, and the Downtown Development Plan (1992). In addition to the Economic Development Plan, the proceeding Downtown Tecumseh Plan will also outline specific land use and physical improvements for the Downtown.

TRADE AREA AND MARKET STATISTICS

The Primary Trade area is the geographic area around the City’s commercial centers, which contains the majority of the customer base for the City’s retailers. For purposes of comparison, the same trade area identified in the *Economic Enhancement Strategy 1995: Our Hometown -*

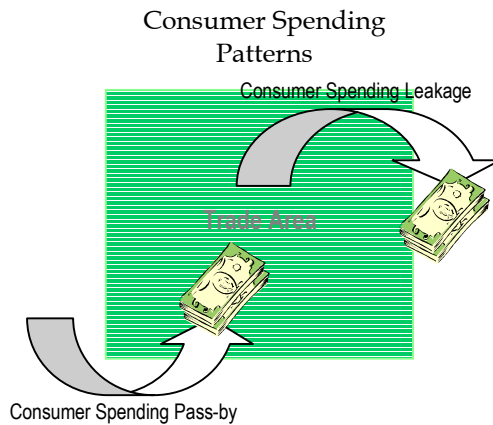
Map 8-1
Tecumseh Commercial Centers



Tecumseh is used for this analysis. The Primary Trade area currently has the following characteristics (See Appendix for detailed report):

- ❖ In 1995 the estimated population of the trade area was 29,448. The 2003 population is estimated at 33,984 with an estimated 12,870 households (1995 estimate households at 10,412). These numbers have increased from 1995 by approximately 12 percent and 19 percent, respectively.
- ❖ The average 2003 household size is estimated at 2.63 persons per household, which is a decrease of approximately 6 percent from the 1995 size of 2.81.
- ❖ The average 2003 household income within the Primary Trade Area is \$68,080, which is an increase of about 58 percent from the 1995 figure of \$43,152.
- ❖ According to the Demographic Profile (See Appendix), which reflects forecasted data, the percentage of the population that is in their teenage years will increase over the next five years as will the percentage of the population over the age of 45. This statistic is particularly important for development of business recruitment programs and residential planning.
- ❖ There are currently 253 retail businesses within the Primary Trade Area and of these there are approximately 28 retail businesses within the Downtown. Downtown businesses generate approximately 15 million dollars in sales annually.

Consumers within the Primary Trade Area spend approximately 394 million dollars for retail items, which reflects a 4 percent market share captured by Downtown businesses. All of the businesses within the Trade Area capture approximately 74 percent of the potential demand, which identifies that approximately 26 percent of all trade area retail sales are being made outside the trade area. This is



referred to as retail leakage. Retail leakage identifies a potential area for retail growth or capturing market share within the trade area. Providing for goods and services that are under-represented in the Trade Area can increase market share. It should be noted that the effect of retail spending occurring from pass-by traffic is not included within this analysis so the amount of spending occurring outside the primary trade area could be greater.

The areas where the greatest percentage of spending is occurring outside the trade area (leakage) and therefore potential exists to capture additional retail dollars include the following:

- ❖ Food and drink sales including full service restaurants, specialty foodservice and limited eating places
- ❖ Beer, wine and liquor stores
- ❖ General merchandise stores including department stores
- ❖ Clothing and clothing accessories stores
- ❖ Shoe Stores
- ❖ Vending machine operators
- ❖ Direct selling

- ❖ Books and music stores
- ❖ Auto parts and accessory
- ❖ Auto dealerships
- ❖ Home Furnishing stores
- ❖ Electronic and appliance stores
- ❖ Grocery Stores
- ❖ Specialty food stores
- ❖ Jewelry stores
- ❖ Sporting goods stores
- ❖ Used merchandise

The three commercial areas in the City would benefit by providing for these retail uses in addition to office and housing units. However, not all of the uses listed are appropriate within all three commercial areas. The West End would be a good location for auto-oriented and larger uses such as gas stations, auto dealerships, grocery stores, electronic and appliance stores and furniture stores with a large inventory.

Due to the location of the North End and its limited exposure to major thoroughfares, businesses that serve the daily, convenience needs of nearby neighborhoods would be more appropriate for this area. These could include carryout restaurants, beer/wine/liquor stores and general/discount merchandisers.

The Downtown is suitable for modest sized businesses that can fit into existing historic structures such as unique specialty gift stores, clothing stores, stationary stores, specialty food stores, sit-down restaurants, clothing boutiques, home furnishings stores, shoe stores, and children's apparel. If redevelopment of some of the less historic parcels were to occur then larger users could be accommodated such as department stores or restaurants with entertainment. It should also be stressed that auto-oriented businesses such as gasoline service stations and fast-food restaurants are not appropriate for the Downtown since they are conflicting to the pedestrian environment and demand individual driveways, which is inconsistent with the rhythm of the downtown streetscape. These auto-oriented uses should be encouraged to locate in more appropriate areas such as the West End commercial area.

Business retention, expansion and recruitment efforts on the City's part should not only focus on these business categories but also businesses that would meet the needs of the age groups expected to dominate population growth within the Primary Trade Area. Combining under-served businesses with uses that meet the needs of age groups would provide an even higher level of service to the community. The age groups that are expected to experience the greatest growth over the next 5 years are teenagers, early retirement and seniors. Businesses that would cater to teens would include a teen center with multiple activities for after school and weekends, a game room or family restaurant with an entertainment component, and small coffee shops that offer older teens and young adults a place to socialize.

Businesses that cater to seniors and empty-nesters could include services such as financial planning and investment, recreation and leisure activities such as golf, travel agencies and organized events through the senior center, and health care support uses such as pharmacies and medical offices. Providing housing alternatives for seniors and empty-nesters would also be beneficial. Housing alternatives could include townhomes, condominium units and senior housing including independent living facilities and congregate care centers.

Another use that is an important consideration is office. Office uses provided needed services for the Trade Area as well as provide a local consumer base for retail businesses during the day.

Office uses within the Downtown should be encouraged to locate on upper floors rather than first floor space desired by retailers for display of merchandise and advertising. Office uses within first floor spaces also interrupt the retail streetscape within the Downtown and disrupt the pedestrian flow. Larger office buildings could be accommodated within the West End or medical offices within the east area of the Downtown, close to the hospital.

Housing is also an important consideration. Housing not only provides shelter for residents but can also act to support retail and office uses. A customer base that has adequate access and is located within a reasonable proximity to commercial centers will help sustain and support local merchants. The North End and West End commercial areas are auto-oriented with more parking available compared with the Downtown, which make these areas convenient for existing and future residents. Distance between consumers and businesses is less important for these destinations compared with the Downtown since most consumers would be driving.

The Downtown, however, faces more critical issues in terms of access. Parking is restricted to public lots and on-street: auto-access is limited. During organized events and festivals parking can be even more challenging. Pedestrian circulation and access is the cornerstone of the downtown. Planning for residential close to Downtown or residential units above retail space within the Downtown would provide a customer base that could walk rather than drive to the downtown area thus relieving some of the demand for parking. The proceeding Downtown Tecumseh Plan explains in more detail proposed residential areas within the East Downtown planning area that can be accommodated to help support the overall Downtown.

PHYSICAL AND ECONOMIC CHARACTERISTICS

The physical and economic characteristics of the commercial centers can be viewed as characteristics that attract customers or, in industry terms, help to capture market share. A commercial center's ability to meet customer expectations for the center will help make the site attractive to consumers and increase market share. These characteristics vary between the three commercial areas in Tecumseh. Characteristics for the three centers include:

Physical

- ❖ *Buildings including architecture, signage, window display and design that create the overall retail environment.*
- ❖ *Site Design including pedestrian and traffic circulation and access.*
- ❖ *Streetscape or the "look" of the commercial area from the roadway or sidewalk.*

The West End includes large retail uses, which are setback from the roadway with front yard parking and individual driveways. Signs are large to compensate for the increased setback from the road. There is a lack of internal connections



Amenities such as decorative walls and landscaping serve to partially screen parking areas and provide an attractive front yard and side yard area.

between businesses, and pedestrian access is limited to connecting parking lots and individual businesses. Enhancement of the physical characteristics for this area would include the following:

- ❖ Use access management standards where driveways are limited and shared by multiple businesses and internal connections between parcels are encouraged.
- ❖ Encourage and/or require quality building materials that reflect a timeless appearance. The use of brick, stone or similar materials should be encouraged while less desirable materials such as concrete masonry unit or E.I.F.S. are limited to accenting building facades.
- ❖ Break up expansive parking lots with interior landscaped islands.
- ❖ Soften the look of large developments with exterior landscape treatments and berms that serve to diminish the appearance of parking lots.
- ❖ Ensure that loading areas, outdoor storage and dumpsters are located behind buildings and adequately screened by walls composed of materials similar to main buildings or by use of landscape treatments that have a similar screening effect.
- ❖ Adopt sign standards that limit signage to modest wall signs and monument signs.



This Flagstar Bank is an outlet building within a neighborhood center that continues the design theme of the main center.

The North End is composed of small neighborhood strip centers and fast food restaurants. Similar to the West End, there are generous building setbacks to accommodate front yard parking. Enhancement of the physical characteristics could come in the form of the following:

- ❖ Concentrating commercial development at centralized locations rather than permitting strip development along Evans Street.
- ❖ Encourage and/or require quality building materials that reflect a timeless appearance. The use of brick, stone or similar materials should be encouraged while less desirable materials such as concrete masonry unit or E.I.F.S. is limited to accent building facades.
- ❖ The use of landscaping is also an important feature within small-scale retail



This Domino's Pizza has an understated appearance, producing a unified look with the main center.

centers. Street trees and parking lot landscaped islands can have a significant impact on the appearance of neighborhood centers.

- ❖ Signage should be appropriate to the scale of the center. A uniform appearance to signs for individual businesses within a center can help to unify the development and create an identity.

The Downtown encompasses the historical character of a traditional small downtown. The pedestrian environment is created by the opportunity for window shopping, ample sidewalks, limited driveways, and an attractive streetscape. There are a number of specific recommendations for the physical enhancement of the Downtown, which are explained in more detail in the Downtown Tecumseh Plan component of the Master Plan.

Economic

- ❖ *The business mix offered to consumers and spin-off support uses*

The previous Trade Area and Market Characteristics section discusses in detail the recommended uses for the three commercial areas in the City. In general terms, uses that are underrepresented in the trade area, that are appropriate in the retail area, meet customers expectations, and fit within the design and scale of the area should be encouraged.

- ❖ *Planned events that draw customers and residents to the area*

Public events and festivals can benefit the residential community as well as the business community. Residents benefit from local festivals since they help create a community identity that can be “owned” by residents and in some cases a connection to the community’s past. Events and festivals also bring in tourists and out-of-towners that in turn eat, drink and buy from local merchants. Businesses can parallel their marketing efforts with local events to increase consumer patronage. For example during a Christmas festival held by the City, merchants could host an ice sculpture competition where local artists are commissioned to create sculptures for individual businesses. During the Music in the Park, local restaurants could host a “Taste of Tecumseh” for patrons in the park. To make these events successful, a coordinated effort between the various business organizations is crucial. The City of Tecumseh is fortunate to have a number of organizations that cater to the business community including the Chamber of Commerce, the Downtown Development Authority and the Central Business Association. In addition, the City has an active Economic Development Department that assists with the coordination of and dissemination of information for these various business organizations.

COMPETING CENTERS AND “NICHE” IDENTIFICATION

Competition for the City of Tecumseh’s share of market demand is generally within the communities of Adrian, Ann Arbor, Saline and Clinton. Communities like Adrian and Saline share characteristics similar to Tecumseh with regard to location, access, and commercial development. Tecumseh’s ability to compete with these centers and gain market share will be directly related to their ability to define and strengthen what makes Tecumseh different from the competition or in other words their “niche” in the market place. “Niche” is defined as something perceived as special, unique or one of a kind. It is difficult for a business to be all things to all people. Not only

is this difficult but it can be costly. Many businesses try to separate themselves from the pack by creating a “niche” or specialty.

The community can take steps similar to what businesses do by creating their own unique identity. This community identity should be focused and specific and not overly broad. A community that announces itself as a great community to live in with good schools, safe neighborhoods, recreation and the like is barely discernable from its neighbors. But a community that claims itself to be the Antique Capital of the State or famous for its restaurants has a specific “niche” that other communities in the area are not likely to possess. This is not to say that a City should not strive to provide quality housing, an unparalleled school system and numerous recreation opportunities for its residents, but a focused “niche” building program can enhance the community’s overall business environment, focus economic development funding, and have a positive effect on the overall local economy.

One of the first steps in a “Niche” building program is identification of a unique identity for the community or their “niche”. Developing this identity could be accomplished through a visioning process that involves business owners, residents and community leaders. The product of this process will be a unique identity that can help focus marketing and economic development efforts for the community.

One important focus area for “niche” building activities is the Downtown. As discussed earlier, the Downtown is the community’s historical and governmental core, so “niche” building efforts should begin within this core. The Downtown should be the cornerstone of the community’s identity and represent its niche in the trade area. Uses and architecture should compliment the City’s identity.

The other commercial centers, West End and North End, should be viewed as gateways to the Downtown area. They should serve two primary functions; provide goods and services to the Trade Area and enhance the community’s identity.

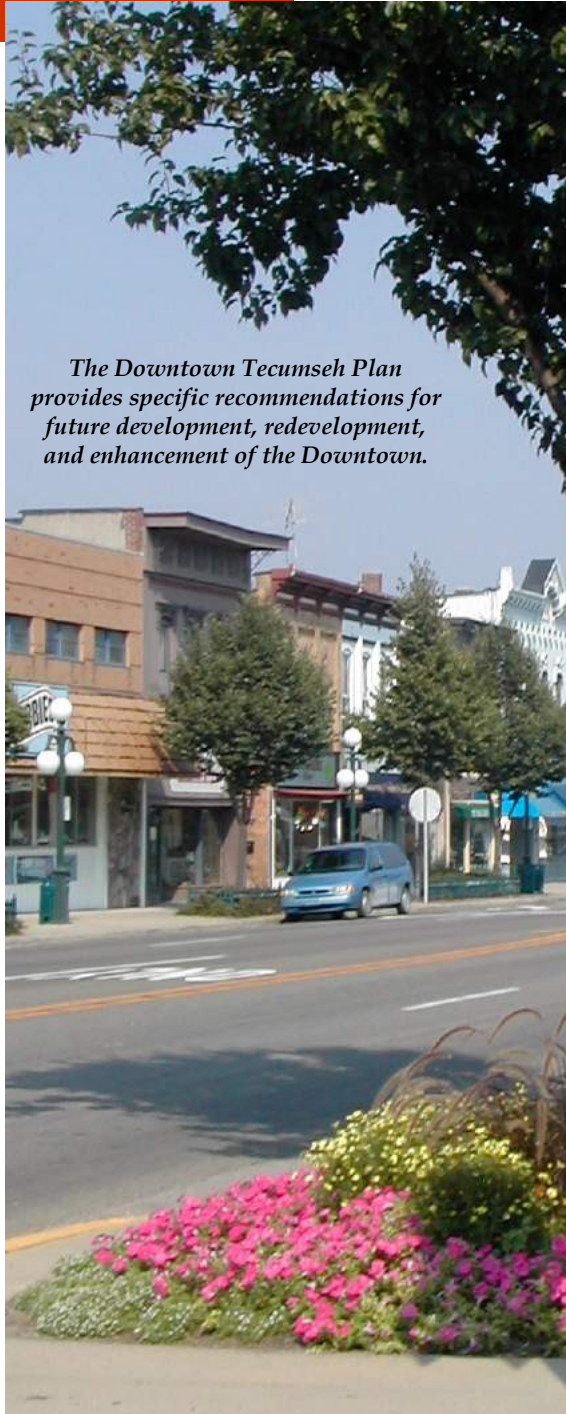
ECONOMIC DEVELOPMENT PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing conditions, Goals and Objectives, Market Characteristics and observations:

- Target specific businesses for recruitment that are under-served within the Trade Area.
- Ensure that retail and office uses are appropriate to the design and function of the three distinct commercial centers.
- Plan for residential near the Downtown to bolster its unique pedestrian environment.
- Continue Economic Development programs that assist with the coordination of business associations to ensure a balanced supply of businesses are locating within the City and the adequate dissemination of information.
- Develop a unique identity for the community, which can act as a springboard for “niche” building programs. Particular emphasis should be placed on the Downtown.
- The West End and North End areas should serve two primary functions: provide needed goods and services for their market area, and enhance the community’s “niche”.

9

DOWNTOWN TECUMSEH PLAN



The Downtown Tecumseh Plan provides specific recommendations for future development, redevelopment, and enhancement of the Downtown.

The City of Tecumseh's Downtown is perhaps the City's most visible asset. With its historic architecture and traditional downtown character, it is truly the heart of the City. Because the Downtown plays such a key role in defining the City's overall character and identity, this area deserves careful consideration within the context of the Comprehensive Plan.

The Downtown Tecumseh Plan component of the Comprehensive Plan provides an analysis of Downtown Tecumseh's urban pattern, parking locations, circulation, facades, signage, streetscape, land uses, and entry features. This Plan provides specific recommendations for future development, redevelopment, and enhancement of the Downtown.

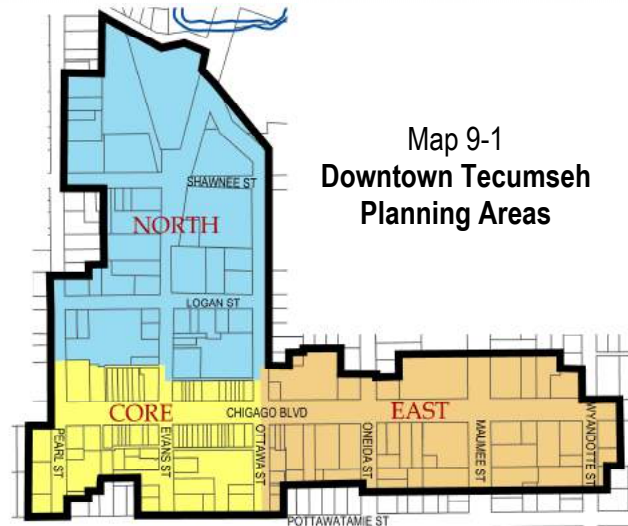
OVERVIEW

Downtown Tecumseh encompasses the City's main commercial center located along Chicago Boulevard and Evans Street. With approximately five blocks of frontage along Chicago Boulevard and four blocks along Evans Street, the Downtown includes a mix of retail, office, service, residential, and public uses. While the Downtown is faced with many challenges including increased commercial development at the western and northern boundaries of the City and significant truck traffic along Chicago Boulevard, its many assets and opportunities for improvement continue to make it the focal point of the City.

For the purposes of this chapter, Downtown Tecumseh has been divided into the following three planning areas (illustrated on Map9-1):

- ❖ **Core Downtown** – Chicago Boulevard from Pearl Street to Ottawa Street.
- ❖ **East Downtown** – Chicago Boulevard from Ottawa Street to Wyandotte Street.
- ❖ **North Downtown** – Evans Street from Chicago Boulevard to Bidwell Street.

An analysis of issues and recommendations for improvement are provided for each of the above planning areas.



CORE DOWNTOWN

The Core Downtown planning area encompasses Tecumseh’s historic “Main Street” area. It is comprised primarily of two and three-story buildings built up to the sidewalk to form a continuous building wall. While much of the area’s historic character has been retained over the years, the Core has experienced change – some of which has detracted from the Downtown’s overall character and pedestrian atmosphere. The Core Downtown Façade and Signage Assessment on pages 9-8 through 9-13 highlights many of the assets, challenges, and opportunities for improvement found within the Core Downtown planning area. The following is a summary of the main issues facing the Core Downtown with recommendations for improvement.

BUILDING FACADES

- ❖ **Issue:** Many buildings have experienced façade and roof additions that detract from the appearance of the building and the overall character of the downtown.

Recommendation: Updates and renovations to existing buildings are encouraged but should be compatible with and complimentary toward the traditional architectural style of the Core Downtown. Quality materials, complimentary colors, and classic design should be incorporated into building facades. Consideration should be given toward the individual building style as well as the overall character of the Core Downtown. Specific design guidelines assist in giving property owners guidance in this area. The DDA should evaluate the feasibility of a grant program in addition to its low-interest loan program for façade changes that are consistent with DDA guidelines.

- ❖ **Issue:** Building height varies from one story to three stories within the Core Downtown.

Recommendation: Buildings within the Core Downtown should be at least two stories and similar in height to surrounding buildings. Greater building height is appropriate and encouraged at the Downtown’s main intersection of Chicago Boulevard and Evans Street. One-story buildings should be avoided.

- ❖ **Issue:** Interruptions in the building wall exist in several locations.

Recommendation: Whenever possible, breaks in the continuous building wall should be avoided. Infill and/or redevelopment of these areas with appropriately designed buildings is encouraged.

- ❖ **Issue:** The style, size, and placement of many second-story replacement windows is inconsistent with surrounding buildings and out of character with the overall Core Downtown. In addition, many upper-story windows have been covered over or shuttered.

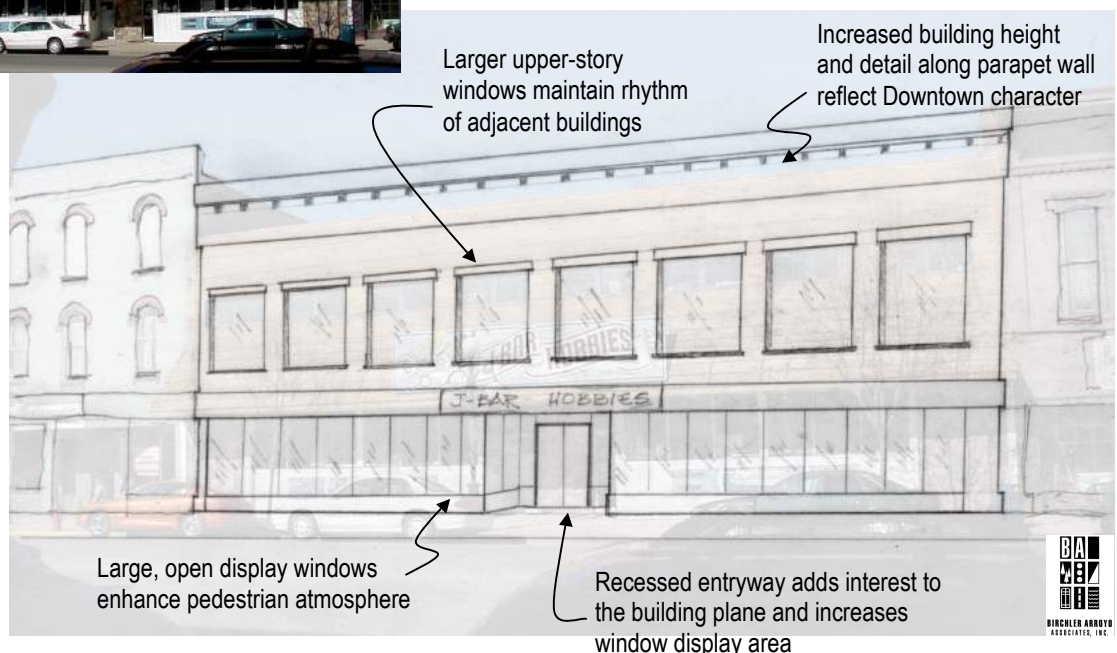
Recommendation: Upper-story windows should be consistent throughout the Core Downtown in style, size, and rhythm. Replacement windows should be in keeping with the building's original character, should be of similar size as the originals, and should maintain the overall rhythm found in the Core Downtown.

- ❖ **Issue:** Some buildings in the Core Downtown lack key features of well-designed, attractive, and pedestrian-friendly storefronts.

Recommendation: Storefronts should be open to the street and sidewalk and inviting to pedestrians. To achieve this, storefronts should be designed with large display windows that extend low to the ground and recessed or covered entryways that encourage window-shopping and enhance the overall pedestrian character of the Downtown. In addition, awnings should be limited to above windows and entryways only rather than extending across entire building facades.



Example of façade renovation in keeping with surrounding buildings and Downtown character.



- ❖ **Issue:** Some buildings in the Core Downtown lack attractive rear facades, signage, and customer entrances.

Recommendation: Rear building facades should be attractive and well-maintained to provide secondary customer entrances in proximity to public parking areas. Signage should be limited to identifying customer entrances and should not be utilized for advertising. Loading areas and trash receptacles should be appropriately screened, and these areas should be kept clear of refuse. Outdoor storage should not be permitted. Streetscape design should be continued to reinforce the Downtown character.



Appropriate signage and awnings can be incorporated into rear facades to enhance secondary entrances.



Attractive masonry enclosures effectively screen dumpsters in rear parking areas (left). Unscreened dumpsters have a negative appearance and clutter rear facades (right).

SIGNAGE

- ❖ **Issue:** Signage within the Core Downtown does not always reflect the area's traditional character and often lacks quality in both design and materials.

Recommendation: Signs on building facades should be integrated into the architectural design of the building in a manner that will compliment rather than detract from the overall character of the Core Downtown. Through the use of appropriate design, material, color, and texture, signage should enhance the primary and architectural significant features of the building. Signage should not cover, obscure, or overshadow building features and design. A DDA-sponsored sign grant program could provide seed money to encourage new signs consistent with DDA guidelines.

- ❖ **Issue:** There is inadequate and ineffective pedestrian-oriented signage within the Core Downtown.

Recommendation: Signs with a pedestrian orientation,



Examples of attractive, pedestrian-oriented projecting signs in the Downtown.



such as projecting signs and attractive window signs, are strongly encouraged. Menu boards are also encouraged near sidewalk entrances to restaurants. All pedestrian-oriented signage should be of high quality materials and design that are complimentary to the character of the building and Downtown.



Examples of open spaces that maintain pedestrian atmosphere.

STREETSCAPE

- ❖ **Issue:** Interruptions in the building wall exist in several locations.

Recommendation: Whenever possible, use open spaces between buildings in a manner that will continue the pedestrian streetscape. For example, such spaces could be transformed into pedestrian plazas or attractive outdoor display areas. Appropriate nighttime lighting of these spaces is important to create a sense of security among pedestrians, as well as to maintain pedestrians' interest.



- ❖ **Issue:** Some buildings in the Core Downtown lack key features of well-designed, attractive, and pedestrian-friendly storefronts.

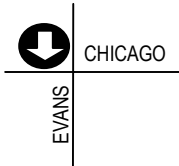
Recommendation: In addition to façade improvements discussed above, storefronts and the overall pedestrian atmosphere should be enhanced with amenities such as planters, A-frame signs, menu boards, sidewalk furniture, outdoor eating areas, pedestrian-scaled signage and lighting, etc.



Examples of amenities and details that enhance the Downtown streetscape and pedestrian experience.



Example of streetscape improvements that enhance the pedestrian atmosphere.



- ❖ **Issue:** The intersection of Chicago Boulevard and Evans Street is at the heart of the Downtown and lacks any significant streetscape treatment.

Recommendation: A special streetscape treatment for this important intersection should be developed in order to enhance its visual appearance and signify its central position within the Downtown. Key design elements could include mast arm traffic signals, planters, directional signage, decorative sidewalk pavers, etc.



Example of mast arm traffic signal at Chicago Boulevard and Evans Street.

- ❖ Issue: The City lacks any features that announces the South entrance into the Downtown.

Recommendation: Evans Street south of Chicago Boulevard should be enhanced through streetscape improvements including decorative, pedestrian-scaled lighting, sidewalk furniture, planters, decorative pavers, on-street parking, etc. Streetscape improvements should achieve the goal of announcing the entrance into the Downtown.

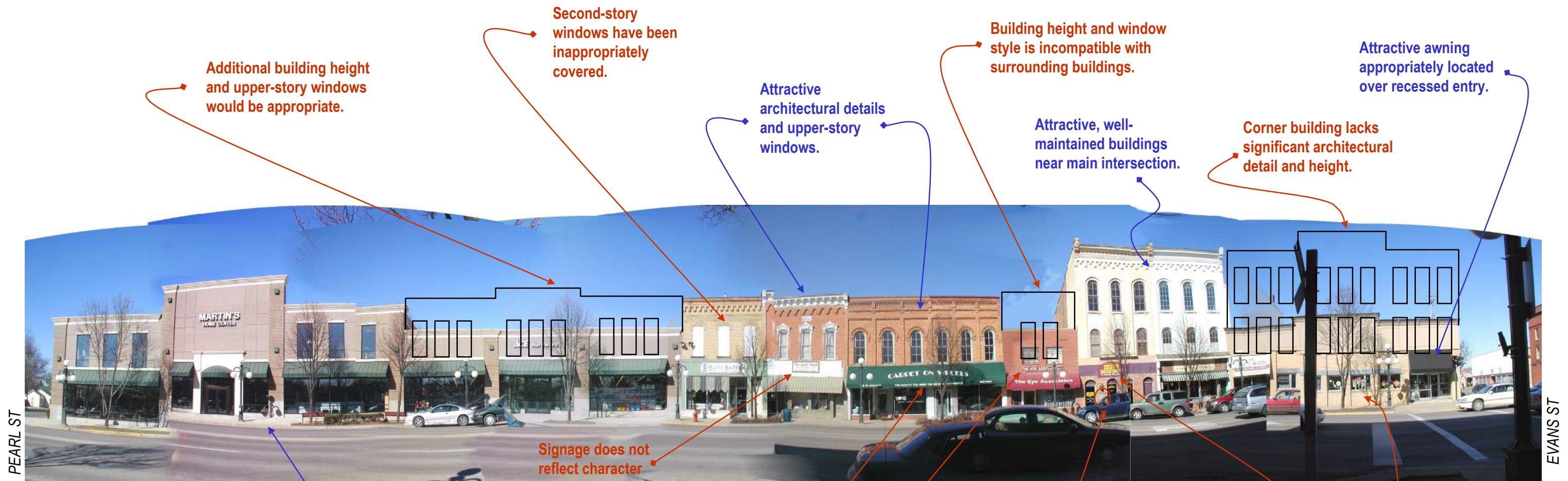


Existing streetscape looking north along Evans Street toward Chicago Boulevard.

CORE DOWNTOWN PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing conditions and the Goals and Objectives:

- Develop and encourage participation in business assistance programs through the Downtown Development Authority, such as Façade Enhancement, Sign Enhancement, and Market Assistance.
- Review design guidelines and criteria to address building façade and signage issues (e.g. window style and rhythm, building materials, storefronts and display windows, rear entrances and signage, etc.)
- Amend the Zoning Ordinance to address building placement and height issues (i.e. establish a minimum building height and zero side and front setbacks).
- Adopt one common color for street furniture, light posts, trash receptacles, etc. to be carried through the entire Downtown streetscape.



Additional building height and upper-story windows would be appropriate.

Second-story windows have been inappropriately covered.

Attractive architectural details and upper-story windows.

Building height and window style is incompatible with surrounding buildings.

Attractive awning appropriately located over recessed entry.

Attractive, well-maintained buildings near main intersection.

Corner building lacks significant architectural detail and height.

PEARL ST

EVANS ST

Signage does not reflect character of building.

Awning extends across entire building and overwhelms the façade.

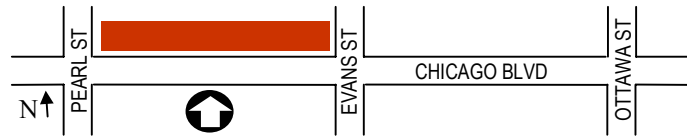
Signage does not reflect character of building.

Newer building maintained storefront rhythm and pedestrian atmosphere through the appropriate design of large display windows, awnings, and variation in building height.

Signage on building and awning is excessive.

Small display windows close storefront to the street and sidewalk and interrupt the pedestrian experience.

**CORE DOWNTOWN
FAÇADE AND SIGNAGE
ASSESSMENT**



Ineffective use of menu board.



- ◆ Assets
- ◆ Opportunities for improvement

Inappropriate roof addition overwhelms the facade and detracts from building's historic character.

Boarded and shuttered upper-story windows.

Building style, height, and façade is incompatible with surrounding buildings and overall Downtown character.

Upper-story replacement windows are too small and lack architectural detail of original windows.



Significant corner building with architectural details including date.

Well-maintained building.



Signage does not reflect character of building.

Awning is appropriately located above individual storefront.

Attractive display window and signage.

Awning is appropriately located above windows and entryway, rather than extended across entire building façade.

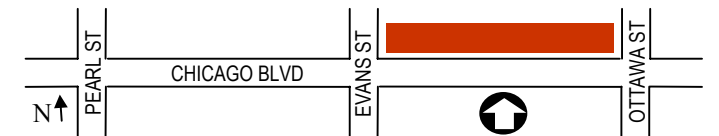
Appropriately sized display windows and recessed entries.

Significant corner building with increased height and attractive details such as date block and decorative window.

Small display windows and excessive signage close storefront to the street and sidewalk and interrupt the pedestrian experience.



CORE DOWNTOWN FAÇADE AND SIGNAGE ASSESSMENT



◆ Assets

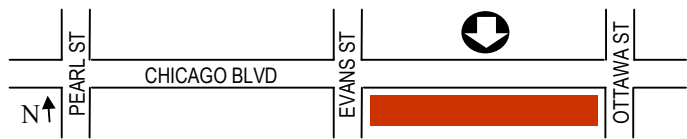
◆ Opportunities for improvement

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**CORE DOWNTOWN
FAÇADE AND SIGNAGE
ASSESSMENT**



- ◆ Assets
- ◆ Opportunities for improvement



Rhythm of all upper-story windows maintained among all multiple story buildings.

Well-maintained, attractive buildings.

Building height and style not in character with surrounding buildings.

Break in continuous building wall.

EVANS ST

PEARL ST

Significant corner building with increased height and attractive display windows.

Attractive display window and signage.

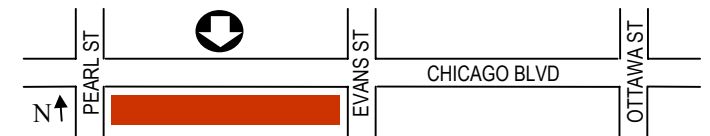
Roof addition does not reflect character of buildings.

Lack of display windows close storefront to the street and sidewalk and interrupt the pedestrian experience.

Break in continuous building wall.



CORE DOWNTOWN FAÇADE AND SIGNAGE ASSESSMENT



◆ Assets

◆ Opportunities for improvement

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NORTH DOWNTOWN

LAND USE

- ❖ **Issue:** A significant amount of land within the North Downtown is under developed and/or utilized for uses that would be more appropriate at other locations in the community.

Recommendation: Encourage relocation of light industrial and general commercial businesses to more appropriate locations in the City. Redevelopment of properties along Evans and Logan Streets should include multi-story retail buildings set close to the street with off-street parking limited to shared lots located in the rear. All new development and redevelopment should achieve the goal of creating a natural extension of the Core Downtown. The property located north of Shawnee between Evans and Ottawa should be redeveloped for office use in order to provide a transition between the Downtown retail area and the residential area to the north. Office development at this location should consist of one or more two-story buildings that are compatible with and complimentary toward the traditional architectural style of the overall Downtown. Building placement and design should achieve the goal of announcing the entrance into the Downtown.



Examples of light industrial activity along Evans Street.



- ❖ **Issue:** The City lacks any significant entry feature that announces the North entrance into the Downtown.

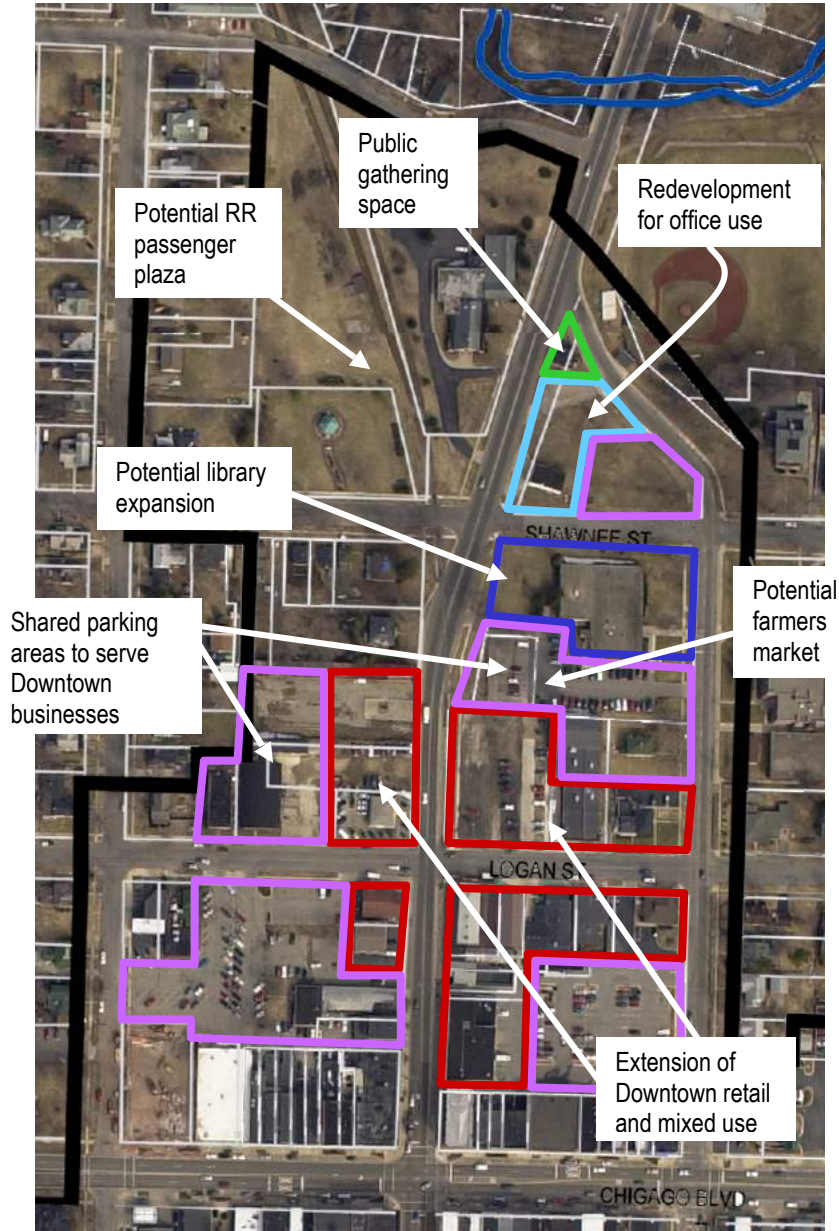
Recommendation: Enhancement and redevelopment of the properties located at the southeast corner of the intersection of Evans and Ottawa is strongly encouraged to provide a focal point and attractive entrance into the Downtown. Along with special signage and landscaping, this key intersection should be developed to include a small public plaza with pedestrian furniture and space for public art.

- ❖ **Issue:** The library is located in a prominent location at the entrance to the Downtown but lacks any visual presence in the Evans Street corridor.

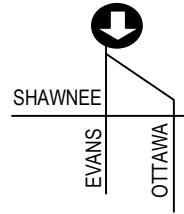
Recommendation: Any future expansion of the library facility should be designed to increase its visual presence along Evans Street. Potential for the addition of a second story and/or new main entrance off Evans Street should be explored.

❖ **Issue:** The City lacks a significant public gathering space within the Downtown.

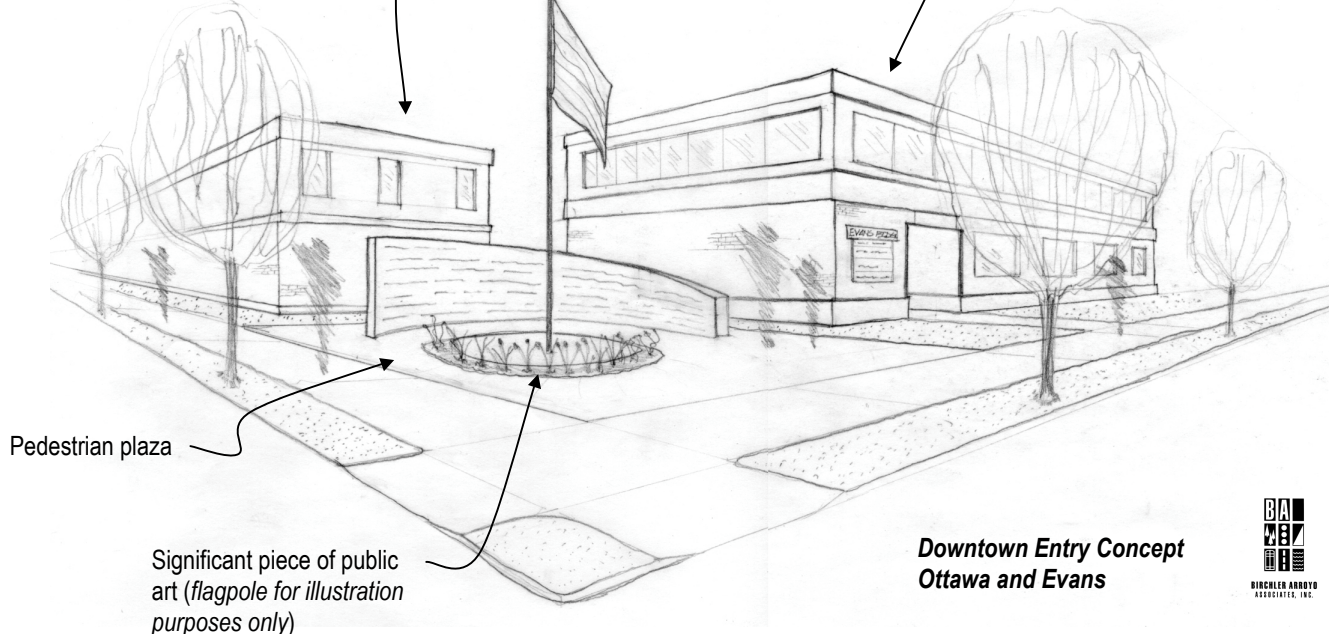
Recommendation: The City should explore the development of an open-air farmers market south of the library. Such a facility could also provide additional parking for the Downtown when events are not taking place.



Downtown Tecumseh Plan



Two-story office buildings with parking behind



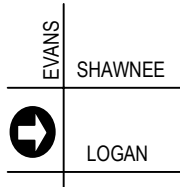
Pedestrian plaza

Significant piece of public art (flagpole for illustration purposes only)

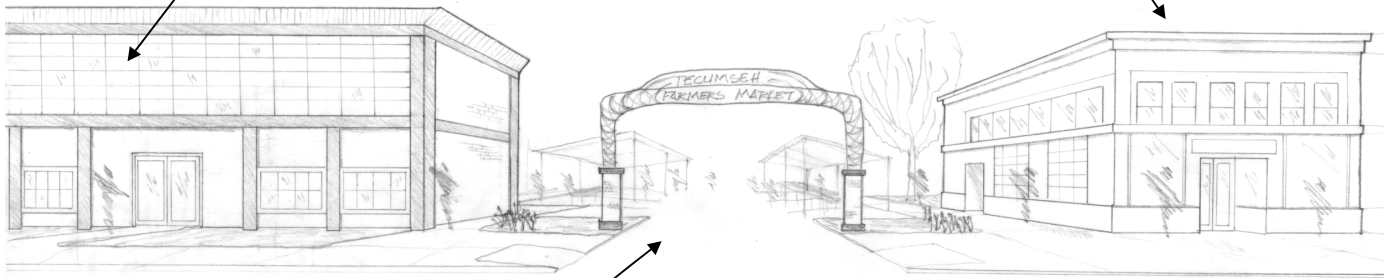
**Downtown Entry Concept
Ottawa and Evans**



Library expansion



New retail building



Farmers Market entrance

**Evans Street Redevelopment
Concept**



BUILDING FACADES AND SIGNAGE

- ❖ **Issue:** The existing character of retail buildings in the North Downtown planning area is inconsistent with the historic and architectural character of the Core Downtown.

Recommendation: Building styles, materials, and facades should be compatible with and complimentary toward the traditional architectural style of the Core Downtown. Retail buildings should be at least two stories in height, set close to the sidewalk, and contain pedestrian-friendly storefronts with features such as awnings, large display windows, and recessed entries. The use of features such as date markers, corner stones, and parapet wall and window details is encouraged to give newer buildings a character similar to that found in the Core. Building design should achieve the goal of creating a natural extension of the Core Downtown.

- ❖ **Issue:** Signage within the North Downtown area often lacks quality in both design and materials.

Recommendation: Signs on building facades should be integrated into the architectural design of the building in a manner that will compliment rather than detract from the overall character of the Downtown. Through the use of appropriate design, material, color, and texture, signage should enhance the primary and architectural significant features of the building and achieve the goal of creating a natural extension of the Core Downtown. Signs with a pedestrian orientation, such as projecting signs and attractive window signs, are strongly encouraged to enhance the North Downtown's pedestrian atmosphere.

STREETSCAPE

- ❖ **Issue:** The North Downtown planning area lacks a pedestrian-friendly atmosphere.

Recommendation: In addition to the façade and signage recommendations above, the Evans Street pedestrian atmosphere should be enhanced through streetscape improvements including decorative, pedestrian-scaled lighting, sidewalk furniture, planters, decorative pavers, on-street parking, etc. Off-street parking should be limited to shared lots located behind commercial buildings and should be appropriately separated from the sidewalk by decorative screen walls and landscaping. Streetscape improvements should achieve the goal of creating a natural extension of the Core Downtown.



Example of window and parapet details on historic building in Downtown (above). Example of quality materials and date marker on newly constructed building in another community (below).



Example of Evans Street building facades that lack appropriate design elements needed to continue the Downtown streetscape and pedestrian atmosphere.



- ❖ **Issue:** While an asset to tourism in the City, the existing railroad tracks located along Evans Street are a challenge to the corridor's visual character and pedestrian atmosphere.

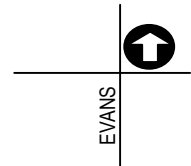
Recommendation: If use of the tracks ceases in the future, removal of the railroad tracks in the Downtown south of Shawnee Street would be encouraged. This would provide an opportunity for reconstruction of the roadway and right-of-way to include on-street parking, enhance the overall pedestrian atmosphere, and strengthen the corridor's connection to the Core Downtown. In the short-term, the City should work with the Railroad to evaluate the potential for construction of a passenger plaza near the Downtown's north entrance which would allow for removal of the tracks south of Shawnee (in the event that the tracks are not extended south of Tecumseh).

- ❖ **Issue:** The City lacks any significant entry feature that announces the North entrance into the Downtown.

Recommendation: Enhancement and redevelopment of the properties located at the southeast corner of the intersection of Evans and Ottawa is strongly encouraged to provide a focal point and attractive entrance into the Downtown. Along with consistent streetscape amenities and appropriately designed retail and office buildings along Evans Street, a special treatment, including signage and landscaping, for this key intersection is recommended.



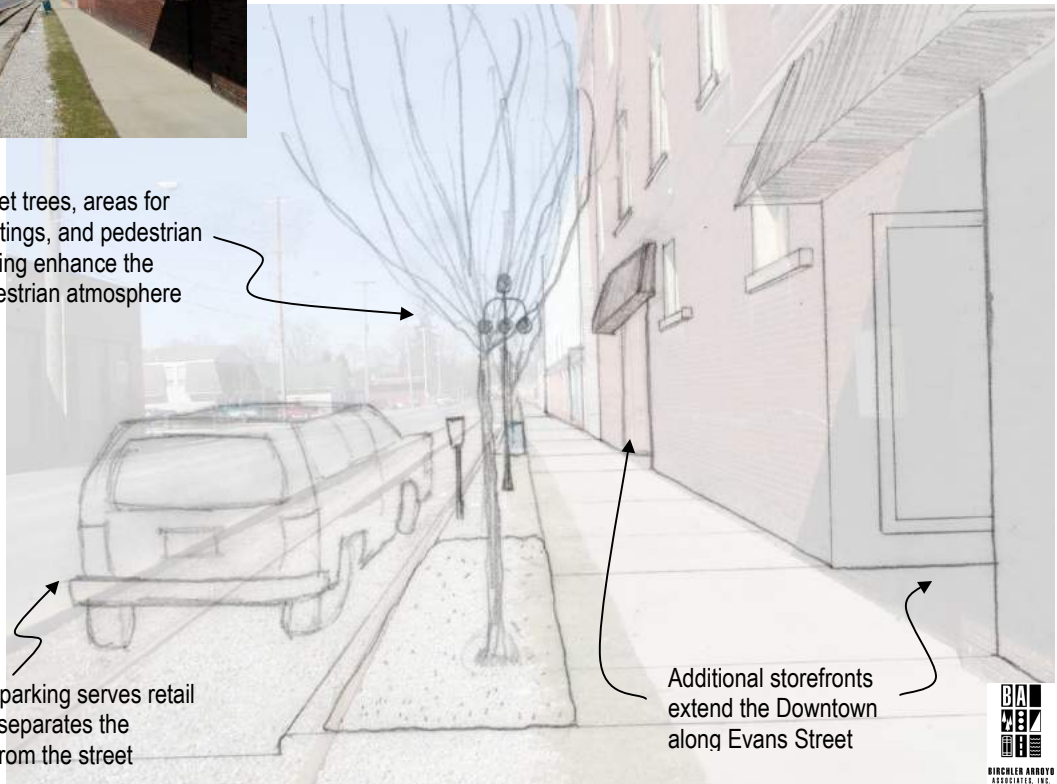
Example of streetscape improvements that enhance the pedestrian atmosphere and extend the Downtown character (in the event that the railroad tracks are removed from the Downtown).



Street trees, areas for plantings, and pedestrian lighting enhance the pedestrian atmosphere

On-street parking serves retail uses and separates the sidewalk from the street

Additional storefronts extend the Downtown along Evans Street



NORTH DOWNTOWN PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing conditions and the Goals and Objectives:

- ❑ Monitor the long-range plans of the Southern Michigan Railroad with regard to railroad tracks in the Evans Street corridor.
- ❑ Consider purchase of key properties to be developed through a preferred Development Agreement with a private company.
- ❑ Develop and encourage participation in business assistance programs through the Downtown Development Authority, such as Façade Enhancement, Sign Enhancement, and Market Assistance.
- ❑ Review design guidelines and criteria to address building façade and signage issues (e.g. window style and rhythm, building materials, storefronts and display windows, rear entrances and signage, etc.)
- ❑ Amend the Zoning Ordinance to address building placement and height issues (i.e. establish a minimum building height and zero side and front setbacks).

EAST DOWNTOWN

LAND USE

- ❖ **Issue:** The character of the Core Downtown appears to “unravel” as the existing development east of Ottawa Street creates less of the traditional downtown atmosphere due to more interruptions in the building wall and more visible off-street parking.



Example of existing building with driveway onto Chicago Boulevard east of Ottawa Street.

Recommendation: If infill development is not possible, gaps in the building wall should be designed to continue the pedestrian streetscape. For example, open spaces could be used for pedestrian plazas and outdoor display areas. When parking areas exist between buildings, they should be appropriately screened. Curb cuts onto Chicago Boulevard should be limited in order to preserve the pedestrian atmosphere and continue the Downtown streetscape.

- ❖ **Issue:** A significant amount of land east of Maumee Street is under developed considering its proximity to Downtown businesses.



Existing view of Chicago Boulevard looking west from Wyandotte Street

Recommendation: Development and redevelopment of properties east of Maumee Street for residential use at a

higher density than what currently exists would provide for increased housing opportunities close to the Downtown. Development such as townhouses fronting on Chicago Boulevard would enhance the streetscape as well as provide a transitional entry into the Downtown retail area. Building placement and design should achieve the goal of announcing the entrance into the Downtown. Attached single family housing in this area may also provide additional housing options for empty nesters and seniors in proximity to the hospital and surrounding medical offices, as well as local shopping and services. In addition, well-designed residential development rather than commercial development in this area may better preserve significant trees that exist along Chicago Boulevard.

BUILDING FACADES AND SIGNAGE

- ❖ **Issue:** The character of the Core Downtown appears to “unravel” as the type and style of existing development changes east of Ottawa Street.



Example of existing development east of Ottawa Street.

Recommendation: Updates and renovations to existing buildings are encouraged to achieve facades and signage that are compatible with and complimentary toward the traditional architectural style of the Core Downtown. Quality materials, complimentary colors, and classic design should be incorporated into building facades. All new development and redevelopment should achieve the goal of creating a natural extension of the Core Downtown.

STREETSCAPE

- ❖ **Issue:** The character of the Core Downtown appears to “unravel” as the streetscape is increasingly interrupted by driveways and parking lots.



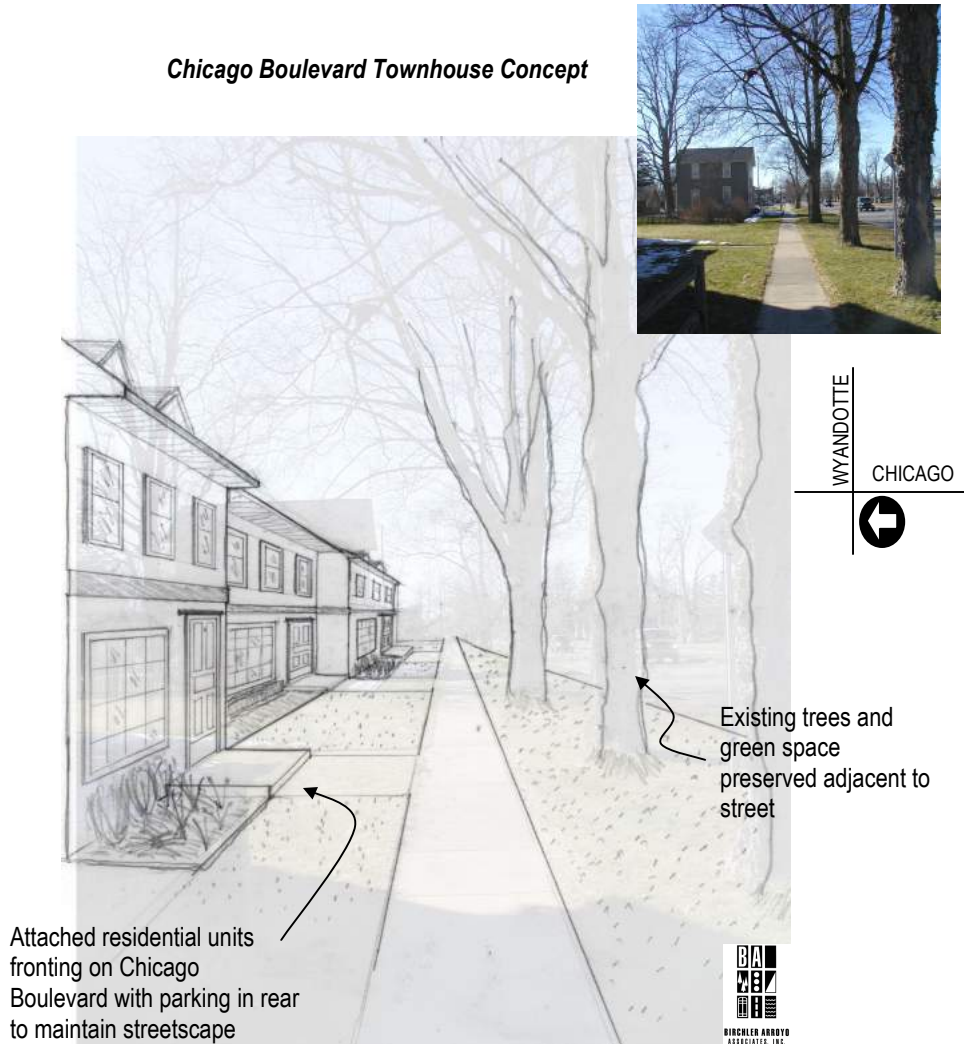
Example of street trees, lighting, and planters that maintain and enhance the pedestrian atmosphere east of Ottawa Street (left and right). Example of screen wall and plantings that separate the sidewalk from adjacent parking area (left).

Recommendation: Key streetscape elements should be maintained along Chicago Boulevard in the East Downtown planning area.

Features such as pedestrian-scaled lighting, planters, street trees, benches and other street furniture should be consistent with those found in the Core Downtown. Mast arm traffic signals at the Chicago Boulevard intersections with Ottawa and Maumee Streets are also recommended to enhance the overall streetscape appearance and create a unified Downtown character. Parking areas should be appropriately separated from the sidewalk

by decorative screen walls and landscaping. Streetscape improvements should achieve the goal of creating a natural extension of the Core Downtown.

Chicago Boulevard Townhouse Concept



EAST DOWNTOWN PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing conditions and the Goals and Objectives:

- Develop and encourage participation in business assistance programs through the Downtown Development Authority, such as Façade Enhancement, Sign Enhancement, and Market Assistance.
- Review design guidelines and criteria to address building façade and signage issues (e.g. window style and rhythm, building materials, storefronts and display windows, rear entrances and signage, etc.)
- Amend the Zoning Ordinance to permit the type of attached residential development desired along Chicago Boulevard.
- Consider rezoning areas targeted for townhouse residential to a new zoning district that incorporates appropriate design standards.

10

CAPITAL IMPROVEMENTS / INFRASTRUCTURE PLAN



*The Capital Improvements /
Infrastructure Plan provides a general
overview of capital improvement and
facilities needs for the City of Tecumseh.*

The location and timing of infrastructure improvements such as water and sewer lines, streets, and public facilities such as police and fire stations should be planned in advance to minimize unnecessary costs and promote efficiency of service. This component of the Comprehensive Plan provides a general overview of capital improvement and facilities needs for the City of Tecumseh.

PUBLIC SERVICES ANALYSIS

EMERGENCY SERVICES

The Emergency Services Department provides fire, rescue, and EMS response to the City of Tecumseh and Tecumseh Township. The Department maintains one fire station located at the northeast corner of Evans Street and Russell Road. The station is equipped with two fire engines, one fire engine ladder truck, one tanker, one light rescue vehicle, one brush truck, and a chief's vehicle. Currently, the Department consists of four full time career firefighters with medical training at or above the EMT-Specialist level, as well as twenty paid on-call firefighters and paramedics.

For future planning purposes, there are several methods available to fire departments for projecting future staffing needs and facility locations. Such methods include analysis of personnel to population ratios, response times, and service areas.

The Department's current personnel represent a ratio of 2.3 firefighters per thousand City and Township residents. When based on the City's population only, that ratio increases to 2.8 firefighters per thousand residents. Table 10-1 below provides the estimated range of additional firefighters required in order to maintain the current 2.8 ratio based upon future population projections for the City. It is important to note that for the purposes of this analysis, Department personnel needs have been projected based upon the City's population only. Future personnel needs may vary as a result of changes in Tecumseh Township's population, as well as potential changes in the service agreement between the City and Township.

Table 10-1
**Emergency Services Department
 Projected Personnel Needs**

Year	Forecast Population	Number of Firefighters	Additional Firefighters Needed
2000	8,574 (Actual)	24 (Actual)	0
2010	9,072 – 9,283	25 – 26	1 – 2
2020	9,403 – 10,050	26 – 28	2 – 4

As indicated in the table above, this method of projection estimates that one to two additional firefighters will be needed by the year 2010, with a Department total of 26 to 28 needed by 2020.

The National Fire Protection Association (NFPA) is a non-profit agency with more than 75,000 members worldwide that recommends standards for personnel deployment and response times to fires and medical emergencies. NFPA standards are not law, nor are they a federally mandated regulation; however, they are intended to maximize the effectiveness, efficiency, and safety of emergency operations and are based on industry research.¹ The current NFPA recommendation for *volunteer* departments suggests that a minimum of four firefighters arrive at a fire scene within five minutes of the dispatch center receiving the call, 90 percent of the time. The City's Emergency Services Department reported a typical response time of five to six minutes to calls within the City or Township.

For planning purposes, many fire departments look to the Insurance Services Office, Inc. (ISO) for recommended standards for siting fire stations and locating apparatus within a community. For built-up or fully developed areas, the ISO recommends that fire departments operate an engine company within 1.5 road miles and a ladder service company within 2.5 road miles. Map 10-1 depicts the location of the City's Emergency Services Department with 1.5-mile and 2.5-mile service area radii. (Note that the ISO distance recommendations are for *road miles*. For simple illustration purposes, Map 10-1 portrays these distance recommendations using *radii* from the Emergency Services Department.) This map is intended for general planning purposes only, and it is important to note that these service area standards are *recommendations* made by the ISO for completely *developed areas*. While some areas of the City and Township appear to be outside of the recommended service areas, much of this area has not been fully developed. As additional development occurs in these areas, it is a long-

¹ NFPA standard 1720 issued in 2001 sets minimum criteria for response times as well as the staffing of firefighter crews and emergency medical services.

range goal of the Department to construct and operate a second station within the City. No sites have been targeted for any future station(s), as this is a long-range goal; however, it is likely that a second station would be located in the northwest quadrant of the City in order to maximize the Department's service area.

POLICE

The City of Tecumseh Police Department, located at City Hall, employs fifteen sworn officers, one animal control and enforcement officer, and two full-time and several part-time dispatchers.

The Department is operating at a level of service of 1.7 sworn officers per thousand residents (based on fifteen total sworn officers). The Federal Bureau of Investigation's annual report, Crime in the United States (CIUS) for 2002, indicates that cities in the East North Central Division of the Midwest Region with a population under 10,000 persons employ an average number of 2.7 sworn officers per thousand residents.² The current staffing level of sworn officers of the City of Tecumseh Police Department is below the region's average. It is important to note that CIUS data is not intended to serve as recommended staffing levels for individual communities. The data contained in the report should only be viewed as a guideline. Appropriate personnel needs of the Department can only be determined after careful analysis of the many conditions affecting the City's police service requirements.



Increased development in and around the City may result in additional crimes against property, such as theft and vandalism, as well as crimes against individuals. Traffic violations and the number of traffic crashes in the City will likely multiply as a result of increasing traffic volumes associated with development. Additional sworn officers, civilian staff, facilities and equipment will likely be required to meet these needs. Table 10-2 below provides estimated ranges of additional sworn officers required in order to maintain the current level of service (1.7 sworn officers per thousand residents), as well as to increase the level of service to be consistent with the region's average (2.7 sworn officers per thousand residents) using future population projections for the City.

Table 10-2
**Police Department
 Projected Personnel Needs**

Year	Forecast Population	Level of Service	Number of Sworn Officers	Additional Sworn Officers Needed
2000	8,574 (Actual)	1.7 (Current)	15 (Actual)	0
		2.7 (Region Average)	23	8
2010	9,072 – 9,283	1.7 (Current)	15 – 16	0 – 1
		2.7 (Region Average)	25	10
2020	9,403 – 10,050	1.7 (Current)	16 – 17	1 – 2
		2.7 (Region Average)	25 – 27	10 – 12

² Federal Bureau of Investigation, Uniform Crime Reports 2002, Section VI Law Enforcement Personnel.

In order to maintain the current level of service of 1.7 sworn officers per thousand residents, the City may need to add one additional sworn officer by 2010 and employ a total of up to 17 sworn officers by the year 2020. To be consistent with the regional average of 2.7 sworn officers per thousand residents, however, the Department would need to add eight additional officers to its current staff and employ a total of 25 sworn officers by 2010 and a total of 25 to 27 sworn officers by the year 2020.

In the Department's effort to analyze the adequacy of its officer staffing, Tecumseh Police Chief Mack Haun conducted a survey of seventeen cities in the area comparable to Tecumseh in size and population. The survey indicated the *average* number of sworn officers employed by the cities surveyed to be 18.5.³ Tecumseh's police force falls short of this average by three to four officers.

The number of officers is not the only level of service indicator used to evaluate the adequacy of policing in a community. It can be important to also take into consideration a department's staff assignments. For example, included in the Department's fifteen total sworn officers is the Police Chief, a D.A.R.E. officer, and a multi-county drug unit officer. So while the Department currently employs fifteen sworn officers, only twelve are regularly available to patrol the streets and respond to calls. With this knowledge, it becomes evident how staff positions can affect the way in which a department operates. It is interesting to note that many of the cities included in the Chief's survey employed an Assistant Chief of Police, which is a position Tecumseh's Department is lacking. The Department also operates without a Detective on the force, the result of which is often a reduction in the number of officers on the streets even further. According to Chief Haun, the Department could benefit greatly from the addition of an Assistant Chief as well as a Detective. At a minimum, there may be an opportunity of fulfilling both positions with the addition of one new officer.

A police department's average response time can also be an important level of service indicator. Quick response times can be critical when responding to a crime in progress. Per Chief Haun, police response times within the City are excellent.

The estimates and guidelines provided in this document are for planning purposes and are intended to assist the City as it prepares for continued growth. Ultimately, it is up to the City to determine what constitutes an adequate level of service including the number of officers necessary to serve the population.

COMMUNITY FACILITIES IMPROVEMENTS

CITY HALL

To better serve the public and meet the needs of the City's administration, future expansion and remodeling of City Hall will likely be needed to accommodate additional office space and make it more user friendly and attractive to the public. The need for expansion, however, may be limited by offering greater access to information and services via the Internet.

³ Phone interview with City of Tecumseh Police Chief, Mack Haun, March 9, 2004.

CEMETERY

While the City does not have immediate plans for the expansion of Brookside Cemetery, it may choose to consider the acquisition of adjacent vacant property to enable future expansion of the grounds. In addition, the need to expand the existing storage building has been identified by the City.

PARKS AND RECREATION

The City of Tecumseh Parks and Recreation Department currently operates a system of sixteen public recreation sites and facilities. The City adopted a five-year Parks and Recreation Master Plan in February of 2001 which identifies desired improvements to existing facilities and programs and includes recommendations for future facilities and the acquisition of additional parkland in the City.

Land Acquisition

The Parks and Recreation Master Plan's evaluation of parks and open space distribution identified a need for additional neighborhood parkland within the northwest and northeast quadrants of the City. The Plan also identified the potential for future parkland acquisition in the City's southeast quadrant due to the availability of undeveloped land and the potential for additional residential development in this area. The Plan indicates an approximate 11.3-acre deficit in community parkland by the year 2010.⁴

Existing and Future Facilities

The following is a summary of the key recommendations regarding existing and future recreation facilities included in the Plan and identified by the Parks and Recreation Department⁵.

- ❖ Construction of a multi-purpose indoor recreation building.
- ❖ Upgrade of existing ball fields at Cal Zorn Park.
- ❖ Construction of a "spray park" at Tecumseh Park.
- ❖ Improvements to Indian Crossing Trails Park including construction of:
 - Nature center
 - Non-motorized boat launch
 - Barrier free fishing pier
 - Barrier free trail system
- ❖ Construction of intra-city pathway system with connections to River Raisin Greenway trails.
- ❖ Reconstruction of existing tennis courts at Community Center.

CIVIC AUDITORIUM

Future improvements to the Tecumseh Civic Auditorium may include both maintenance and expansion projects. To improve safety, reconstruction of the parking area and sidewalks may be necessary. The City may also consider the possibility of developing a Black Box Theatre

⁴ Based on population estimates and projections detailed in the City of Tecumseh Parks and Recreation Master Plan 2001-2006.

⁵ Phone interview with Parks and Recreation Director, Pat Sorise, March 18, 2004.

and Art Gallery on the Civic property to be used for events such as dramas, recitals, lectures, town meetings, business rentals, art showings, etc.

PUBLIC UTILITY PROJECTS

The Department of Utilities is responsible for the treatment and distribution of the municipal water supply as well as the treatment of the City's wastewater. While the City is not planning for any major expansion of its utility services, the Department has identified the following upgrades and minor extension projects that may be needed in the future to better serve the City's residents (refer to Map 10-2).

- ❖ N. Union Street lift station upgrade.
- ❖ Burt Street lift station upgrade.
- ❖ Rogers Highway lift station construction.
- ❖ Burt Street sanitary sewer upgrade.
- ❖ Macon Road sanitary sewer construction.
- ❖ Mill and Mohawk Streets sanitary sewer construction (between Maumee Street and Russell Road).
- ❖ Small Farms Subdivision abandoned railroad right-of-way water and sanitary sewer construction (between Patterson Street and Russell Road).

PUBLIC WORKS PROJECTS

The Department of Public Works is responsible for the construction and maintenance of streets, storm sewers, and City-owned parking lots. In addition to overall City-wide roadway and sidewalk improvements, the following specific large-scale street and storm sewer maintenance and upgrades have been identified as the Department's priority projects over the next ten years (refer to Map 10-3).

- ❖ Occidental road and storm sewer improvements between Sunset and Brown.
- ❖ Evans Street resurfacing between Russell and Kilbuck.
- ❖ Ottawa Street resurfacing.
- ❖ N. Ottawa Street two-way traffic flow conversion between W. Chicago and Shawnee.
- ❖ Ottawa and Evans Street intersection reconfiguration.
- ❖ Maumee Street storm sewer upgrade between Patterson and Russell.
- ❖ E. Kilbuck Street storm sewer upgrade from Maumee Street east to outfall.
- ❖ W. Chicago storm sewer upgrade between Union Street and the River Raisin (in conjunction with MDOT).
- ❖ Maumee, Evans, and Union Street bridges repair and/or replacement of road surface, sidewalks, and railings in accordance with AASHTO standards.
- ❖ Bidwell Street resurfacing and sidewalk and storm sewer installation.
- ❖ Southwestern Drive roadway, storm sewer, and sidewalk reconstruction.
- ❖ W. Cummins Street reconstruction and sidewalk and storm sewer installation between Pearl and Union.



- ❖ Rogers Highway resurfacing between M-50 and Russell.
- ❖ Conklin Drive reconstruction and storm sewer upgrade between N. Occidental and Spring Street.
- ❖ N. Union storm sewer upgrade from Red Maple north to the City limit.
- ❖ Mill and Mohawk Streets reconstruction and storm sewer installation.

STREETSCAPE IMPROVEMENTS

In order to improve traffic and pedestrian circulation and enhance the visual character of the City's main entrances from the north, south, and west, significant streetscape projects are planned for W. Chicago Boulevard and North and South Evans Street. Improvements will likely include implementation of access management techniques, burying overhead utilities, installation and/or improvement of sidewalks, as well as installation of decorative pedestrian and street lighting, landscaping, and other pedestrian amenities.

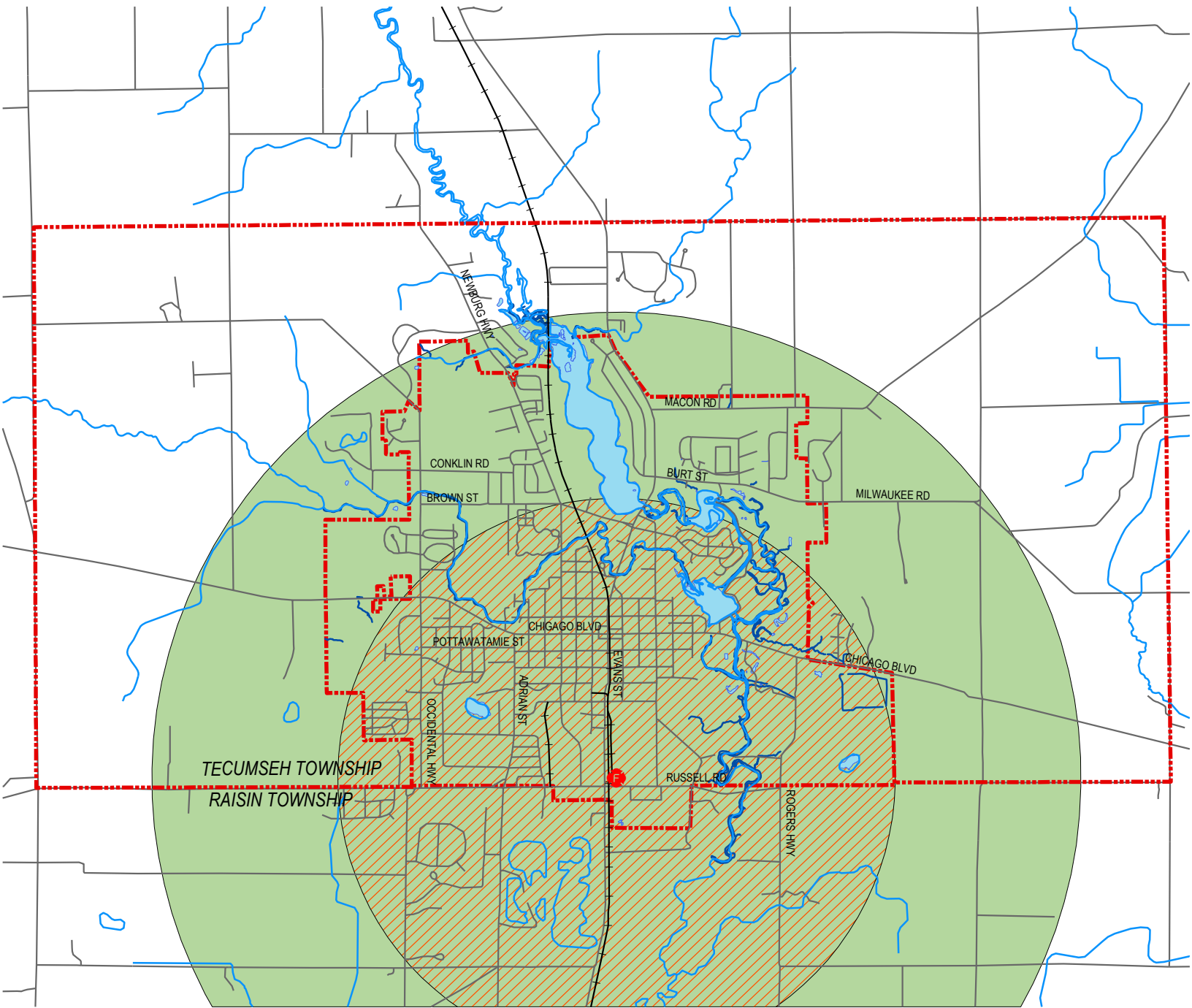
CONCLUSION




As discussed in this chapter, the City has engaged in planning for its future infrastructure, facility, and service needs. The recommendations and priorities of the City's various departments should be coordinated where possible. The City's Comprehensive Plan includes future population projections which allow the City to reasonably prepare for necessary infrastructure improvements, municipal services, and administrative capabilities. The recommendations of the Land Use Plan should assist the City in targeting capital expenditures in a manner consistent with the Plan, enabling the City to manage and direct its future growth and development according to its vision.

CAPITAL IMPROVEMENTS / INFRASTRUCTURE PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing conditions and Goals and Objectives:

- Monitor adequacy of police and emergency services to determine when additional staffing and/or facilities are required.**
- Link the City's Capital Improvements Program (CIP) to the annual budgetary process by making the first year of the CIP match the anticipated capital expenditures for the upcoming fiscal year budget.**
- Schedule public works and utility improvements as necessary to keep pace with increased development and demand.**
- Pursue grant funding for parks and recreation projects in accordance with the adopted Parks and Recreation Plan.**
- When necessary, update the 5-Year Parks and Recreation Plan.**
- Pursue grant funding for priority public works projects.**



-  EMERGENCY SERVICES STATION
-  RECOMMENDED ENGINE COMPANY SERVICE AREA (1.5-MILE RADIUS)
-  RECOMMENDED LADDER COMPANY SERVICE AREA (2.5-MILE RADIUS)

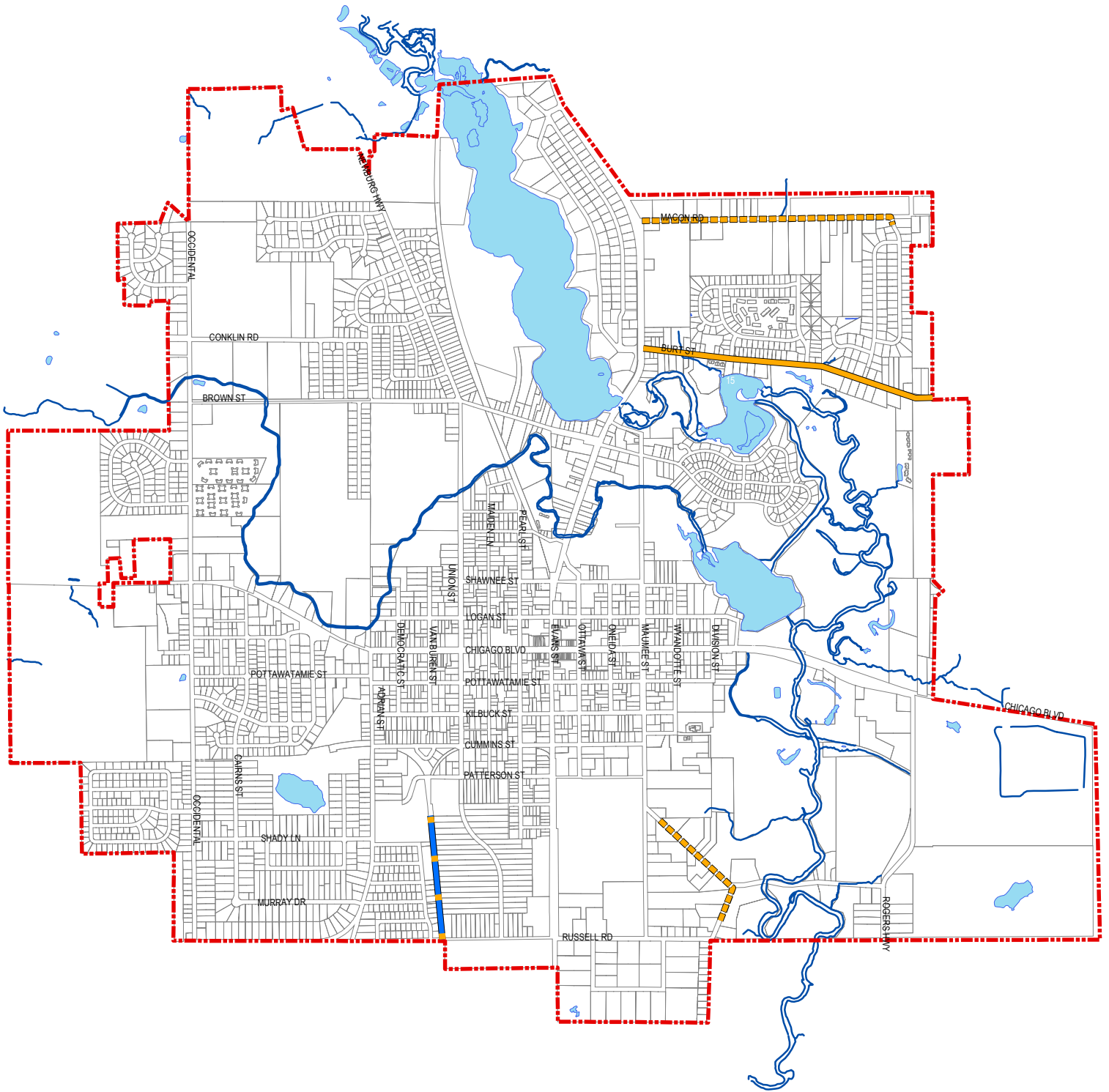
EMERGENCY SERVICES DEPT
 SERVICE AREA ANALYSIS
 CITY OF TECUMSEH
 LENAWEE COUNTY, MICHIGAN
 MAP 10-1




500
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BIRCHLER ARROYO
 ASSOCIATES, INC.

SERVICE AREA RECOMMENDATIONS
 FROM INSURANCE SERVICES OFFICE, INC.
 (REFER TO TEXT FOR EXPLANATION)

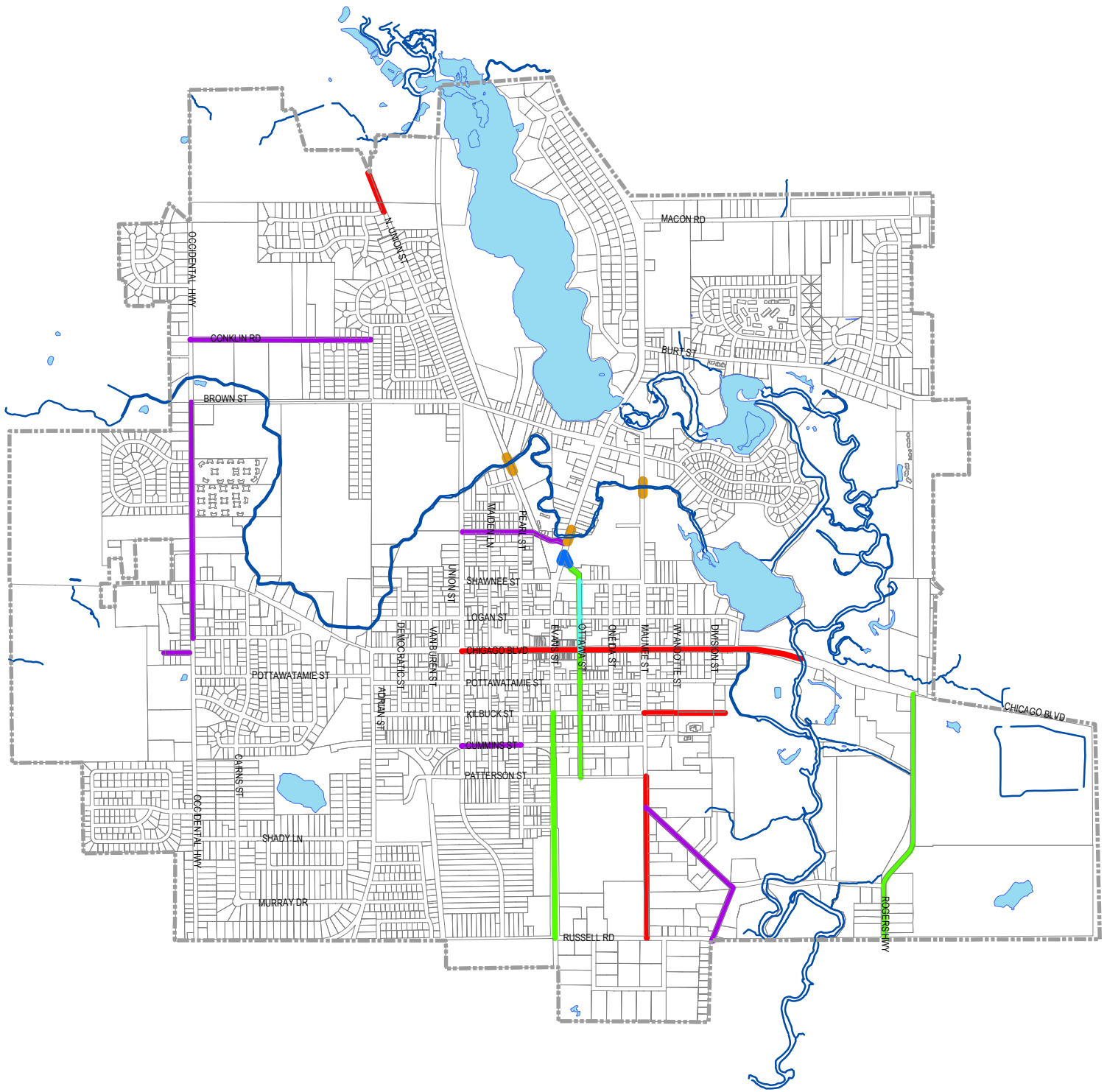


-  Sewer & Water Extension
-  Sewer Extension
-  Sewer Upgrade

**FUTURE
UTILITY PROJECTS**
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP 10-2

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- ROAD RESURFACING/RECONSTRUCTION
- STORM SEWER IMPROVEMENTS
- ROAD AND STORM SEWER PROJECT
- BRIDGE RESURFACING/RECONSTRUCTION
- TWO-WAY STREET CONVERSION
- INTERSECTION RECONFIGURATION

**FUTURE
PUBLIC WORKS PROJECTS
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP 10-3**

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Feet



**BIRCHLER ARROYO
ASSOCIATES, INC.**

11

NATURAL RESOURCES / ENVIRONMENTAL PROTECTION PLAN



The Natural Resources / Environmental Protection Plan provides strategies for protecting the City's resources.

Natural features such as wetlands, woodlands, and waterways help define the City of Tecumseh as much as its beautiful architecture does. These features have been preserved and in some cases enhanced since the City was founded in the Nineteenth Century, but today they are in greater need of protection than ever before. Although many natural areas are preserved as parks, land is becoming scarce within the City. Since it is such a desirable place to live, developers and landowners will want to use as much of the land as possible. It is important that the needs and rights of landowners and new residents be balanced with the very real need to preserve the natural aspects of Tecumseh.

REGULATORY APPROACHES

Although much of its land is developed, the City of Tecumseh is fortunate to have retained a wealth of natural features, as detailed in Appendix Chapter A4. This section discusses strategies for protecting the resources the City already has.

TREE AND WOODLAND PROTECTION ORDINANCE

Woodland growth protects public health through the absorption of air pollutants and contamination, through the reduction of excessive noise and mental and physical damage related to noise pollution, and through its cooling effect in the summer months. Woodlands provide for public safety through the prevention of erosion, siltation, and

flooding. Trees and woodland growth are an essential component of the general welfare of the City by maintaining play areas for children and natural beauty, recreation and irreplaceable heritage for existing and future residents. The protection of such natural resources is a matter of paramount public concern in the interest of health, safety and general welfare of the residents of the City.

The Environmental Residential Community district, mentioned in the Open Space Developments section below, already provides for the preservation of trees on land being developed, but due to the importance of woodlands in Tecumseh and the difficulty of adequately replacing mature trees, the City should consider requiring tree and woodland preservation in all districts. Tree and woodlands preservation ordinances differ, but most prohibit clear-cutting and indiscriminate removal of trees. Before site plan approval can be granted, the developer must submit a tree survey, identifying important trees (for example, all trees with a diameter breast height, or d.b.h. of 6 inches or greater, as well as any conifers over 20 feet in height). A certain percentage of these trees must be preserved regardless of any development activity. Adopting an ordinance of this type would give the City another tool to maintain its woodlands for future generations.

WATERWAY PROTECTION OVERLAY DISTRICT

The Waterway Protection Overlay District includes 150 feet on either side of the River Raisin between Chicago Boulevard and Russell Road. The intent of the overlay district is to provide an area to filter out nutrients and pollution before runoff reaches the river, to maintain vegetation along the river bank to prevent erosion, and to preserve the natural character of the river for wildlife habitat and human enjoyment. Within the Waterway Protection Overlay District, it is recommended that no structures be built and no pavement laid. Some of the area included in the overlay district would be unbuildable in any case, due to the floodplain, but in many instances the district extends beyond the boundaries of the floodplain in order to provide maximum protection for the river.



NATURAL FEATURES IN NEW DEVELOPMENTS

OPEN SPACE DEVELOPMENTS

The City of Tecumseh Zoning Ordinance currently contains provisions for an Environmental Residential Community development. This type of residential development requires the developer to preserve a portion of the land in perpetuity as open space. The Ordinance states that “topography, tree cover, and natural drainage ways shall be treated as fixed determinants of road and lot configurations rather than as malleable elements that can be changed to follow a preferred development scheme.” This type of development can be a valuable tool to help minimize the impact of new development on land that currently exists in its natural state or is used for farming. Since the Environmental Residential Community district may require the developer to incur added expenses in the form of more complicated road layouts and slower construction progress resulting from a more cautious attitude about topography and tree disturbance, it may

be necessary to build in incentives to use this option, rather than simply making it available. Some form of density bonus or expedited review process could help make the Environmental Residential Community option more attractive and thus more frequently exercised. This would hopefully result in more land preserved within the City.

MANAGING STORM RUNOFF

Waterways are very important in the City of Tecumseh. People can easily see and appreciate such features as the two ponds and the River Raisin, but a less visible contributor to water quality in the City is storm runoff. The degree to which runoff is cleansed of pollutants and excess nutrients from fertilizers has a great deal to do with how quickly stormwater enters streams, rivers, and ponds. Stormwater that falls on parking lots, driveways, streets, building roofs, and to some extent, lawns, runs quickly into storm sewers, carrying with it soot, oil, fertilizer, and other harmful elements. In contrast, water that runs into wetland areas stops, at least temporarily, as it soaks into the soil, helping to prevent flooding and erosion caused by fast currents. Wetlands soils filter harmful materials from the water, and wetlands plants use water, further slowing the progress of runoff into streams, rivers, and lakes.

When development and redevelopment occurs in the City of Tecumseh, care should be taken to protect the City's waterways. Impervious surfaces (those that do not absorb water) should be limited as much as possible. The size of parking areas should not be excessive for the needs of the development. In subdivisions, the amount of street surface should be limited by creative arrangement of lots, and street width requirements should be examined to ensure they are not excessive. Wetlands should be preserved where they exist, enhanced where they have become degraded, and consideration should be given to constructing wetlands where they do not currently exist.

The amount of lawn area in a development can also have a significant effect on the amount of runoff produced. Lawns are made up of tightly packed plants. Much of the rain that falls on lawns simply runs off into the street before soaking into the soil. In addition, lawns require care including watering from sprinklers, which contributes to runoff, and applying fertilizer, which pollutes streams and rivers when it is carried away from the site. Preserving more of a development as open space while reducing the size of individual lots can result in a smaller overall lawn area. Planting barrier areas of native shrubs and grasses at the edges of lawns can also enable more water absorption and less runoff into streams. The use of native plants is also advantageous since maintenance and water needs are less substantial. These open space and runoff barrier areas can be approved as part of a site plan or a subdivision plat, and maintained by a condominium or homeowners' association.

SITE PLAN REVIEW PROCESS

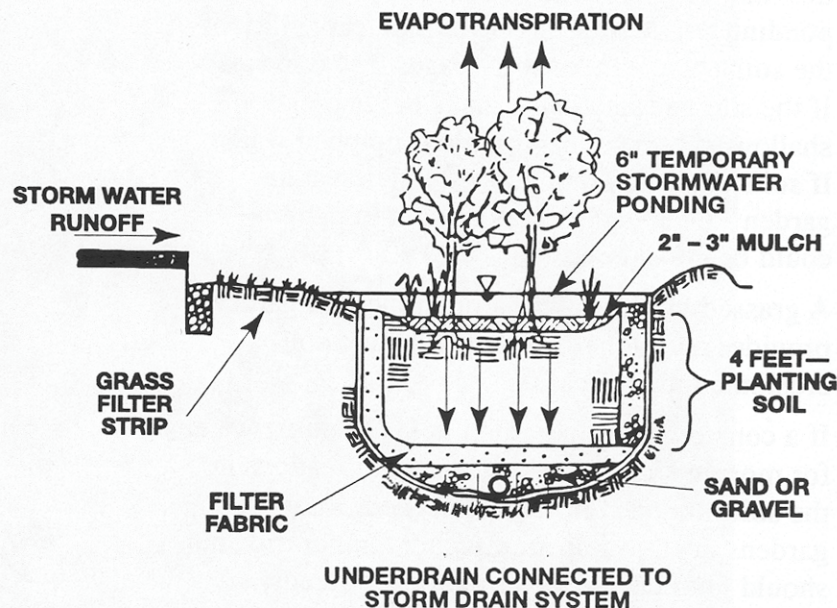
During the site plan review process, woodlands and wetlands reviews should be conducted for all projects in accordance with the zoning ordinance. Landscaping plans should be evaluated for their use of native species. Incentives for open space developments should be applied as described above, and the planning staff and planning commission should encourage developers to minimize lawn area and include runoff barrier plantings.

NATURAL FEATURES IN EXISTING DEVELOPMENTS

Many of the stormwater management techniques appropriate for new development can also be incorporated into existing developments. Homeowners are especially able to affect the amount of stormwater runoff their yards produce through replanting part of the lawn with native grasses and shrubs, using pervious or semipervious driveway materials, conserving roof runoff through the use of rain barrels, or constructing a rain garden as mentioned below.

One technique that is particularly appropriate for existing development is bioretention. Bioretention is a low impact development (LID) and best management practice that uses soils and vegetation to remove pollutants from storm water. These specialized landscape areas are used to filter and store runoff and promote groundwater recharge through infiltration. As shown in Figure 11-1, runoff is conveyed as sheet flow to the treatment area, which can consist of a grass buffer strip, sand bed, ponding area, organic layer or mulch layer, planting soil, and plants. Once runoff reaches the treatment area, water is ponded and gradually infiltrates the bioretention area or is evapotranspired. This reduces the amount of water flowing from the impervious surface into streams and rivers, and reduces the amount of pollution that ends up in those water bodies.

Figure 11-1
Bioretention Area Profile

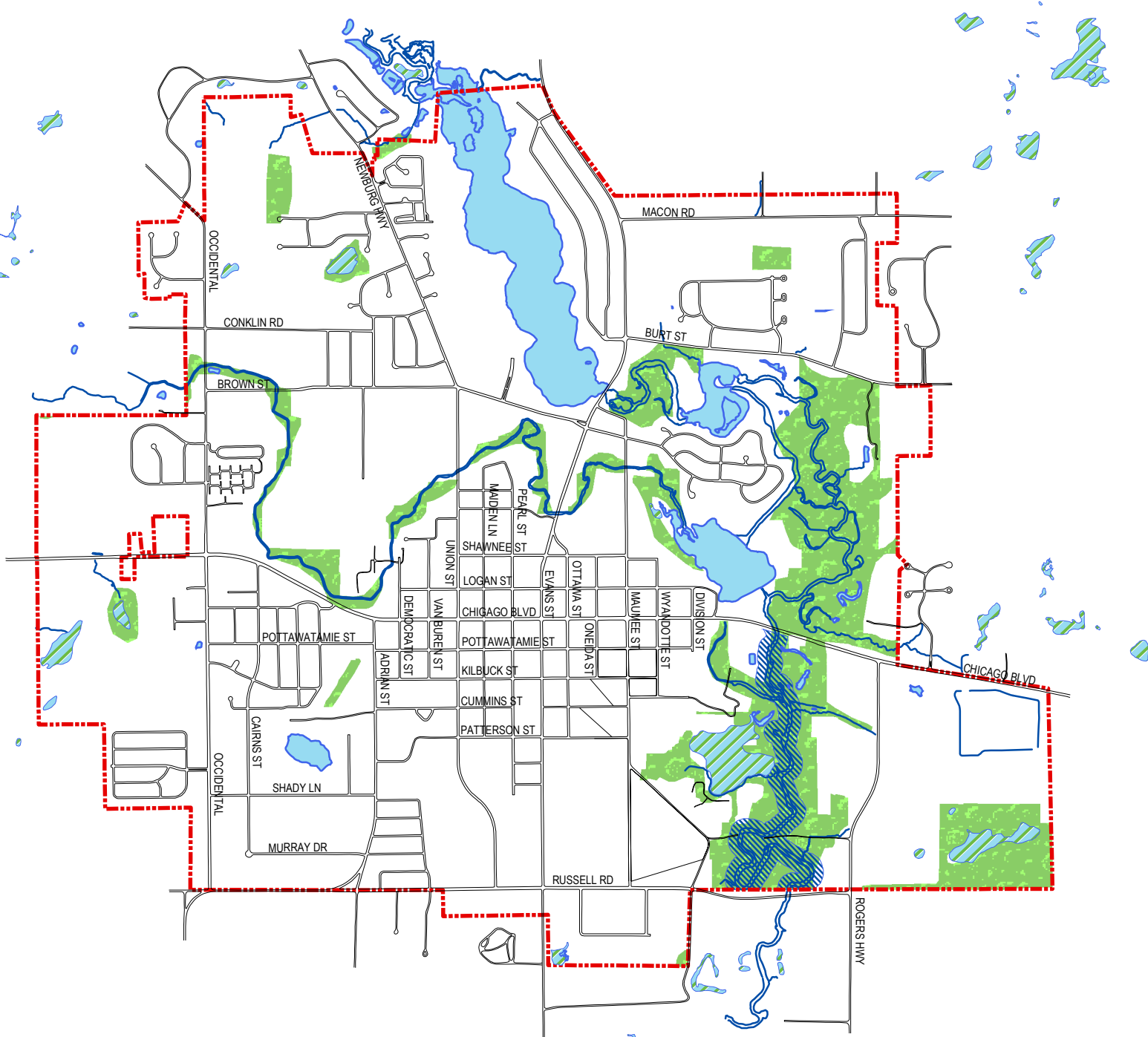


Bioretention areas can be incorporated into the design of median strips, parking lot islands, and swales. Small “rain garden” bioretention areas can be established in landscaped areas in residential and other developments. Bioretention can be incorporated into developed urban areas by retrofitting existing landscaped areas, parking lots, and streetscape improvements.

NATURAL RESOURCES PLAN ACTION ITEMS

The following key recommendations were derived from the examination of existing natural features, national and local trends in natural resource protection, and the Goals and Objectives of the Master Plan:

- Develop a tree and woodland ordinance to protect Tecumseh's vegetation.
- Consider offering density bonuses or expedited review processes for open space developments.
- Amend Zoning Ordinance to include the Waterway Preservation Overlay District.
- Apply and enforce the Waterway Preservation Overlay District.
- For new development, encourage environmentally responsible practices such as the use of native species and minimization of lawn area.
- Ensure that parking and paving standards do not lead to excessive impervious surfaces in the City.
- Consult the Goals and Objectives chapter on a regular basis to ensure that Planning Commission decisions are in keeping with the intent of the Master Plan.



-  LAKES
-  WOODLANDS
-  WETLANDS
-  WATERWAY PRESERVATION OVERLAY DISTRICT

NATURAL FEATURES
 CITY OF TECUMSEH
 LENAWEE COUNTY, MICHIGAN
 MAP 11-1

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 ASSOCIATES, INC.

12

IMPLEMENTATION

Aggressive implementation techniques permit the City to turn potential problems into real opportunities and solutions.



No plan can achieve its full potential unless its recommendations are implemented. The City's thoughtful preparation and adoption of any plan would all be of diminished value without a program of implementation strategies. Aggressive implementation techniques permit the City to turn potential problems into real opportunities and solutions.

ZONING PLAN

The City's Zoning Plan is intended to encourage short-term implementation of the long range land use recommendations included in the Land Use Plan. These short-term actions could be implemented through a City-initiated rezoning of select areas as recommended on the Zoning Plan. Alternatively, private applications for rezoning consistent with the Comprehensive Plan should be given high priority by the Planning Commission and City Council.

The Zoning Plan identifies all areas that would require rezoning to be consistent with the Plan. However, it is not realistic or even desirable that all of these areas would be rezoned in the short term. Instead, the Zoning Plan highlights specific key areas where existing zoning would inhibit development in accordance with the Plan. For example, the area along East Chicago Boulevard between Maumee Street and the river is currently zoned a combination of Central Business and Office Service. The Plan calls for the creation of a new Townhouse Residential District that would line both sides of the street in this area. By

rezoning these properties to a more appropriate zoning district(s), the City can work toward the realization of the type of land uses it wishes to promote in this area, consistent with the Plan. Other areas shown on the Zoning Plan are shown for the Planning Commission to keep in mind as they consider future rezoning requests.

ACTION ITEM TABLE

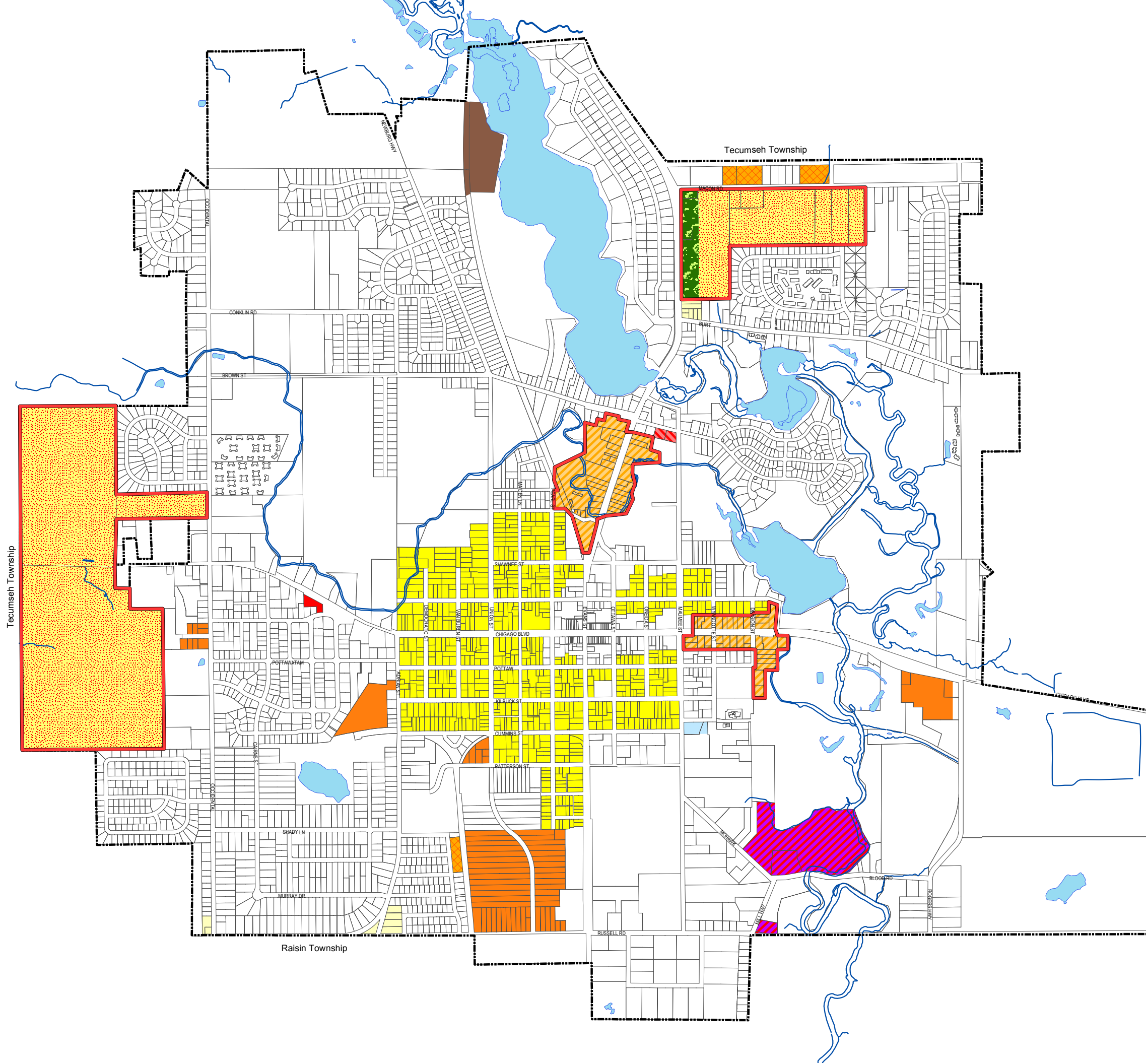
The Action Items found throughout the City of Tecumseh Comprehensive Plan are designed to make implementation straightforward. Carrying out the Action Items will help implement the Plan. For easy reference, the Action Items from the entire Plan are collected into one table that appears on the following page. Throughout the years until the next Comprehensive Plan update, the City should also revisit the Goals and Objectives chapter to make certain that its decisions are consistent with the vision of the Plan.

PRIORITY ACTION ITEMS

Based on the Visioning Session and discussions with the Planning Commission, the following Action Items* have been identified as top priorities for the City:

- Encourage the development of new Senior Housing options.
- Encourage the use of alternative truck routes to protect the downtown area from pollution, noise, road damage, and vibration.
- Consider the purchase of key properties at the North entry of the City, to be developed through a preferred Development Agreement with a private company.
- Amend the Zoning Ordinance to permit the type of attached residential development desired along Chicago Boulevard.
- Consider rezoning the areas targeted for townhouse residential to the new zoning district, which would incorporate appropriate design standards.

*These action items appear as part of other chapters in the Plan.



FUTURE LAND USE CATEGORY

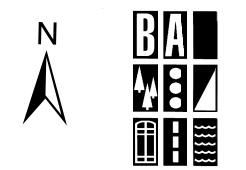
- SINGLE FAMILY RESIDENTIAL
- IN TOWN RESIDENTIAL
- TWO FAMILY
- MULTIPLE FAMILY RESIDENTIAL
- MOBILE HOME PARK
- TOWNHOUSE/ATTACHED SINGLE FAMILY
- FLEXIBLE DEVELOPMENT
- DOWNTOWN MIXED USE
- NEIGHBORHOOD COMMERCIAL
- GENERAL COMMERCIAL
- OFFICE/MEDICAL
- INDUSTRIAL COMMERCIAL
- INDUSTRIAL
- PUBLIC / QUASI-PUBLIC
- RECREATION
- GREENBELT
- PRIORITY REZONING AREAS

NOTES:

1. The Zoning Plan Map indicates where planned future land use is inconsistent with current zoning.
2. These recommendations have a long-range planning horizon and do not necessarily imply that short-term rezoning decisions are appropriate.
3. Priority rezoning areas are those that the Planning Commission should study in the near future to determine if City-initiated rezoning is appropriate.
3. The City of Tecumseh Comprehensive Plan includes the Land Use Plan Map and all text, maps, charts, tables, and other graphics in the full Comprehensive Plan report.

**ZONING PLAN
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN**

MAP 12-1
500 Feet



CITY OF TECUMSEH COMPREHENSIVE PLAN GOALS & ACTION ITEMS

1. COMMUNITY CHARACTER GOAL: Maintain and enhance the unique, historic character of Tecumseh.
2. NATURAL FEATURES GOAL: Protect and enhance the environmental assets in the City, including clean air, water, and soils, as well as woodlands, wetlands, and lakes.
3. RESIDENTIAL GOAL: Provide a variety of housing options for City residents in various stages of their lives, while maintaining the historic character and sense of community in the City of Tecumseh.
4. COMMERCIAL GOAL: Encourage an appropriate mix of new commercial development that is compatible with Tecumseh's character and needs.
5. INDUSTRIAL GOAL: Provide for redevelopment and expansion of the City's industrial base, while mitigating the effects on other uses.
6. TRANSPORTATION GOAL: Provide a transportation network that facilitates the safe and efficient movement of people and goods.

Master Plan Chapter	Action Item*	Goal (s) Addressed	Implementation	
			Short Term	Long Term
5	Rezone land as appropriate to eventually achieve the arrangement shown on the Land Use Plan Map.	3,4,5	X	X
5	Develop Design Guidelines to help builders/developers understand the community's vision for development. Include incentives, such as density and development flexibility, within development options that encourage their use by developers.	1,2,3,4,5	X	
5	Encourage the maintenance and rehabilitation of existing housing stock by assisting residents with grants and other funding.	1,3	X	X
5	Update standards to ensure that parking is sufficient but not excessive. Allow land-banking of parking spaces.	1,4,6	X	
6	Develop site plan review standards that encourage service drives and combined parking and drives.	4,5,6	X	
6	Adopt and implement good access management practices to reduce the number of driveways for individual sites.	4,5,6	X	X
6	Improve problem intersections by means of additional turn lanes, revised traffic controls, and lighting where appropriate.	1,6	X	X
6	Request that MDOT study the M 50 / Occidental Highway intersection to determine if improvements can be made to reduce traffic crashes.	6	X	
6	Work cooperatively with MDOT to provide an efficient and safe system of arterial roadways.	1,4,5,6	X	X
6	Encourage use of alternative truck routes to protect the downtown area from pollution, noise, road damage, and vibration.	1,6	X	X
6	Promote the use of non-motorized facilities.	1,2,6	X	X
7	Provide for an appropriate mix of housing types to meet the needs and lifestyles of City residents, including all categories described in this chapter.	1, 3	X	X
7	Rezone land as appropriate to eventually achieve the arrangement shown on the Land Use Plan Map.	1,2,3,4,5,6	X	X
7	For new development, encourage high-quality architecture that makes the dwelling the visual focus of the structure from the road and obscures accessory uses such as garages.	1,3	X	X
7	For infill and redevelopment, ensure that new structures enhance their neighborhoods rather than detracting from them.	1,3	X	X
7	Encourage the development of new Senior Housing options.	3	X	X
7	Encourage the maintenance and rehabilitation of existing housing stock by assisting residents with grants and other funding.	1,3	X	X
8	Ensure that retail and office uses are appropriate to the design and function of the three distinct commercial centers.	1,4	X	X
8	Plan for residential near the Downtown to bolster its unique pedestrian environment.	1,3,4,6	X	X
8	Continue Economic Development programs that assist with the coordination of business associations to ensure a balanced supply of businesses are locating within the City and the adequate dissemination of information.	1,4	X	X
8	Develop a unique identity for the community, which can act as a springboard for "niche" building programs. Particular emphasis should be placed on the Downtown.	1,3,4,5	X	
8	Target specific businesses for recruitment that are under-served within the Trade Area.	1,4	X	
9	Develop business assistance programs through the Downtown Development Authority, such as Façade Enhancement, Sign Enhancement, and Market Assistance.	1,4	X	
9	Review design guidelines and criteria to address building façade and signage issues (e.g. window style and rhythm, building materials, storefronts and display windows, rear entrances and signage, etc.)	1,4	X	
9	Amend the Zoning Ordinance to address building placement and height issues (i.e. establish a minimum building height and zero side and front setbacks).	1,4	X	
9	Adopt one common color for street furniture, light posts, trash receptacles, etc. to be carried through the entire Downtown streetscape.	1	X	
9	Monitor the long-range plans of the Southern Michigan Railroad with regard to the railroad tracks in the Evans Street corridor.	1,4,6		X
9	Consider purchase of key properties to be developed through a preferred Development Agreement with a private company.	1,4		X
9	Amend the Zoning Ordinance to permit the type of attached residential development desired along Chicago Boulevard.	1,3	X	
9	Consider rezoning areas targeted for townhouse residential to a new zoning district that incorporates appropriate design standards.	1,3	X	X
10	Monitor adequacy of police and emergency services to determine when additional staffing and/or facilities are required.	1	X	X
10	Link the City's Capital Improvements Program (CIP) to the annual budgetary process by making the first year of the CIP match the anticipated capital expenditures for the upcoming fiscal year budget.	1,3,4,5,6	X	X
10	Schedule public works and utility improvements as necessary to keep pace with increased development and demand.	1,3,4,5,6		X
10	Pursue grant funding for parks and recreation projects in accordance with the adopted Parks and Recreation Plan.	1,2	X	X
10	When necessary, update the 5-Year Parks and Recreation Plan.	1,2		X
10	Pursue grant funding for priority public works projects.	1,3,4,5,6	X	X
11	Develop a tree and woodland ordinance to protect Tecumseh's vegetation.	1,2	X	
11	Consider offering density bonuses or expedited review processes for open space developments.	1,2,3	X	X
11	Amend Zoning Ordinance to include the Waterway Preservation Overlay District.	1,2	X	
11	Apply and enforce the Waterway Preservation Overlay District.	1,2	X	X
11	For new development, encourage environmentally responsible practices such as the use of native species and minimization of lawn area.	1,2	X	X
11	Ensure that parking and paving standards do not lead to excessive impervious surfaces in the City.	1,2,4	X	X
ALL	Consult the Goals and Objectives chapter on a regular basis to ensure that Planning Commission decisions are in keeping with the intent of the Master Plan.	1,2,3,4,5,6	X	X

*Action items that are repeated in different chapters or different chapter sections are listed only once in this table.

A1

POPULATION, HOUSING, & ECONOMIC ANALYSIS



Armed with knowledge of historical and current population, housing, and economic trends, the City can establish the most appropriate goals to guide its future development.

As noted in the Regional Setting chapter, both internal and external factors influence the City of Tecumseh's growth and development. It is important to be aware of differences and similarities between the City and other communities that may be influenced by the same factors.

This chapter includes analysis of data relating to trends in the community's population growth and characteristics, housing base, and economy. This information points out areas of growth and decline, the pace of residential construction, and general information about the community's residents. By looking at both local and regional data, the community may examine surrounding region as a whole, as trends that are not yet evident in the City may emerge on a larger scale.

POPULATION

The City of Tecumseh had a 14.9 percent increase in population between 1990 and 2000. As shown in Table A1-1 on the following page, this is a major increase from the previous two decades, in which the City experienced increases in population of 1.9 and 2.8 percent. It is also significantly higher than Lenawee County's 8.1 percent rate of growth between 1990 and 2000.

Between 1990 and 2000, the majority

of the communities near Tecumseh grew in population. The exceptions were the City of Adrian and the Village of Clinton, which both lost population in the last decade. In contrast, Adrian Township experienced the highest rate of growth, with a 32.6 percent increase in population. Franklin, Raisin, and Tecumseh Townships also had higher growth rates between 1990 and 2000 than did the City of Tecumseh.

Table A1-1
Population Comparison
City of Tecumseh and Surrounding Communities, 1970 – 2000

Community	1970 Population	1980 Population	% Change, 1970 - 1980	1990 Population	% Change, 1980 - 1990	2000 Population	% Change, 1990 - 2000	% Change, 1970 - 2000
City of Tecumseh	7,120	7,320	2.8%	7,462	1.9%	8,574	14.9%	20.4%
City of Adrian	20,382	21,186	3.9%	22,097	4.3%	21,574	-2.4%	5.8%
Adrian Township	3,725	4,612	23.8%	4,336	-6.0%	5,749	32.6%	54.3%
Clinton Township	2,540	3,413	34.4%	3,557	4.2%	3,624	1.9%	42.7%
Village of Clinton	-	2,342	-	2,475	5.7%	2,293	-7.4%	-
Franklin Township	1,768	2,463	39.3%	2,473	0.4%	2,939	18.8%	66.2%
Macon Township	1,316	1,430	8.7%	1,421	-0.6%	1,448	1.9%	10.0%
Raisin Township	4,322	5,499	27.2%	5,648	2.7%	6,507	15.2%	50.6%
Ridgeway Township	1,756	1,746	-0.6%	1,572	-10.0%	1,580	0.5%	-10.0%
Tecumseh Township	1,048	1,480	41.2%	1,539	4.0%	1,881	22.2%	79.5%
Lenawee County	81,609	89,948	10.2%	91,476	1.7%	98,890	8.1%	21.2%

Source: United States Census Bureau, 1970 – 2000

Table A1-2 shows the City of Tecumseh’s share of Lenawee County’s population and growth. Since 1970, the City’s share of the County’s population has remained steady, in the range of 8.1 to 8.7 percent. However, the City’s share of the County’s growth has been increasing, from 2.4 percent in 1980 to 9.3 percent in 1990 to 15 percent in 2000.

Table A1-2
City of Tecumseh’s Population Growth as a
Percent of Lenawee County’s, 1970 – 2000

Year	City of Tecumseh Population	Lenawee County Population	Tecumseh Share of County Population	Tecumseh Share of County Growth
1970	7,120	81,609	8.7%	-
1980	7,320	89,948	8.1%	2.4%
1990	7,462	91,476	8.2%	9.3%
2000	8,574	98,890	8.7%	15.0%

In order to properly plan for the City of Tecumseh’s future, it is important to establish a reliable estimate of the City’s population for the next twenty years. A good estimate of the future population will help Tecumseh in determining future infrastructure and municipal service needs.

There are a variety of mathematical methods that can be used to forecast population levels for a community, including the constant proportion and growth rate methods. The constant proportion method uses population projections for Lenawee County to establish future population outcomes for the City of Tecumseh. In the Lenawee County Comprehensive Land Use Plan, which was adopted in 2002, the Region 2 Planning Commission used two methods to generate County population forecasts for 2010 and 2020. According to the linear regression technique¹, the County’s population will be 107,067 in 2010 and 115,921 in 2020. The cohort-survival method² predicts a population of 104,640 in 2010 and 108,452 in 2020. The constant proportion method, which assumes that the City of Tecumseh will maintain the same percentage of the County’s projected 2010 and 2020 population as it experienced in 2000, uses all four County population projections. Table A1-3 shows projections for the City using the constant proportion method and the Region 2 Planning Commission’s projections for the County population.

Table A1-3
Constant Proportion Population Projection
City of Tecumseh, 2010 – 2020

	City of Tecumseh	Lenawee County	Tecumseh's Population as a Percent of County Total
2000 Population	8,574	98,890	8.67%
2010 Projection (Linear Regression)	9,283	107,067	8.67%
2010 Projection (Cohort-Survival)	9,072	104,640	8.67%
2020 Projection (Linear Regression)	10,050	115,921	8.67%
2020 Projection (Cohort-Survival)	9,403	108,452	8.67%

Sources: Region 2 Planning Commission; United States Census Bureau, 2000

Between 1970 and 2000, the City of Tecumseh’s percentage of the County’s population fluctuated by only 0.6 percent, indicating that the constant proportion method may generate an accurate projection of future population in Tecumseh.

The growth rate method assumes that the City’s 2000 – 2010 growth rate will be the same as the growth rate between 1990 and 2000, and the 2000 – 2020 growth rate will be the same as between 1980 and 2000. This method of projection results in the predictions found in Table A1-4.

¹ Linear regression equations reflect the fact that population does not always grow in a straight line with the same incremental increase each year.

² Cohort survival looks at designated age groups and forecasts how the population of each age group will grow or decline throughout the study period.

Table A1-4
**Growth Rate Population Projection
City of Tecumseh, 2010 – 2020**

2000 Population	8,574
1990 - 2000 Percent Change	14.9%
2010 Projection	9,852
1980 - 2000 Percent Change	17.1%
2020 Projection	10,040

This method uses population growth patterns during the previous 20 years to establish population growth predictions for the subsequent 20 years. Because the City of Tecumseh's population growth rate from 1980 to 1990 differed so significantly from its population growth rate from 1990 to 2000, the growth rate method may not be the most accurate predictor of future growth.

In conclusion, the population levels predicted by the constant proportion method may be the most accurate. In 2010, the City of Tecumseh can expect a population ranging between 9,072 and 9,283 people, while the 2020 population will likely range between 9,403 and 10,050 people. For planning purposes, these population levels will be assumed to ensure that adequate levels of service are provided.

HOUSEHOLDS

As discussed above, the City of Tecumseh's population increased by 14.9 percent between 1990 and 2000. During the same time period, the number of households in the community increased by 20.5 percent (see Table A1-5). The number of households in the City increased at a rate that was higher than the rate of population increase. This is consistent with a nationwide trend of decreasing household size. In keeping with this trend, the City's average household size has decreased since 1970, when Tecumseh had an average household size of 3.21 persons per household. Household size decreased to 2.75 in 1980, 2.56 in 1990, and in 2000 it dropped again to 2.42 persons per household. From 1970 to 2000, average household size in the City of Tecumseh was smaller than Lenawee County's average household size (see Table A1-6).

**Table A1-5
Total Households
City of Tecumseh and Adjacent Communities, 1970-2000**

Community	1970 Households	1980 Households	% Change, 1970 - 1980	1990 Households	% Change, 1980 - 1990	2000 Households	% Change, 1990 - 2000	% Change, 1970 - 2000
City of Tecumseh	2,204	2,650	20.2%	2,903	9.5%	3,499	20.5%	58.8%
City of Adrian	6,099	7,241	18.7%	7,479	3.3%	7,908	5.7%	29.7%
Adrian Township	1,006	1,435	42.6%	1,465	2.1%	2,147	46.6%	113.4%
Clinton Township	800	1,204	50.5%	1,295	7.6%	1,395	7.7%	74.4%
Village of Clinton	-	862	-	929	7.8%	925	-0.4%	-
Franklin Township	523	811	55.1%	887	9.4%	1,071	20.7%	104.8%
Macon Township	321	422	31.5%	423	0.2%	502	18.7%	56.4%
Raisin Township	1,201	1,668	38.9%	1,834	10.0%	2,265	23.5%	88.6%
Ridgeway Township	505	533	5.5%	540	1.3%	576	6.7%	14.1%
Tecumseh Township	300	474	58.0%	530	11.8%	672	26.8%	124.0%
Lenawee County	24,239	30,044	23.9%	31,635	5.3%	35,930	13.6%	48.2%

Source: United States Census Bureau, 1970 - 2000

**Table A1-6
Persons per Household
City of Tecumseh and Lenawee County, 1970 - 2000**

	1970 Household Size	1980 Household Size	% Change, 1970 - 1980	1990 Household Size	% Change, 1980 - 1990	2000 Household Size	% Change, 1990 - 2000	% Change, 1970 - 2000
City of Tecumseh	3.21	2.75	-14.3%	2.56	-6.9%	2.42	-5.5%	-24.6%
Lenawee County	3.26	2.91	-10.7%	2.77	-4.8%	2.61	-5.8%	-19.9%

Source: United States Census Bureau, 1970 - 2000

Some factors that have led to a decrease in household size include people having fewer children, a large population of baby boomer “empty nesters”, longer life expectancies, more elderly people staying in their own homes, and people marrying later and living alone for longer periods. The decline in average household size may stabilize in the next decade or two, although the general aging trend of the U.S. population may continue to drive down the average.

HOUSING UNITS

As displayed in Table A1-7, the number of housing units in the City of Tecumseh increased by 21.7 percent during the last decade, which is consistent with the 20.5 percent increase in households reflected in Table A1-5. These figures indicate, again, that the City of Tecumseh is growing at a faster rate than Lenawee County as a whole. However, three other nearby communities are seeing even larger proportional increases in housing units. The number of housing units in Adrian Township increased by 47.8 percent between 1990 and 2000, while the

number of housing units in Raisin and Tecumseh Townships increased by 23.9 and 26.2 percent, respectively.

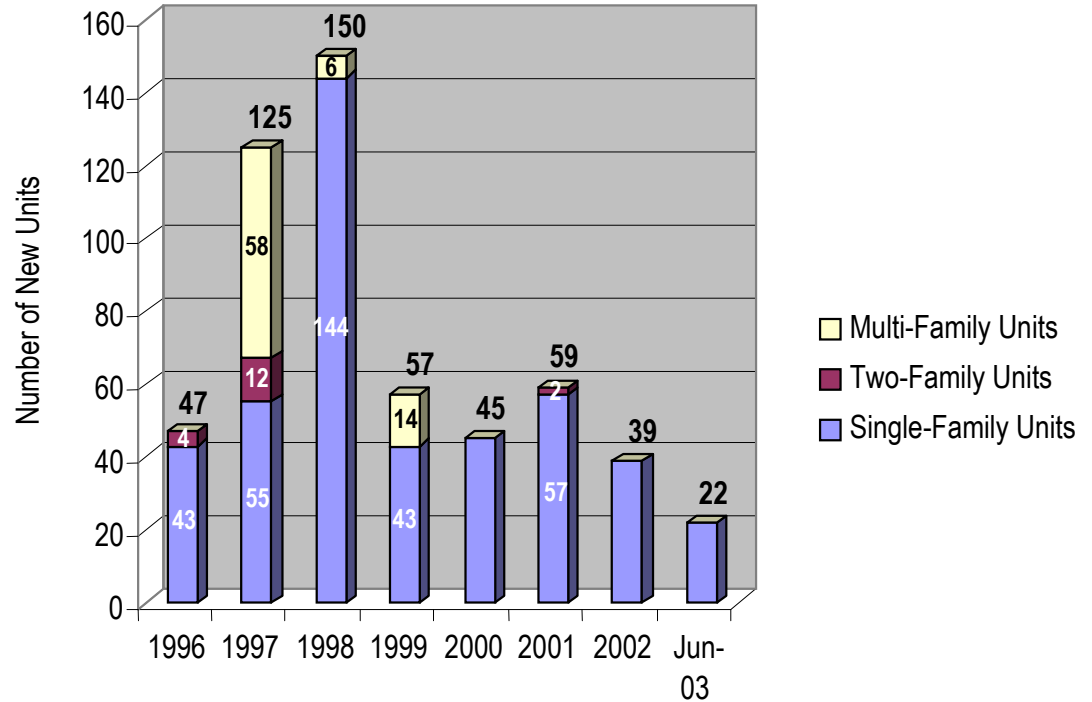
Table A1-7
Total Housing Units
City of Tecumseh and Adjacent Communities, 1990 – 2000

Community	1990 Total Housing Units	2000 Total Housing Units	% Change, 1990 - 2000
City of Tecumseh	2,999	3,651	21.7%
City of Adrian	7,842	8,498	8.4%
Adrian Township	1,505	2,224	47.8%
Clinton Township	1,343	1,448	7.8%
Village of Clinton	960	965	0.5%
Franklin Township	1,094	1,275	16.5%
Macon Township	444	516	16.2%
Raisin Township	1,894	2,347	23.9%
Ridgeway Township	563	591	5.0%
Tecumseh Township	546	689	26.2%
Lenawee County	35,104	39,769	13.3%

Source: United States Census Bureau, 1970 – 2000

According to the 2002 Lenawee County Comprehensive Land Use Plan, 691 residential building permits were issued in the City of Tecumseh between 1990 and 2000. Beginning in 1996, the U.S. Census has annual building permit data available for the City, as shown in Figure A1-1. On average, 75 new housing units were constructed each year between 1996 and 2002. In the seven-and-a-half year period spanning from January 1996 until June 2003, the number of permits issued peaked in 1998, then took a downturn. This downturn is most likely related to the weakening of the economy.

Figure A1-1
**Number of Housing Units for Which Building Permits Were Issued
 City of Tecumseh, 1996 – 2003**



Sources: City of Tecumseh Development Services Department; United States Census Bureau, 1996 – 2003

AGE

In addition to total population, housing units, and household size, it is also important to examine the overall age groupings of a community's population. The overall age grouping provides figures for the number of school-age children, the size of the workforce, and the size of the elderly population. This data can be used for school enrollment projections and planning for recreation facilities, special services for the elderly, and other governmental services. On the next page, Table A1-8 and Figure A1-2 illustrate the age distribution of City of Tecumseh residents in 2000.

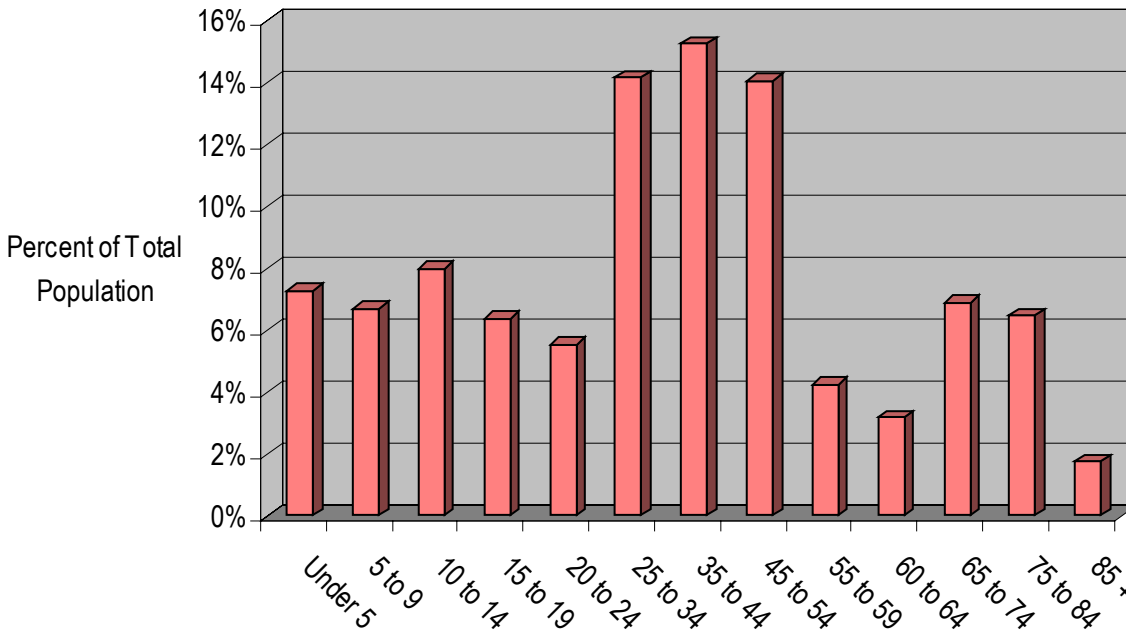
Another way to obtain information about age-related trends in a community is to look at residents' median age. In 1980, the median age in the City of Tecumseh was 30. By 1987, the median age had jumped by two years, and by 1992, the median age was 33.1. As of 2000, the median age in the City of Tecumseh was 36.3, just one-tenth of a year less than Lenawee County's median age. This nationwide trend of an aging population is fueled by a number of factors, including the increase in life expectancy, decline in fertility, and aging of the baby boom generation, who are now in their 40's and 50's.

Table A1-8
Age Distribution
City of Tecumseh, 2000

Age Group	Number of Residents	Percent of Total Population
Under 5	625	7.3%
5 to 9	574	6.7%
10 to 14	687	8.0%
15 to 19	548	6.4%
20 to 24	475	5.5%
25 to 34	1,216	14.2%
35 to 44	1,308	15.3%
45 to 54	1,206	14.1%
55 to 59	365	4.3%
60 to 64	274	3.2%
65 to 74	591	6.9%
75 to 84	555	6.5%
85 +	150	1.7%

Source: United States Census Bureau, 2000

Figure A1-2
Age Distribution
City of Tecumseh, 2000

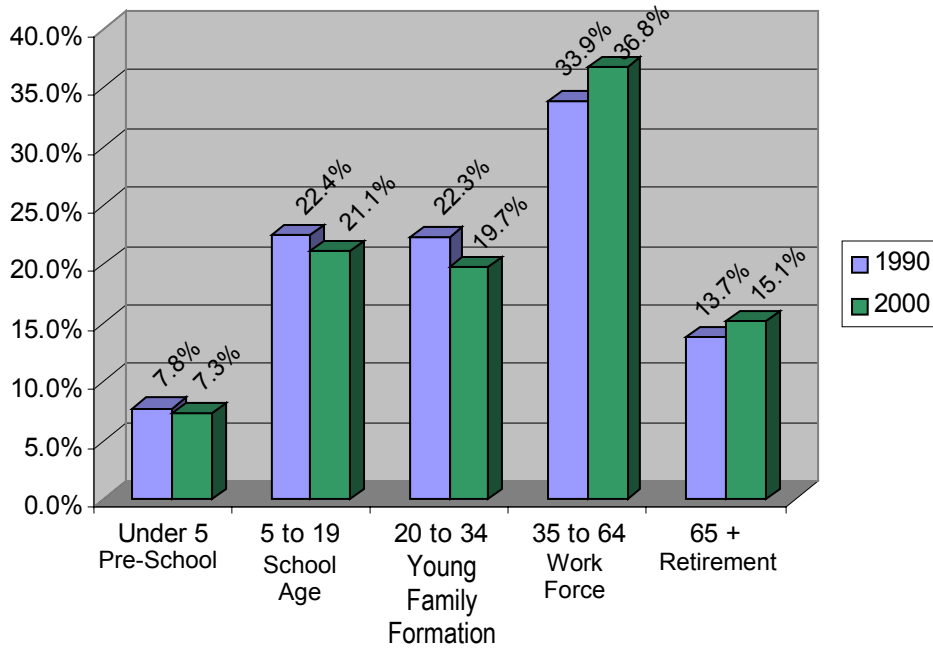


Source: United States Census Bureau, 2000

A third way to examine age distribution is to divide ages into categories based on common life stages. Figure A1-3 on the following page displays the Major Age Groups distribution of City of Tecumseh residents. The five Major Age Groups are Pre-School, School Age, Young Family

Formation, Work Force, and Retirement. As shown in Figures A1-2 and A1-3, these demographics indicate that it is important for the community to plan for the needs of people of all ages. It may be appropriate to place particular emphasis on programs and facilities for families, as 35 percent of City of Tecumseh residents are “parenting” age (20-44) and 28.4 percent are under 20. Since the population is generally getting older, more retirement and assisted living housing may be necessary in the future, as well as programs and facilities for seniors.

Figure A1-3
Major Age Groups
City of Tecumseh, 1990 and 2000



Source: United States Census Bureau, 2000

EMPLOYMENT

As shown in Table A1-9 on the next page, “management and professional” and “sales and office” occupations make up the majority of the total occupations of City of Tecumseh residents. The employment industry distribution in Tecumseh is generally consistent with the distribution of employment in Lenawee County, with a few exceptions. The City exceeds the County in percent of workers in management and professional occupations, while the County slightly exceeds the City in percent of workers in service occupations; construction, extraction, and maintenance occupations; and production, transportation, and material moving occupations.

According to the 2000 U.S. Census, the mean travel time to work for City of Tecumseh residents was 27.5 minutes, which was 2.5 minutes more than the mean travel time for Lenawee County residents as a whole. The Census also reports that as of 2000, City of Tecumseh residents’ commuting patterns were generally in line with commuting patterns of County residents. The majority of City of Tecumseh residents (86.5 percent) drove alone to

their place of work, with a slightly smaller percentage (83.3 percent) of Lenawee County residents doing so. In 2000, the other significant means of transportation to and from work for City residents was carpooling, which accounted for 6.7 percent of all trips. In the County as a whole, the carpool rate was higher – 10.1 percent. Using public transportation to travel to work is not an option for most City of Tecumseh residents; according to the 2000 Census, just 0.3 percent of residents used public transit.

Table A1-9
Employment by Occupation and Industry
City of Tecumseh, 2000

Occupation / Industry	City of Tecumseh	Percent of City Total	Lenawee County	Percent of County Total
Employed civilian population 16 years and over	4,123	--	46,895	--
OCCUPATION				
Management, professional, and related occupations	1,255	30.4%	11,871	25.3%
Service occupations	574	13.9%	6,960	14.8%
Sales and office occupations	1,018	24.7%	11,456	24.4%
Farming, fishing, and forestry occupations	5	0.1%	232	0.5%
Construction, extraction, and maintenance occupations	338	8.2%	4,745	10.1%
Production, transportation, and material moving occupations	933	22.6%	11,631	24.8%
INDUSTRY				
Agriculture, forestry, fishing and hunting, and mining	5	0.1%	814	1.7%
Construction	220	5.3%	2,990	6.4%
Manufacturing	1,187	28.8%	13,440	28.7%
Wholesale trade	74	1.8%	1,016	2.2%
Retail trade	571	13.8%	5,694	12.1%
Transportation and warehousing, and utilities	104	2.5%	1,606	3.4%
Information	39	0.9%	790	1.7%
Finance, insurance, real estate, and rental and leasing	214	5.2%	2,134	4.6%
Professional, scientific, management, administrative, and waste management services	245	5.9%	2,211	4.7%
Educational, health and social services	896	21.7%	9,132	19.5%
Arts, entertainment, recreation, accommodation and food services	235	5.7%	3,180	6.8%
Other services (except public administration)	152	3.7%	2,176	4.6%
Public administration	181	4.4%	1,712	3.7%

Source: United States Census Bureau, 2000

INCOME

Table A1-10 on the next page shows the per capita income and median household incomes of the City of Tecumseh and adjacent communities in 1989 and 1999. From 1989 to 1999, the City of Tecumseh's per capita income increased by 54.2 percent and its median household

income increased by 37.4 percent. In 1999, the City's per capita income was higher than that of five surrounding communities and lower than that of four others. In terms of median household income in 1999, the City outranked one of the surrounding communities, but had a lower median household income than did eight other nearby communities. Relative to the entire County, however, the City of Tecumseh's median household income was higher.

Table A1-10
**Per Capita and Median Household Income
 City of Tecumseh and Adjacent Communities, 2000**

Community	1989 Median Household Income	1999 Median Household Income	% Change, 1989-1999	1989 Per Capita Income	1999 Per Capita Income	% Change, 1989-1999
City of Tecumseh	\$33,545	\$46,106	37.4%	\$14,781	\$22,797	54.2%
City of Adrian	\$24,788	\$34,203	38.0%	\$11,006	\$16,528	50.2%
Adrian Township	\$40,351	\$60,640	50.3%	\$17,254	\$24,881	44.2%
Clinton Township	\$34,601	\$51,661	49.3%	\$13,901	\$21,554	55.1%
Village of Clinton	\$31,569	\$47,961	51.9%	\$13,565	\$20,513	51.2%
Franklin Township	\$40,109	\$56,296	40.4%	\$16,035	\$24,300	51.5%
Macon Township	\$41,937	\$61,818	47.4%	\$13,415	\$24,059	79.3%
Raisin Township	\$37,554	\$57,088	52.0%	\$12,790	\$21,703	69.7%
Ridgeway Township	\$33,514	\$50,642	51.1%	\$12,411	\$19,111	54.0%
Tecumseh Township	\$49,750	\$69,276	39.2%	\$17,959	\$28,398	58.1%
Lenawee County	\$31,012	\$45,739	47.5%	\$12,654	\$20,186	59.5%

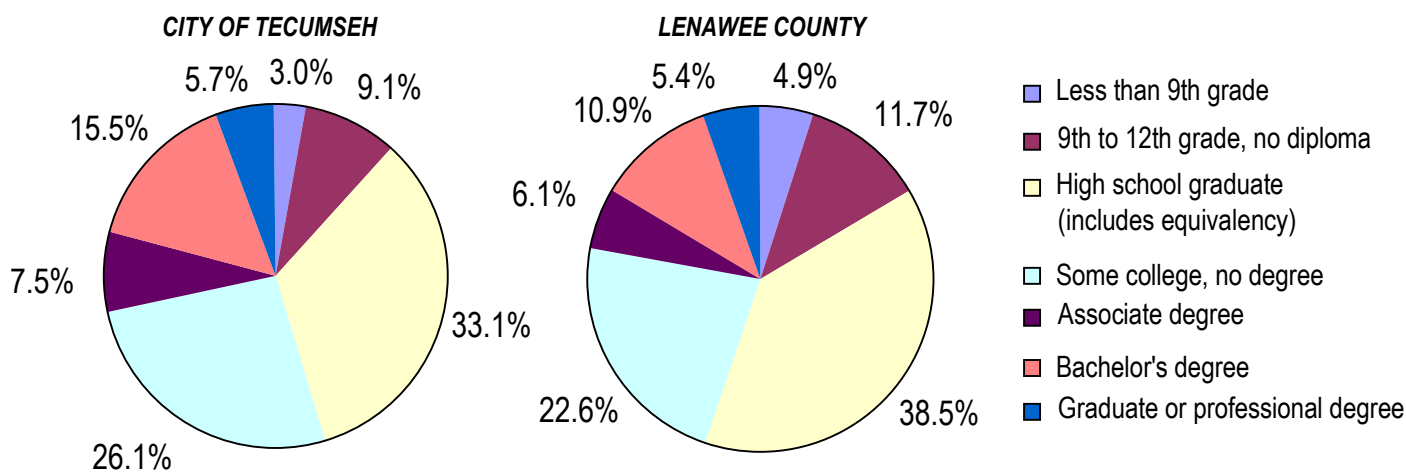
Source: United States Census Bureau, 1990 – 2000

According to the 2000 Census, the unemployment level for the City of Tecumseh was 3.2 percent, which is one-tenth of a percent higher than the reported unemployment rate for Lenawee County as a whole. The Census also reported that 3.5 percent of Tecumseh families were living below the poverty line. This is a lower percentage than that of Lenawee County as a whole, which had a 4.4 percent family poverty rate. It is important to note that recent trends in the national and state economies may have increased the unemployment rates in both the City and the County since the 2000 Census.

EDUCATION

Figure A1-4 shows the educational attainment levels for the City of Tecumseh and Lenawee County. As illustrated, Tecumseh residents generally have a higher level of educational attainment than the County as a whole. For example, 88 percent of City adults have a high school degree or higher, while 83.4 percent of the County's adult population falls into this category. Similarly, the percent of City residents with a bachelor's degree or higher is 21.2 percent, as compared to 16.3 percent of County residents.

Figure A1-4
**Educational Attainment, Population 25 Years and Older
 City of Tecumseh and Lenawee County, 2000**



Source: United States Census Bureau, 2000

RACE

Table A1-11 shows racial composition in the City of Tecumseh and Lenawee County for the years 1990 and 2000. The white population in both the City and the County declined slightly, although both still have relatively small minority populations. On the whole, neither Lenawee County nor the City of Tecumseh experienced any significant change in racial composition between 1990 and 2000.

One aspect of the reporting of racial groups that changed with the 2000 Census was that for the first time, respondents to Census questionnaires could describe themselves as being members of more than one racial group. Therefore, some people who were forced to choose one racial category in the 1990 Census were able to more accurately reflect their backgrounds in the 2000 Census. Based on new reporting options, 1.1% of City residents and 1.5% of County residents described themselves as belonging to more than one racial group.

Table A1-11
**Race and Ethnicity
 City of Tecumseh and Lenawee County, 1990 – 2000**

Race	City of Tecumseh		Lenawee County	
	1990	2000	1990	2000
White	96.9%	95.8%	94.4%	92.5%
Black	0.1%	0.2%	1.6%	2.1%
American Indian and Alaska Native	0.4%	0.6%	0.3%	0.4%
Asian or Pacific Islander	0.9%	0.7%	0.5%	0.5%
Other race	1.6%	1.5%	3.2%	3.0%
Two or more races	--	1.1%	--	1.5%
Hispanic origin	4.2%	4.4%	6.0%	7.0%

Source: United States Census Bureau, 1990 – 2000

A common mistake made in reporting racial data is to add respondents of Hispanic/Latino origin to the other racial categories to calculate the total. Hispanic/Latino is an ethnic/cultural category, not a racial one. People of Hispanic/Latino origin may be of any race, or of more than one race. In 2000, 4.4 percent of City of Tecumseh residents and 7 percent of Lenawee County residents identified themselves as Hispanic/Latino.

CONCLUSION

Historical and current population, housing, and economic trends can be used in various ways to illustrate problem areas of development, identify opportunities for growth and improvement, and provide an indication of probable future needs. An awareness of these trends can enable a community to make the most of its assets while effectively addressing its challenges.

As identified in this chapter, major trends in the City of Tecumseh include the following: the population is growing, having increased by 14.9 percent between 1990 and 2000; the population is aging, with median age having climbed from 33.1 in 1992 to 36.3 in 2000; the amount of residential building activity is increasing, with the total number of housing units having increased by 21.7 percent from 1990 to 2000; and average household size is declining, having decreased from 2.56 to 2.42 persons per household between 1990 and 2000. Additionally, as of 2000, the City of Tecumseh's population had a higher median income, per capita income, and level of education than did Lenawee County residents as a whole; however, the City's unemployment rate was slightly higher than that of the County.

Armed with a knowledge of these trends, the City can establish the most appropriate goals to guide its future development.

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A2

EXISTING LAND USE INVENTORY

An existing land use survey gives a general overview of the current land uses in the City. If good records are kept, the City can track the progress of development over many years.



An updated existing land use inventory provides a base from which current and long-range planning recommendations and decisions can be made for the City of Tecumseh. By updating the parcel-by-parcel inventory of land use, the City is able to analyze current conditions and make comparisons with past studies to identify changes and trends in the City's land use.

A field survey of existing land use in the City of Tecumseh was carried out in September of 2003. A team of planners conducted a "windshield survey" by driving all of the streets in the City. The use of each parcel was recorded on a base map. Once the land uses for all of the parcels in the City were identified, Geographic Information Systems (GIS) software was used to create a land use database that could be linked to the City's parcel map. Map A2-1, the result of this process, illustrates the land use of each individual parcel, as well as the overall land use pattern throughout the City.

It is important to note that an Existing Land Use map is intended to reflect the actual current use of the land, not the zoning classification or the Master Plan designation.

LAND USE CLASSIFICATIONS

The following is a description of the various land use classifications used in the survey.

SINGLE FAMILY RESIDENTIAL



This category includes single family dwelling units and accessory structures. There are many types of single family residences in the City of Tecumseh, from large historic Victorian homes to 1950's-style ranch houses to new houses still being built as this plan is written. Single family homes comprise thirty-three percent of the total land area of the City.

TWO FAMILY RESIDENTIAL

This category includes structures built as duplexes, single family homes that have been converted to two units, and accessory structures. This use makes up approximately fifteen acres of land in the City, which is less than one percent of the total land area of the City.



MULTIPLE FAMILY RESIDENTIAL

This category includes all apartments or multi-plex units where three or more separate dwelling units occupy one or more buildings on a lot. In the City of Tecumseh, many multiple family uses occur in buildings that have been converted from single-family residences. Other multiple family uses include condominiums with three or more attached units, such as some parts of the Meadowlands development and traditional apartment buildings

such as the Conklin Estates Apartments and Stonehenge Apartments. Multiple-family residential uses make up approximately three percent of the City's total acreage.

MOBILE HOME PARK

This category includes planned mobile home parks and their related accessory buildings and recreational areas. The Newburg Meadows Mobile Home Park on Newburg Highway occupies approximately twenty-seven acres¹, which is about one percent of the total land area of the City.



¹ A portion of the Mobile Home Park site is currently used for Mobile Home and RV sales. This portion was estimated and excluded from the acreage attributed to the Mobile Home Park.

DOWNTOWN MIXED-USE



This category includes buildings along the stretch of Chicago Boulevard that makes up the “traditional downtown” of the City of Tecumseh. Typically, these buildings contain a retail or service use on the first floor and office or residential uses on the second and/or third floors. This unique commercial area makes up approximately six acres, less than one percent of the City’s total land area.

COMMERCIAL

This category includes retail sales and service establishments that are not located within the traditional downtown area. Commercial uses make up approximately two percent of the City.

OFFICE

This category includes office uses such as doctors and dentists, legal, accounting, and similar professions, real estate, insurance, and sales and business offices. The Office category includes traditional office buildings as well as other structures that have been converted to office uses, such as the dentist’s office at right. About thirteen acres of land, less than one percent of the City’s land area, are devoted to office uses.



INDUSTRIAL

Uses with or without buildings where materials are processed, fabricated, assembled, or manufactured; or where equipment, materials, or wastes are stored out-of-doors are classified as industrial. Industrial uses account for about seven percent of the City’s total land area.



PUBLIC

This category includes public uses such as schools, government buildings and public parking areas. Public uses make up approximately eight percent of the City’s total land area.

QUASI-PUBLIC

This category includes uses owned by private, non-profit, or religious entities that provide public services. Churches are an example of a Quasi-public use, as are properties owned by service organizations and clubs such as the American Legion or the Knights of Columbus. Quasi-public uses make up approximately six percent of Tecumseh’s land area.



RECREATION

Recreational uses include public and private outdoor recreation areas such as playgrounds, picnic areas, camps, sports fields, and the like. Recreational uses account for approximately eleven percent of the City's land area.



AGRICULTURAL

This category includes all land area used for crops and permanent pasture land. If the parcel appeared to have been farmed in the last few years, though not within the last growing season, it was assumed to be lying fallow and included in this classification. Agricultural uses make up approximately twelve percent of the City's land area.

UTILITY

This category includes power and gas lines, gas compressor stations and production facilities, telephone switching stations, and electricity sub-stations. Utilities take up approximately two acres, less than one percent of the City's land area.

VACANT AND OPEN SPACE

Included in this category are woodlands, permanent open space and temporarily vacant land. Fifteen percent of the City of Tecumseh's land was categorized as vacant or open space.



HOSPITAL

This category includes Herrick Memorial Hospital and its accessory ambulance facility. These uses occupy approximately nine acres, less than one percent of the City's total land area.

AIRPORT

Included in this category are airports, airfields, hangars, and other uses accessory to an aviation facility. Most of Meyers Airport is actually located in Tecumseh Township; the parcels in the City occupy approximately four acres, which is less than one percent of the City's land area.



CURRENT LAND USE INVENTORY

The acreages and percentages mentioned above were calculated for the different land use categories using GIS. The following table summarizes the categories of land use in the City.

Table A1-1
Existing Land Use
City of Tecumseh, 2003

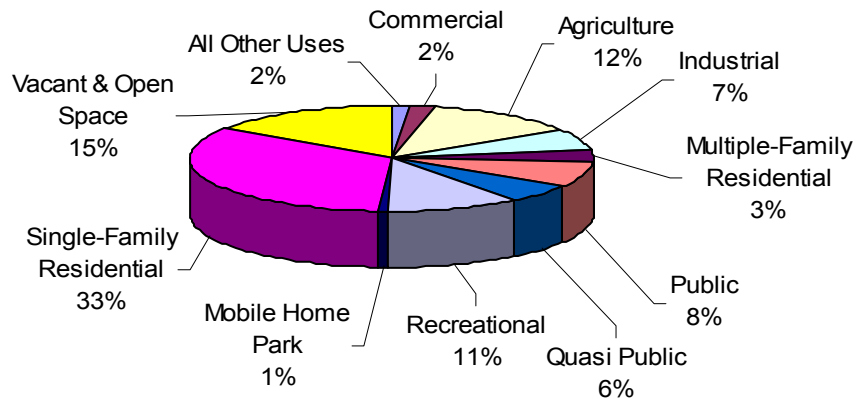
Land Use	Total Acreage	Percent of Total Land Area
Single-Family Residential	1059	33%
Two-Family Residential	15	0%*
Multiple-Family Residential	105	3%
Mobile Home Park	27	1%
Office	13	0%*
Commercial	64	2%
Downtown Mixed Use	6	0%*
Industrial	216	7%
Public	255	8%
Quasi Public	181	6%
Hospital	9	0%*
Airport	4	0%*
Utility	2	0%*
Recreational	342	11%
Agriculture	392	12%
Vacant & Open Space	490	15%

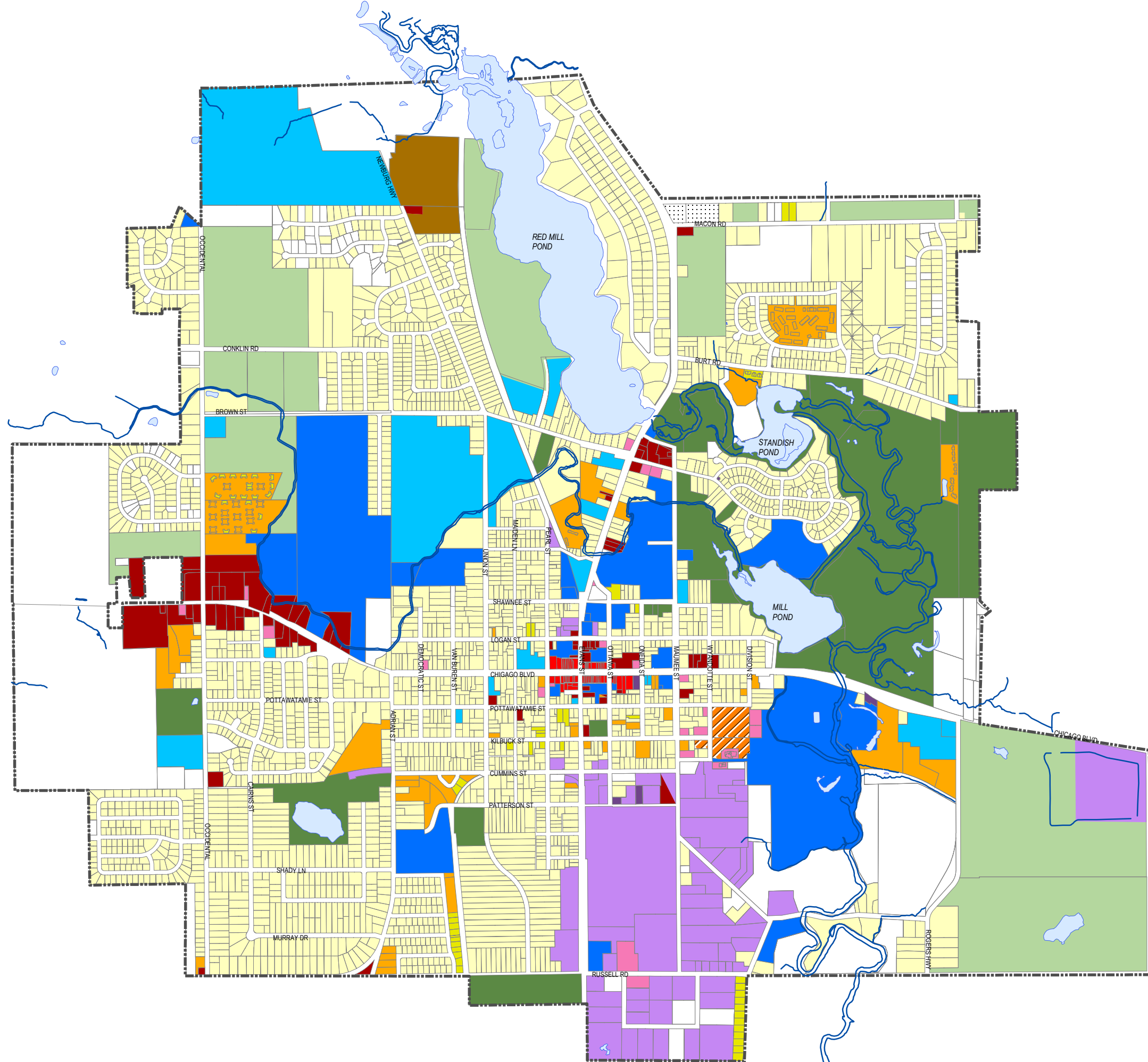
* Less than 1% of total land area

CONCLUSION

As shown in the pie chart on the following page, the 2003 existing land use analysis documents the predominance of Single-Family Residential uses in the City. Vacant and Open land makes up about fifteen percent of the Tecumseh's land area, while Agricultural and Recreational land uses each make up about one eighth of the City's land. The other significant land uses are Public, Industrial, and Quasi-Public, although together they only account for about twenty percent of the City's land. All other land uses combined make up less than ten percent of the City's land area.

Figure A1-1
Percent of Total Land Use
City of Tecumseh, 2003





- RECREATION
- AGRICULTURE
- VACANT / OPEN SPACE
- SINGLE-FAMILY RESIDENTIAL
- TWO-FAMILY RESIDENTIAL
- MULTIPLE-FAMILY RESIDENTIAL
- MOBILE HOME PARK
- PUBLIC
- QUASI-PUBLIC
- OFFICE
- DOWNTOWN MIXED USE
- COMMERCIAL
- INDUSTRIAL
- HOSPITAL
- AIRPORT
- UTILITY

EXISTING LAND USE, 2003
 CITY OF TECUMSEH
 LENAWEЕ COUNTY, MICHIGAN
 MAP A2-1



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A3

COMMUNITY FACILITIES INVENTORY

Quality public services and facilities can contribute to a community's reputation as a desirable place to live.



A municipality's public facilities and services are an important part of community life and are directly related to important issues such as quality of life and protection of the public health, safety and welfare. Quality public services and facilities can contribute to a community's reputation as a desirable place to live. The establishment and maintenance of public buildings, community services, libraries, schools, and recreation facilities can greatly influence the impressions formed by residents, business patrons, and visitors.

Quasi-public facilities, especially passive and active recreation uses, also contribute to the fabric of the community. Many of these uses are available to the public, but may be privately owned.

The location and capacity of a community's utility services can have a significant impact on its future development patterns. Developing an inventory of Tecumseh's existing community facilities and services is an important precursor to the Capital Improvements / Infrastructure component of the Township's Master Plan. Review of this inventory is essential in order to develop long-range planning recommendations and decisions that will provide for adequate public facilities, services and utilities for the City's future.

COMMUNITY FACILITIES

CITY HALL

Tecumseh City Hall is located at 309 E. Chicago Boulevard. It houses the City Assessor,

Treasurer, Clerk, Department of Development Services, Department of Utilities, and Police Department.

ECONOMIC DEVELOPMENT OFFICE

The Economic Development Office is located at 112 South Ottawa Street. Formed in 1996, the Economic Development Department consists of a director, a marketing coordinator, and an office supervisor. The Department works with area stakeholders including the Tecumseh Downtown Development Authority, the Chamber of Commerce, the Central Business Association, and Lenawee Tomorrow to enhance the City's business districts and assist local businesses.



SCHOOLS

The Tecumseh Public School District serves students from an 80-square mile area including the City of Tecumseh and parts of the Townships of Adrian, Clinton, Franklin, Macon, Raisin, Ridgeway and Tecumseh. The district includes four elementary schools (Herrick Park, Patterson, Sutton, and Tecumseh Acres), Tecumseh Middle School, Tecumseh High School, and Options High School (an alternative high school). A major recent development in the Tecumseh Public Schools was the opening of the new thirty-three million dollar high school for the 2001/2002 school year.



Standard & Poor's School Evaluation Services summarizes Tecumseh Public Schools'

performance as providing "moderately above-average student results with moderately below-average spending per student."¹ Tecumseh Public Schools exceed the state average in MEAP passing rate, MEAP excelling rate, and MEAP participation. The graduation rate of 91.9 percent slightly exceeds the state average of 88.8 percent. Operating expenditures per student are \$6,892, while the state average is \$7,733. According to Standard & Poor's, the current enrollment of the district is 3,333 students.



LIBRARY

More information about the Tecumseh District Library will be added to this section following the outcome of the millage in October, 2003. The Tecumseh District Library is located at 215 North Ottawa Street. It serves a population of over 17,000 people, with over 11,000 active

¹ Standard & Poor's School Evaluation Services. August 11, 2003. <http://www.ses.standardandpoors.com>.

borrowers. The library is an independent entity; that is, it does not have any branches nor does it belong to any larger system. The library collection contains over 50,000 print, audio, video, and electronic resources. The Tecumseh District Library is a member of the Woodlands Library Cooperative, which allows it to lend and borrow books from other libraries to extend its collection. The library is overseen by a seven-member Board of Trustees.

CIVIC AUDITORIUM

Given to the City by an anonymous donor in 1981, the Tecumseh Civic Auditorium seats an audience of 572. In addition to use by local groups such as the Tecumseh Players, Tecumseh Community Chorus, and Tecumseh Pops Orchestra, the Civic Auditorium hosts regional and national-level music, dance and drama performances. The Civic Auditorium is funded through a combination of private donors, commercial sponsors, and volunteers who donate their time to build sets, usher, and run the box office.



RECREATION FACILITIES

The City of Tecumseh's Parks and Recreation Department is administered by the Parks and Recreation Director and the Advisory Board. The Director and Board report to and advise the City Council and City Manager on matters having to do with Parks and Recreation. The Advisory Board meets quarterly. The Department also employs a supervisor, secretary, maintenance person, and several part time staff members. The Department is also enhanced by the time and talents of many volunteers.

The offices of the Parks and Recreation Department are located at the historic Hayden-Ford Mill, at 703 E. Chicago Boulevard. The Mill also houses the Tecumseh Community Center and a fitness center operated in partnership with Herrick Memorial Hospital. Various rooms in the Mill are available to rent for events.

In addition to the Community Center, the City of Tecumseh Parks and Recreation Department currently operates a system of fifteen public recreation sites or facilities including:

- ❖ Adams Park
- ❖ Beardsley Park
- ❖ Cal Zorn Recreation Center
- ❖ Douglas W. Bird Kiwanis Memorial Park
- ❖ Elliott Park
- ❖ Evans Park
- ❖ Hotrum Promenade Park
- ❖ Indian Crossing Trails
- ❖ Satterthwaite Park
- ❖ Standish Dam
- ❖ Tecumseh Park
- ❖ South Occidental Park
- ❖ Occidental Street Site



- ❖ Patterson Park
- ❖ Sunset Park

Including school grounds, almost 300 acres of parks and recreation land are available to City residents.² Year round recreation programming administered by the Parks and Recreation Department includes activities and sports for children of all ages, adults, and families. Map A3-2 depicts the location of the Parks and Recreation facilities.

Private recreational facilities near the City of Tecumseh include but are not limited to three golf courses, Indian Creek Camp and Conference Center, a bowling alley, the Fitness Connection in the Hayden-Ford Mill, and Skydive Tecumseh. Located in nearby Brooklyn is the Michigan International Speedway, the state's premier NASCAR racing facility.

The City of Tecumseh adopted its five year Parks and Recreation Master Plan on February 15, 2001. The plan identifies desired improvements to existing public recreation facilities and programs and includes recommendations for the acquisition of additional parkland and the improvement of several existing parks. The complete recommendations of the adopted Recreation Plan will be reviewed and considered for incorporation into the Capital Improvements / Infrastructure component of the City's Master Plan.

HISTORICAL MUSEUM AND DESIGNATED HISTORICAL SITES

Located at 302 E. Chicago, the former St. Elizabeth's Catholic Church houses the Tecumseh Historical Museum. The City of Tecumseh owns the building, but its operations and maintenance are performed by the Tecumseh Area Historical Society. The City of Tecumseh is home to a remarkable number of individual historic buildings and two historic districts. The City has ten listings on the National Register of Historic Places, as well as other buildings designated as historically significant by the State of Michigan. The City of Tecumseh has designated many additional buildings as local Landmarks. Tecumseh's historic structures and districts are vital to the City's character. The historic sites appear in Table A3-1 on the following page and in Map A3-3.



²City of Tecumseh Parks and Recreation Master Plan 2001-2006.

Table A3-1
Tecumseh Historic Sites

Site Name	Year Built	Designation
Peter R. Adams House	c. 1830s	National
Elijah Anderson House	1832	State
Bidwell Building	1852	State
Brookside Cemetery	1853	State, National
Dr. Samuel Catlin House	1866-1867	State, National
Musgrove Evans House	1826	State, National
Joseph E. Hall House	1870	State, National
William Hayden House	1862	State, National
George J. Kempf House	1905	State, National
St. Elizabeth's Church	1913	State, National
G. P. Sparks House	1883	State, National
Downtown Tecumseh Historic District	mid-late 1800s	State, National
Tecumseh Historic District (with 1990 increase)	1830-1900s	State, National
Samuel W. Temple House	1866	State, National

Sources: Michigan Historic Sites Online; National Register of Historic Places

RELIGIOUS AND SPIRITUAL FACILITIES AND CEMETERIES

As shown in Table A3 -2 below, approximately twenty religious and spiritual facilities are located within the City of Tecumseh. Several other religious facilities are located in the surrounding townships just outside the City limits.

Tecumseh Evangelical Friends Church was and still is the community's Quaker congregation. One prominent Tecumseh resident who attended this church was Perry Hayden of the Hayden Milling Company. During and immediately after World War II, Hayden organized a community tithing project to set aside wheat to help the farmers and people of Europe whose lives and food production systems had been disrupted by the war. Henry Ford became a supporter of this project.

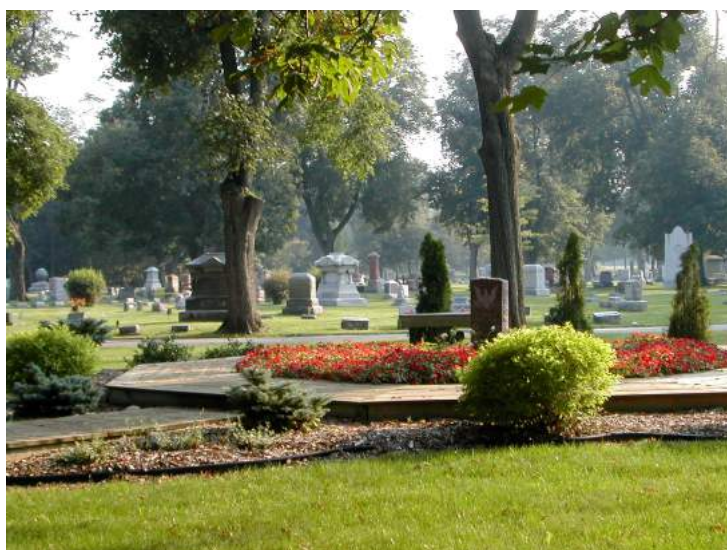
First Presbyterian Church of Tecumseh completed its first building in 1837, the same year Michigan became a state. This building was later sold to the Friends congregation when the Presbyterians built a new meeting house. The current church, the congregation's third building in Tecumseh, is an impressive structure located on Chicago Boulevard. The congregation has been meeting continuously for over 175 years.

St. Elizabeth's Catholic Church, built in 1913, was a relative newcomer to mostly Protestant Tecumseh; the original building is now listed on the National Register of Historic Places and is the location of the Tecumseh Historical Museum. St. Elizabeth's Church is now located nearby and continues to provide a place of worship for local residents.

Table A3-2
Religious Facilities

Facility	Location
Church of God	202 S. Van Buren
Church of Jesus Christ of Latter Day Saints	106 W. Kilbuck
Church of the Nazarene	1001 N. Union St.
First Baptist Church of Tecumseh	5290 Milwaukee Rd.
First United Presbyterian Church	211 W. Chicago Blvd.
Gloria Dei Lutheran Church, ELCA	108 Brown St.
Grace Bible Church	1900 W. M-50
Grace Lutheran Church, Wisconsin Synod	302 N. Maumee St.
Jehovah's Witnesses	2750 W. Monroe Rd.
Lower Light Church	217 S. Maumee St.
Ridgeway Baptist	7758 E. Monroe Rd.
St. Elizabeth Catholic Church	506 Union St.
St. Peter's Episcopal	313 N. Evans St.
Tecumseh Assembly of God	1080 E. Chicago
Tecumseh Church of Christ	213 W. Chicago
Tecumseh Evangelical Friends Church	9500 Tecumseh Clinton Hwy.
Tecumseh Free Will Baptist	5876 Occidental
Tecumseh Grace Mission	Tecumseh-Macon Rd.
Tecumseh Missionary Baptist, SBC	28 Cairns St.
Tecumseh Seventh-Day Adventist	6759 S. Occidental
Tecumseh United Methodist	605 Bishop Reed Dr.

Source: Tecumseh Area Chamber of Commerce



Brookside Cemetery, located at 509 North Union Street, is the only cemetery located within Tecumseh City limits. It is listed on the National Register of Historic Places. An example of the design of the mid-to-late 19th Century rural cemetery movement, which sought to make cemeteries beautiful, “natural” outdoor spaces, Brookside Cemetery is the location of the graves of many of Tecumseh’s founders. It is still in operation.

PUBLIC SERVICES

POLICE

The City of Tecumseh Police Department is located at City Hall, 309 E. Chicago Boulevard. The department includes fifteen sworn officers, one animal control and enforcement officer, two full-time and several part-time dispatchers. The City of Tecumseh Police Department handles a large variety of issues that affect City of Tecumseh residents, from lost dogs and abandoned vehicles to serious crimes such as robbery and assault. The police station is open from 8 a.m. to midnight, but officers of the City of Tecumseh Police department provide service to residents twenty-four hours a day.



EMERGENCY SERVICES DEPARTMENT

The City of Tecumseh Emergency Services Department consists of about twenty paid on-call firefighters and paramedics and four full time Firefighter/EMTs. The Emergency Services



Department is equipped with two fire engines, one fire engine ladder truck, one tanker, one light rescue vehicle, one brush truck, and a chief's car. In addition to fire and emergency medical services, the Department maintains and operates severe weather warning sirens. In 2001, the Emergency Services Department answered 597 service calls.

MEDICAL SERVICES

The residents of the City of Tecumseh are served by Herrick Memorial Hospital. The 100-bed full service hospital is part of the Toledo-based Promedica Health System and also part of the Lenawee Health Alliance with Bixby Medical Center in Adrian.

EXISTING UTILITIES

MUNICIPAL WATER

The City of Tecumseh's municipal water comes from seven wells around the City. The Utilities Department is responsible for drinking water treatment, distribution and wastewater treatment.

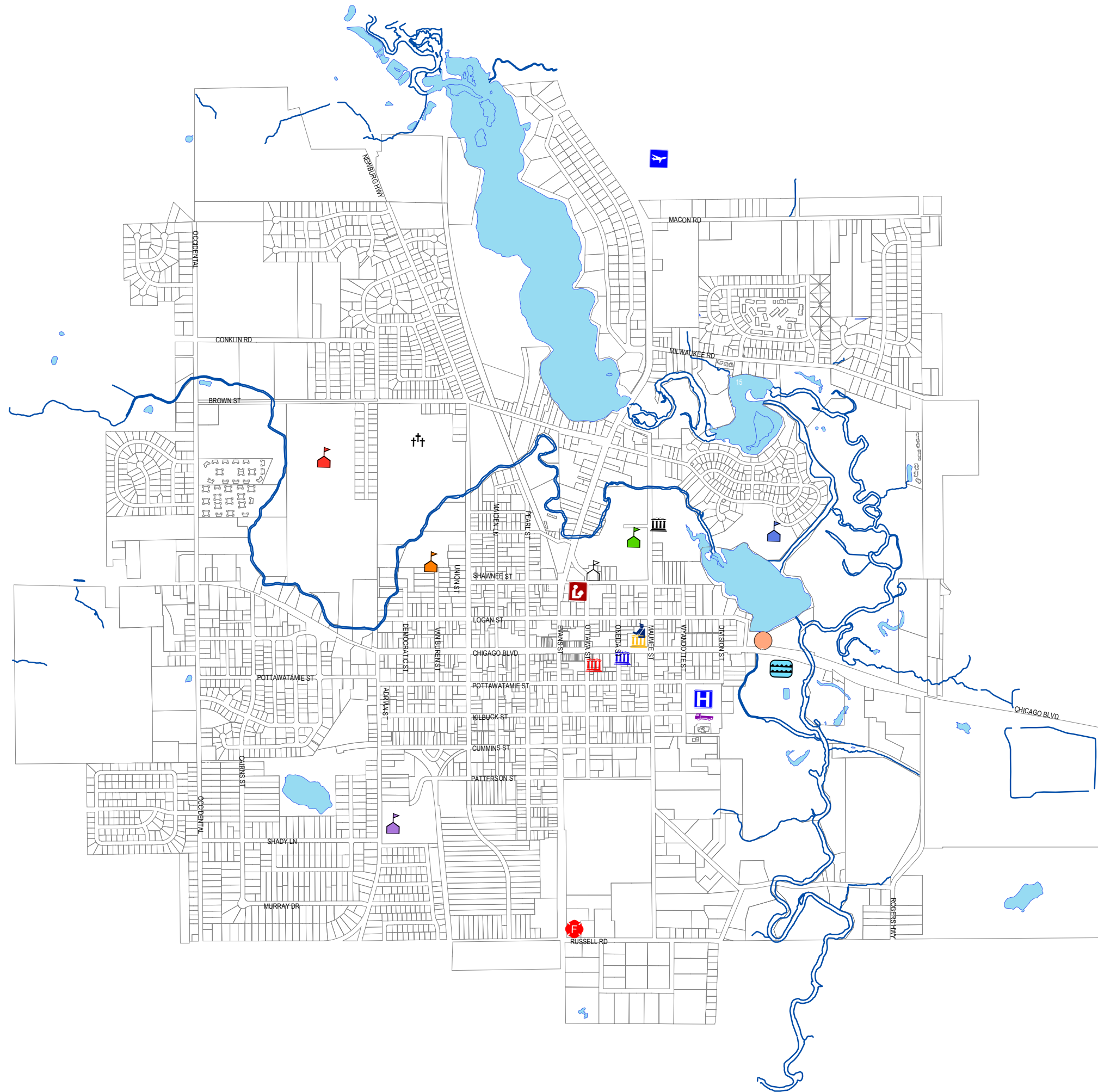
The City maintains 49.5 miles of pipe and the water system can process 5 million gallons of drinking water per day. The City provides some limited water service outside City limits. Water lines are shown on Map A3-4.






















SANITARY SEWER

The City of Tecumseh sewer system essentially provides service to the entire city, except for a few isolated areas. It also provides sewer service to some businesses outside City limits. The City of Tecumseh processes all of its own sewage; the plant currently processes about a million gallons per day. The Utilities Department currently has no plans to expand services or facilities. Sewer lines are shown on Map A3-5.

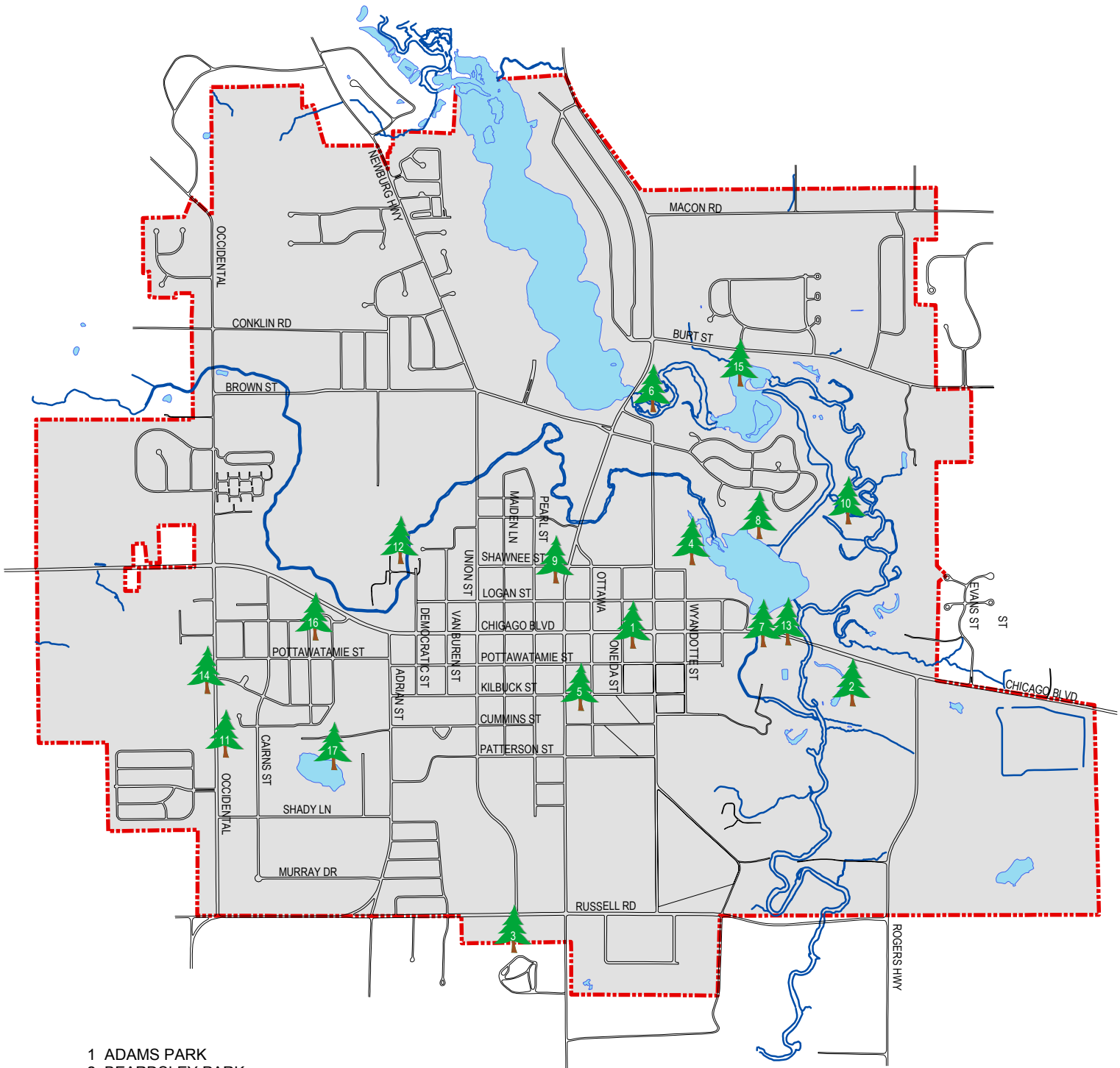
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-  CITY HALL
-  AIRPORT
-  TECUMSEH CIVIC AUDITORIUM
-  BROOKSIDE CEMETERY
-  COMMUNITY CENTER
-  DEPARTMENT OF PUBLIC WORKS YARD
-  ECONOMIC DEVELOPMENT OFFICE
-  EMERGENCY SERVICES STATION
-  HERRICK MEMORIAL HOSPITAL
-  TECUMSEH DISTRICT LIBRARY
-  TECUMSEH HISTORICAL MUSEUM
-  POLICE STATION
-  WASTEWATER TREATMENT PLANT
-  TPS ADMINISTRATION
-  TECUMSEH HIGH SCHOOL
-  TECUMSEH MIDDLE SCHOOL
-  HERRICK PARK ELEMENTARY
-  PATTERSON ELEMENTARY
-  TECUMSEH ACRES ELEMENTARY

COMMUNITY FACILITIES
 CITY OF TECUMSEH
 LENAWEЕ COUNTY, MICHIGAN
 MAP A3-1

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- 1 ADAMS PARK
- 2 BEARDSLEY PARK
- 3 CAL ZORN RECREATION AREA
- 4 DOUGLAS W. BIRD KIWANIS MEMORIAL PARK
- 5 ELLIOT PARK
- 6 EVANS PARK
- 7 HAYDEN-FORD MILL
- 8 HERRICK PARK
- 9 HOTRUM PROMENADE PARK
- 10 INDIAN CROSSING TRAILS PARK
- 11 OCCIDENTAL STREET SITE
- 12 PATTERSON PARK
- 13 SATTERTHWAITTE PARK
- 14 SOUTH OCCIDENTAL PARK
- 15 STANDISH DAM
- 16 SUNSET PARK
- 17 TECUMSEH CITY PARK

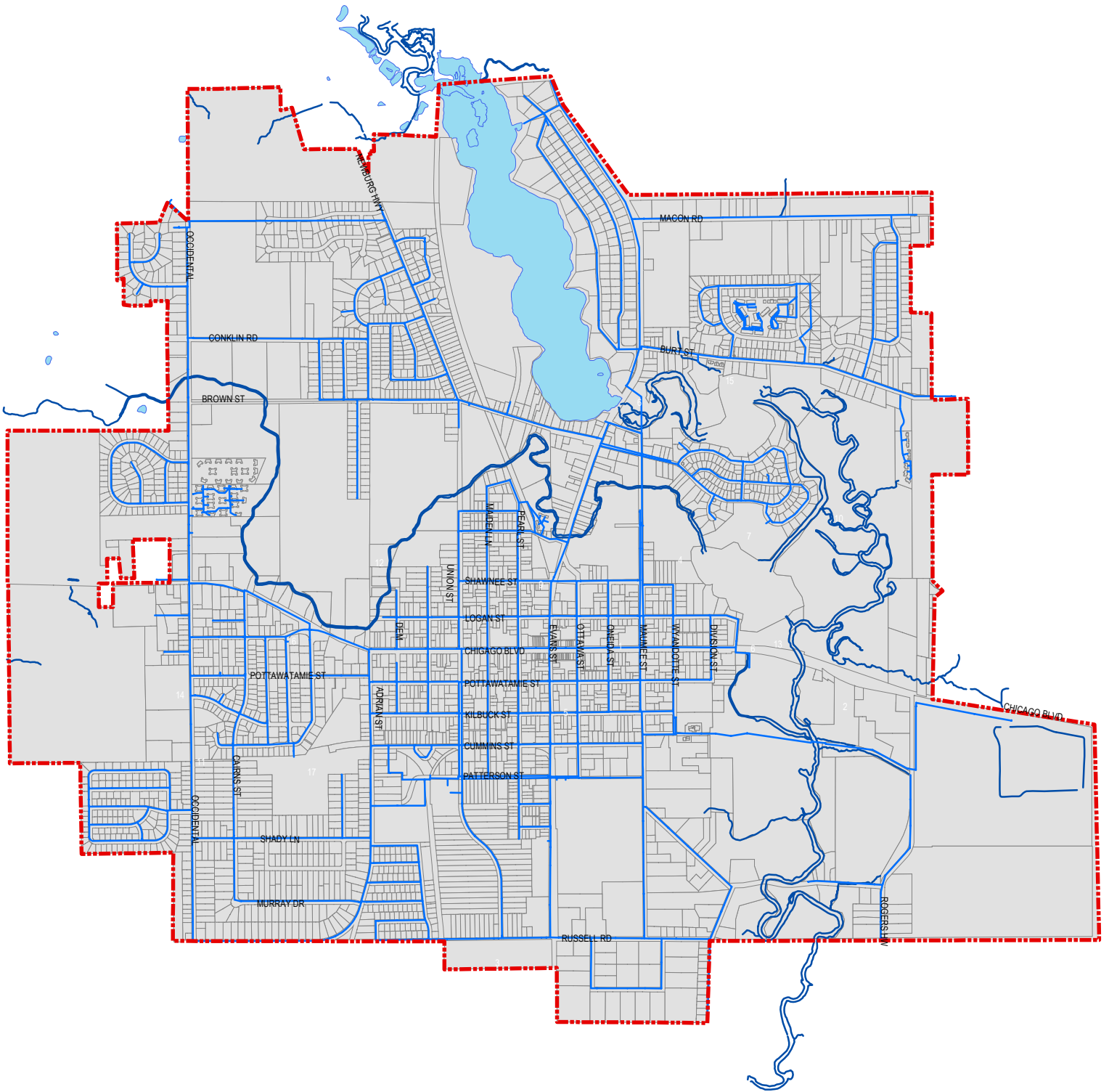
PARKS
 CITY OF TECUMSEH
 LENAWEЕ COUNTY, MICHIGAN
 MAP A3-2

500
 Feet




BIRCHLER ARROYO
 ASSOCIATES, INC.

Source: CITY OF TECUMSEH



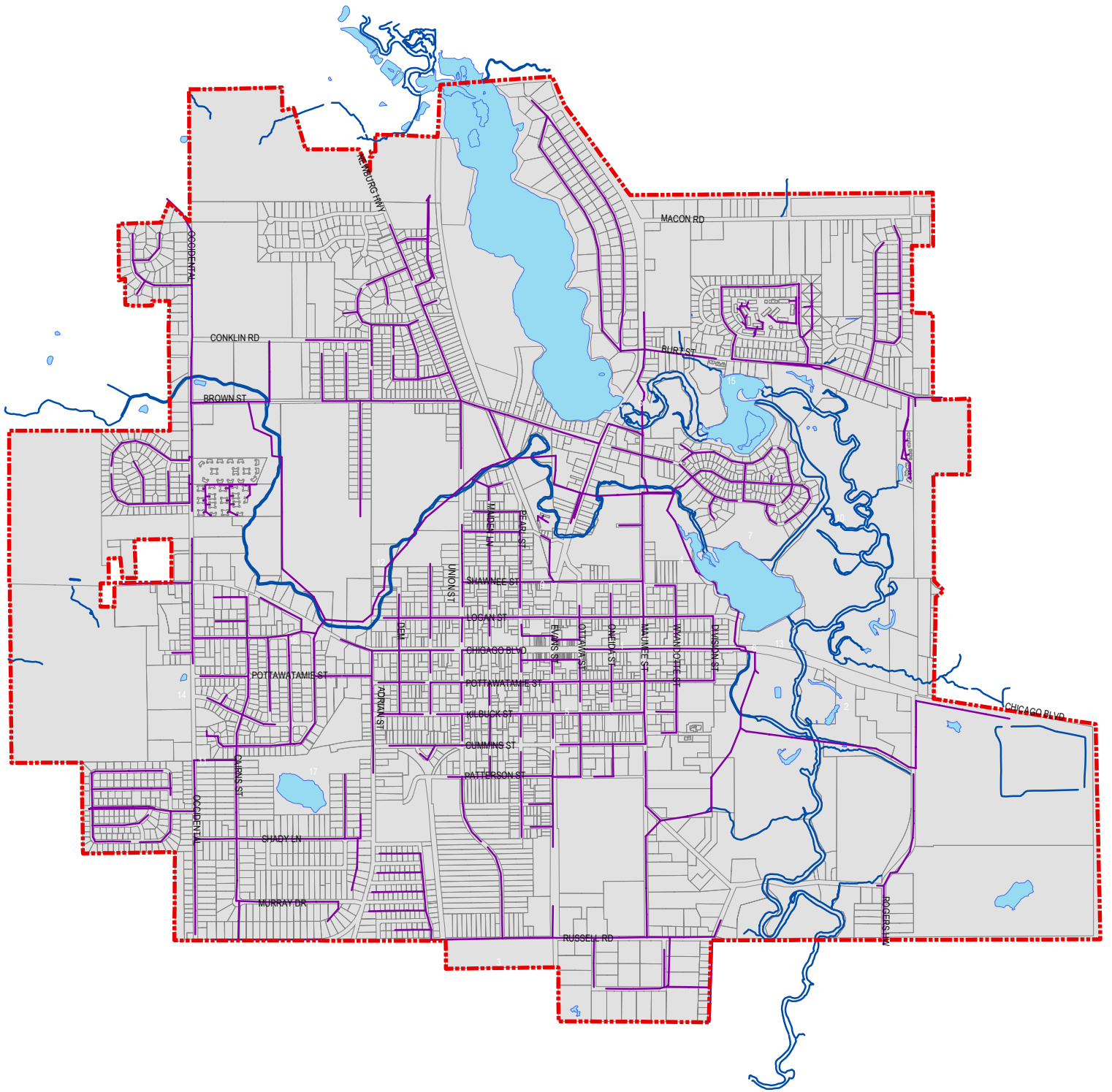
WATER LINES
 CITY OF TECUMSEH
 LENAWE COUNTY, MICHIGAN
 MAP A3-4

 WATER LINES

500
 Feet



BIRCHLER ARROYO
 ASSOCIATES, INC.



SEWER LINES
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP A3-5

— SEWER LINES

500
 Feet



BIRCHLER ARROYO
ASSOCIATES, INC.

NATURAL FEATURES INVENTORY



Natural features contribute to residents' and visitors' impressions of a community. Their presence can make a community a desirable place to live; their absence can discourage people from settling in a community.

The identification of the natural features that presently exist in the City is an important step in the Master Plan process. With such knowledge, decision-makers can make informed proposals for the preservation and protection of the City's natural resources.

TOPOGRAPHY

The City of Tecumseh lies within an area that was covered and uncovered by glaciers as ice ages came and went. The last glacier to affect Lenawee County was during the Late Wisconsinan period, around 9,000 years ago. At this time, the Saginaw lobe (extension of the glacier) retreated northward. Glaciers can have several different effects on the land as they retreat. For example, they may act as "steamrollers" across the land, flattening it as they go; they may create large depressions that later fill with water; or, as they pause in their movement, they may drop large quantities of glacial till—rocks, soil, and other debris caught up in the ice.

In general, the area around Tecumseh—the River Raisin watershed—consists of level plains and gently rolling hills. These areas were essentially "ironed out" by the advance and retreat of the glacier.

An area where a glacier paused in its retreat for a significant amount of time, long enough to drop large deposits of glacial till, is called a moraine. The result on the topography of the land is level ground or gently rolling hills. The Irish Hills to the west of Tecumseh are part of the Kalamazoo Moraine. The lakes in

and around the Irish Hills are “kettle lakes,” formed when giant blocks of ice left behind by glaciers finally melted, creating depressions that then filled with water.

The City of Tecumseh’s topography varies by about 120 feet from the lowest point to the highest point. The lowest areas of 750 feet are located at the bottom of the River Raisin riverbed, and the highest point of 868 feet is located in the very northwest corner of the City near Occidental Road. USGS Quad Map a4-1 illustrates the City’s topography.

QUATERNARY GEOLOGY

The patterns of soils found in an area can generally be explained by the type of surface geology found below the soil. The surficial geology of the City of Tecumseh is shown on Map a4-2. The majority of the City consists of glacial outwash sand and gravel and post-glacial alluvium. As glaciers melt, fast-flowing streams form. These streams, along with slower melting processes, deposit rocks and debris caught up in the glacier, sometimes relatively far from the glacier itself. Glacial outwash sand and gravel and postglacial alluvium is typically fine to coarse sand alternating with layers of small gravel to heavy cobbles. Due to the larger size of these materials, these areas are often quick to provide groundwater recharge.

Two bands of end moraines of fine-textured glacial till run along the eastern and western edges of the City of Tecumseh. End moraines of fine-textured till are deposits of grayish brown or reddish brown, nonsorted glacial debris that were left as glaciers retreated from the area.

SOILS

Map a4-3, Soils, shows the locations of the various soil textures in the City of Tecumseh. A large area in the center of the City contains Fox cobbly gravelly loams. Large areas of Fox loam are found in the north and western parts of the city. Other significant soil types include Brady and Macomb loams, and Morley loams. Loam soils exhibit the properties of sand, silt, and clay equally. Interestingly, gravelly loam soils are preferred by vintners for growing grapes for wine. Perhaps this explains why French settlers found a “Riviere aux Raisins” when they first explored southeast Michigan over two hundred years ago.

GROUNDWATER RECHARGE

Water recharge areas are highly permeable areas where water on the surface travels rapidly through the soil into the groundwater. Map A4-4 illustrates the groundwater recharge areas within the City. As the map illustrates, a majority of the City can be categorized as a high groundwater recharge area. Some small pockets of moderate groundwater recharge areas can be found at the western and eastern edges of the City. Existing and planned development within recharge areas should



include safeguards against negative impacts, as pollution of these areas could have a significant detrimental effect on the area's groundwater.



WATERSHEDS

All runoff water in the City of Tecumseh eventually enters the River Raisin. In the north, east, and south-central parts of the City, water flows into the river directly.

Water in the west-central part of the City flows first into Evans Creek, which empties into Mill Pond, and then into the River Raisin. In the southwest corner of the City, water flows into the South Branch of the River Raisin. These subwatersheds are illustrated on Map A4-5. The management of stormwater runoff by City of Tecumseh government and residents can have a direct effect on the health of the River Raisin.

WETLANDS

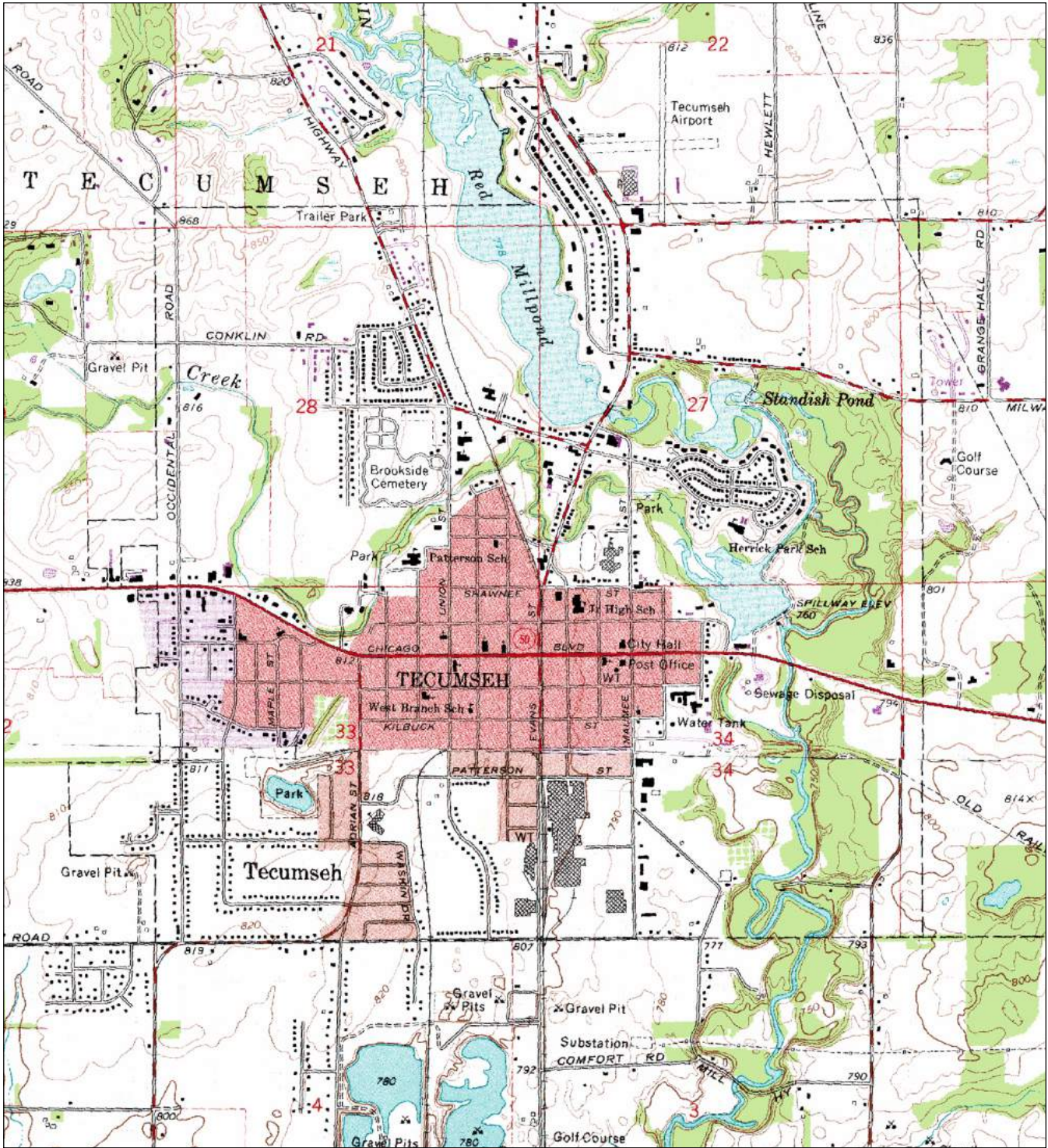
As illustrated on Map a4-6, the City of Tecumseh has only a few wetland areas. Nevertheless, wetlands are valuable natural resources within the City. They may serve as storm water holding areas to reduce flooding; provide for the settling of sediments and pollutants from surface water runoff; reduce stream bank erosion caused by storm water runoff; and provide unique habitat for fish and wildlife. Those which are five acres or more, as well as smaller wetlands hydrologically connected to large wetlands, fall under the jurisdiction of the Michigan Department of Environmental Quality. However, the City itself is in the best position to monitor the health of its wetlands, regardless of size.

The pace of development can impact natural resources - such as wetlands - in several ways. The most immediate loss caused by development is the reduction of open space and associated wetlands and woodlands. Water quality and flora and fauna habitat areas are directly affected by development.

Although the City of Tecumseh is largely built out, there are still development projects occurring just inside the City limits as well as in the surrounding townships. It is important to preserve as many naturally occurring wetlands as possible.

WOODLANDS

Prior to settlement of southeast Michigan, the area that is now the City of Tecumseh was almost entirely covered by forests. As shown on Map a4-7, after more than two centuries of agricultural and urban development the woodlands in the City cover a much smaller area. Still, the remaining woodlands are valuable natural features which serve as windbreaks, aid in the absorption of rainwater, replenish oxygen, create natural beauty and character, and provide wildlife habitat. Often, woodlands and wetlands are found in conjunction with each other, forming an even more diverse habitat for plants and animals. Preservation of these areas is important, and the effects of development on existing woodlands throughout the City of Tecumseh should be minimized.



TOPOGRAPHY
 CITY OF TECUMSEH
 LENAWEE COUNTY, MICHIGAN
 MAP A4-1

PLEASE SEE NEXT PAGE FOR LEGEND.

Source: USGS



BIRCHLER ARROYO
 ASSOCIATES, INC.

TOPOGRAPHIC MAP LEGEND

SOURCE: U.S. DEPARTMENT OF INTERIOR
U.S. GEOLOGICAL SURVEY

CONTROL DATA AND MONUMENTS

Aerial photograph roll and frame number* 3-20

Horizontal control

Third order or better, permanent mark	Neace	Neace
With third order or better elevation	BM 45.1	Pike BM 45.1
Checked spot elevation	79.5	
Coincident with section corner	Cactus	Cactus
Unmonumented*		

Vertical control

Third order or better, with tablet	BM X 16.3
Third order or better, recoverable mark	X 120.0
Bench mark at found section corner	BM 18.6
Spot elevation	X 5.3

Boundary monument

With tablet	BM 21.6	BM 71
Without tablet	171.3	
With number and elevation	67 301.1	

U.S. mineral or location monument

CONTOURS

Topographic

Intermediate	
Index	
Supplementary	
Depression	
Cut; fill	

Bathymetric

Intermediate	
Index	
Primary	
Index Primary	
Supplementary	

BOUNDARIES

National	
State or territorial	
County or equivalent	
Civil township or equivalent	
Incorporated city or equivalent	
Park, reservation, or monument	
Small park	

LAND SURVEY SYSTEMS

U.S. Public Land Survey System

Township or range line	
Location doubtful	
Section line	
Location doubtful	
Found section corner; found closing corner	
Witness corner; meander corner	

Other land surveys

Township or range line	
Section line	
Land grant or mining claim; monument	
Fence line	

SURFACE FEATURES

Levee	
Sand or mud area, dunes, or shifting sand	
Intricate surface area	
Gravel beach or glacial moraine	
Tailings pond	

MINES AND CAVES

Quarry or open pit mine	
Gravel, sand, clay, or borrow pit	
Mine tunnel or cave entrance	
Prospect; mine shaft	
Mine dump	
Tailings	

VEGETATION

Woods	
Scrub	
Orchard	
Vineyard	
Mangrove	

GLACIERS AND PERMANENT SNOWFIELDS

Contours and limits	
Form lines	

MARINE SHORELINE

Topographic maps

Approximate mean high water	
Indefinite or unsurveyed	

Topographic-bathymetric maps

Mean high water	
Apparent (edge of vegetation)	

COASTAL FEATURES

Foreshore flat	
Rock or coral reef	
Rock bare or awash	
Group of rocks bare or awash	
Exposed wreck	
Depth curve; sounding	
Breakwater, pier, jetty, or wharf	
Seawall	

BATHYMETRIC FEATURES

Area exposed at mean low tide; sounding datum	
Channel	
Offshore oil or gas: well; platform	
Sunken rock	

RIVERS, LAKES, AND CANALS

Intermittent stream	
Intermittent river	
Disappearing stream	
Perennial stream	
Perennial river	
Small falls; small rapids	
Large falls; large rapids	
Masonry dam	
Dam with lock	
Dam carrying road	
Perennial lake; Intermittent lake or pond	
Dry lake	
Narrow wash	
Wide wash	
Canal, flume, or aqueduct with lock	
Elevated aqueduct, flume, or conduit	
Aqueduct tunnel	
Well or spring; spring or seep	

SUBMERGED AREAS AND BOGS

Marsh or swamp	
Submerged marsh or swamp	
Wooded marsh or swamp	
Submerged wooded marsh or swamp	
Rice field	
Land subject to inundation	

BUILDINGS AND RELATED FEATURES

Building	
School; church	
Built-up Area	
Racetrack	
Airport	
Landing strip	
Well (other than water); windmill	
Tanks	
Covered reservoir	
Gaging station	
Landmark object (feature as labeled)	
Campground; picnic area	
Cemetery: small; large	

ROADS AND RELATED FEATURES

Roads on Provisional edition maps are not classified as primary, secondary, or light duty. They are all symbolized as light duty roads.

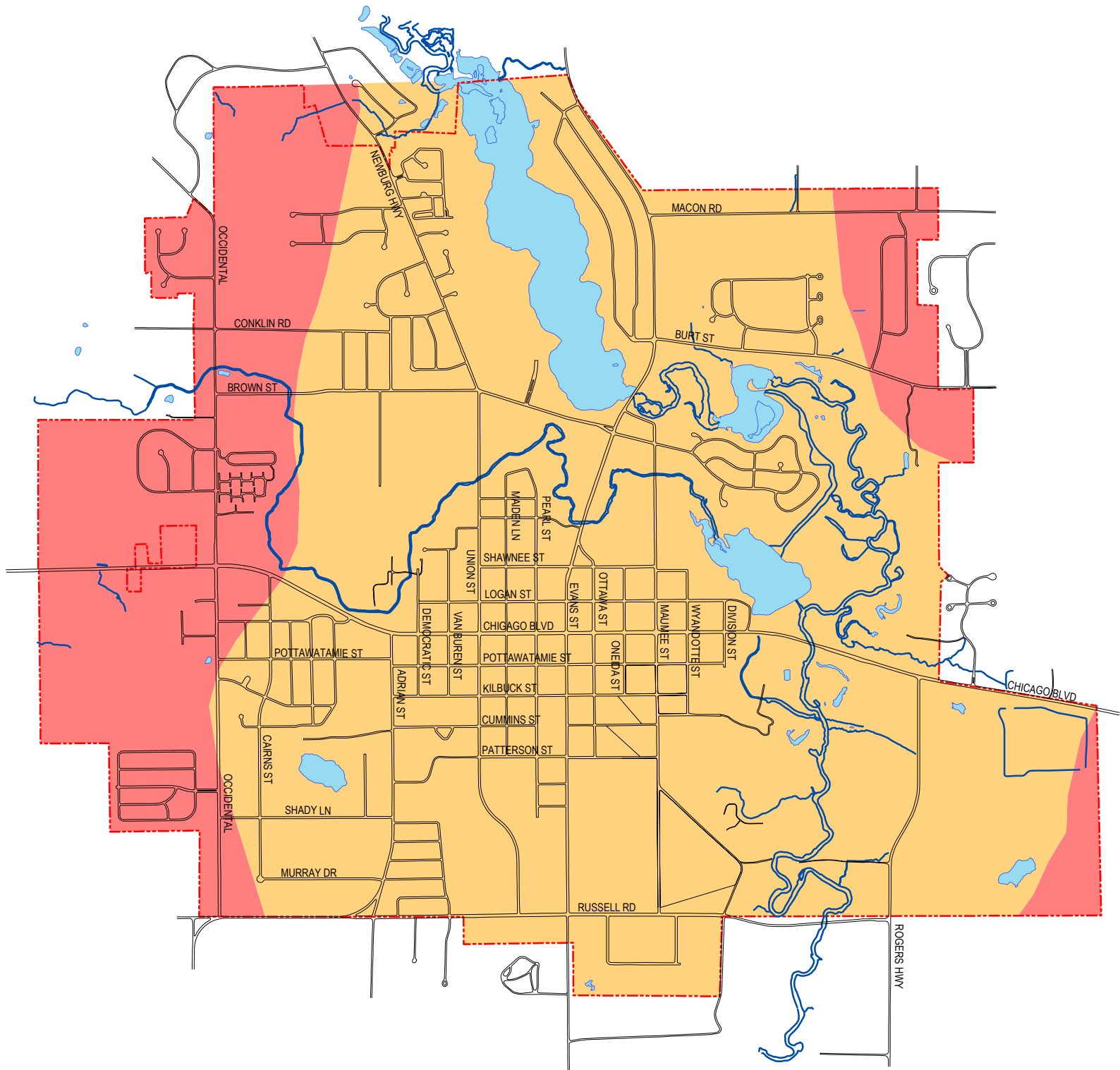
Primary highway	
Secondary highway	
Light duty road	
Unimproved road	
Trail	
Dual highway	
Dual highway with median strip	
Road under construction	
Underpass; overpass	
Bridge	
Drawbridge	
Tunnel	

RAILROADS AND RELATED FEATURES

Standard gauge single track; station	
Standard gauge multiple track	
Abandoned	
Under construction	
Narrow gauge single track	
Narrow gauge multiple track	
Railroad in street	
Juxtaposition	
Roundhouse and turntable	

TRANSMISSION LINES AND PIPELINES

Power transmission line: pole; tower	
Telephone line	
Aboveground oil or gas pipeline	
Underground oil or gas pipeline	



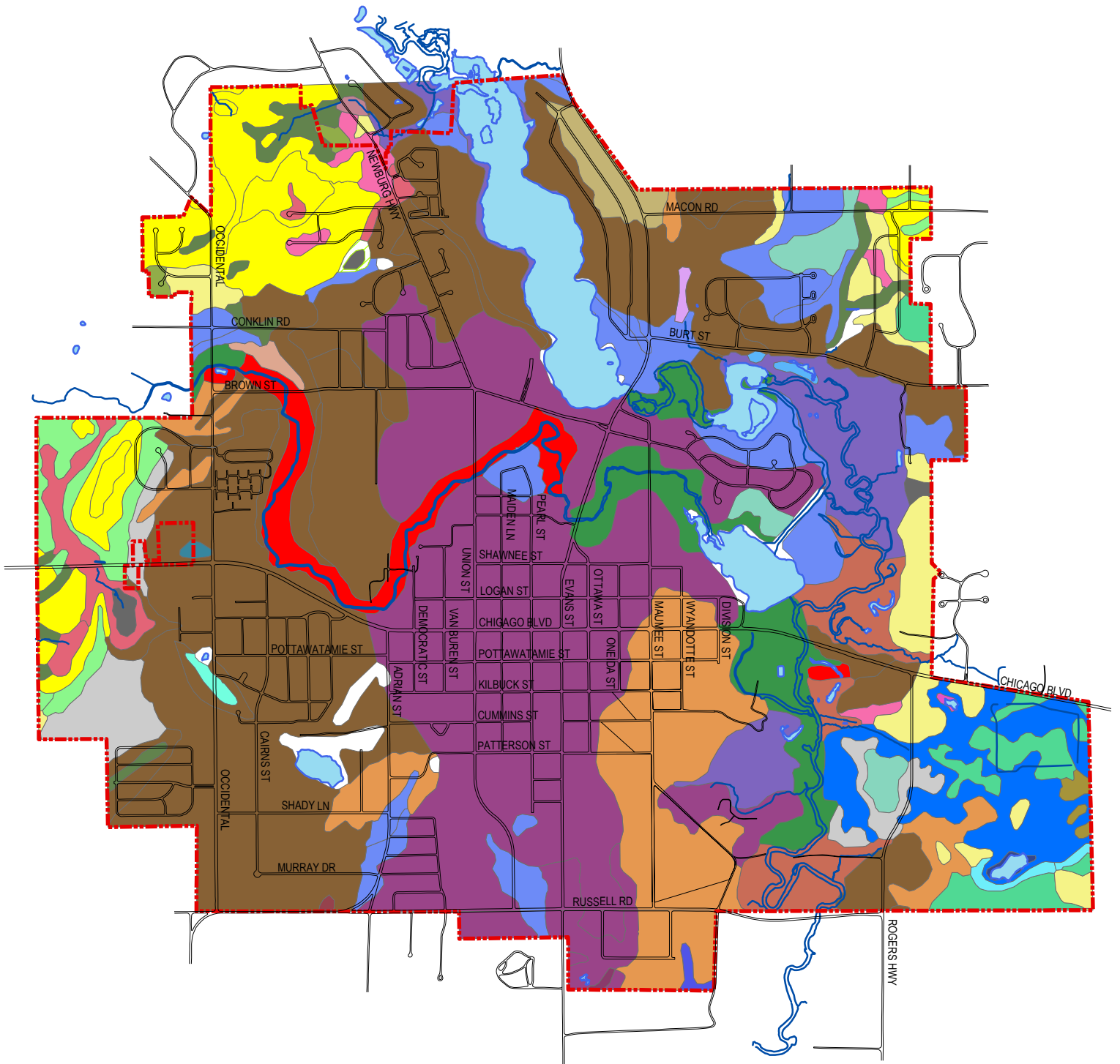
QUATERNARY GEOLOGY
 CITY OF TECUMSEH
 LENA WEE COUNTY, MICHIGAN
 MAP A4-2

- END MORAINES OF FINE-TEXTURED TILL
- GLACIAL OUTWASH SAND & GRAVEL AND POST-GLACIAL ALLUVIUM

500
 Feet

Source: MICHIGAN DNR





- | | |
|------------------------------|--|
| Barry sandy loam | Genesee sandy loam |
| Blount loam | Griffin and Genesee loams |
| Brady and Macomb loams | Griffin and Sloan sandy loams |
| Brady and Macomb sandy loams | Houghton muck |
| Brady sandy loam | Hoytville and Wauseon loams |
| Bronson sandy loam | Hoytville clay loam and Rimer sandy loam |
| Brookston loam | Ionia loam |
| Cadmus and Blount loams | Kendallville loam |
| Cadmus loam | Kerston muck and loams |
| Cadmus sandy loam | Kokomo and Barry loams |
| Carlisle muck | Kokomo, Barry, and Walkkill loams |
| Conover loam | Macomb sandy clay loam and Hoytville clay loam |
| Fox cobbly gravelly loam | Morley loam |
| Fox loam | Nappanee loam |
| Fox sandy loam | Nappanee silt loam |
| Genesee and Eel loams | Pewamo clay loam |
| Genesee loam | Sebewa loam |
| | St. Clair loam |
| | LAKES |

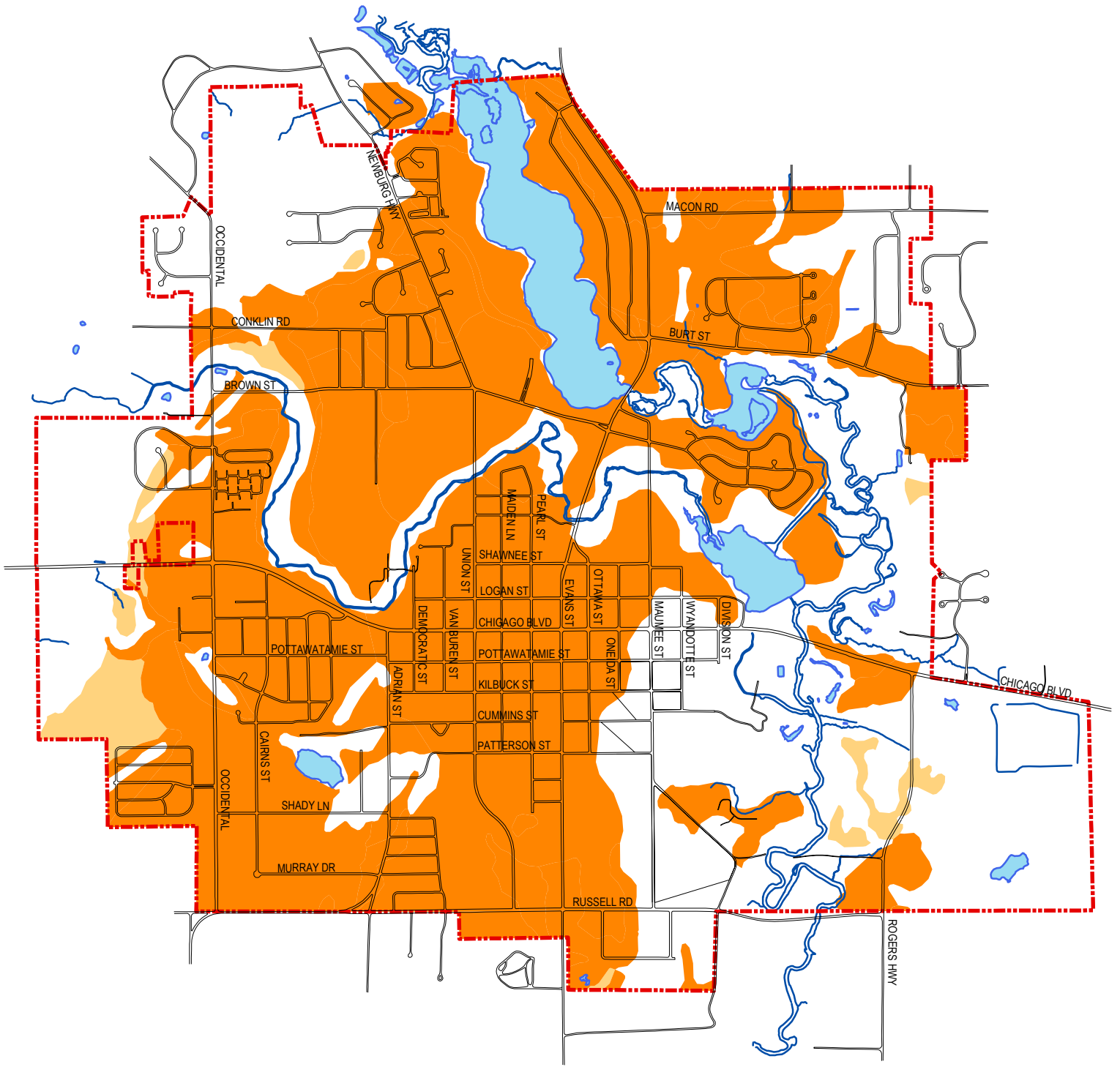
SOILS
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP A4-3

500
 Feet



BIRCHLER ARROYO
 ASSOCIATES, INC.

Source: REGION II



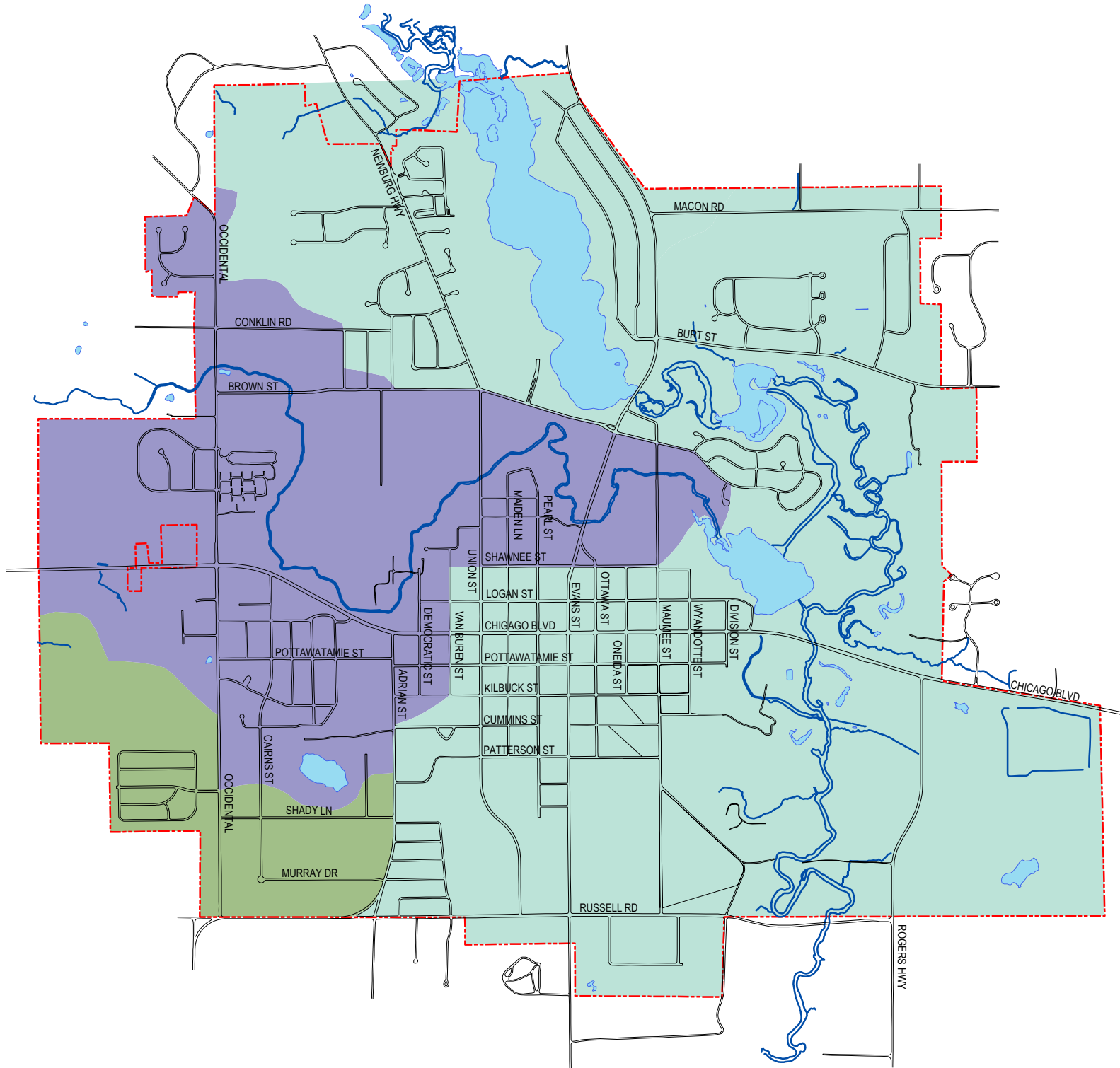
- HIGH GROUNDWATER RECHARGE AREA
- MODERATE GROUNDWATER RECHARGE AREA
- LAKES

GROUNDWATER RECHARGE
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP A4-4

500
 Feet



BIRCHLER ARROYO
ASSOCIATES, INC.



SUBWATERSHEDS OF THE RIVER RAISIN

- EVANS CREEK
- RIVER RAISIN
- SOUTH BRANCH OF RIVER RAISIN

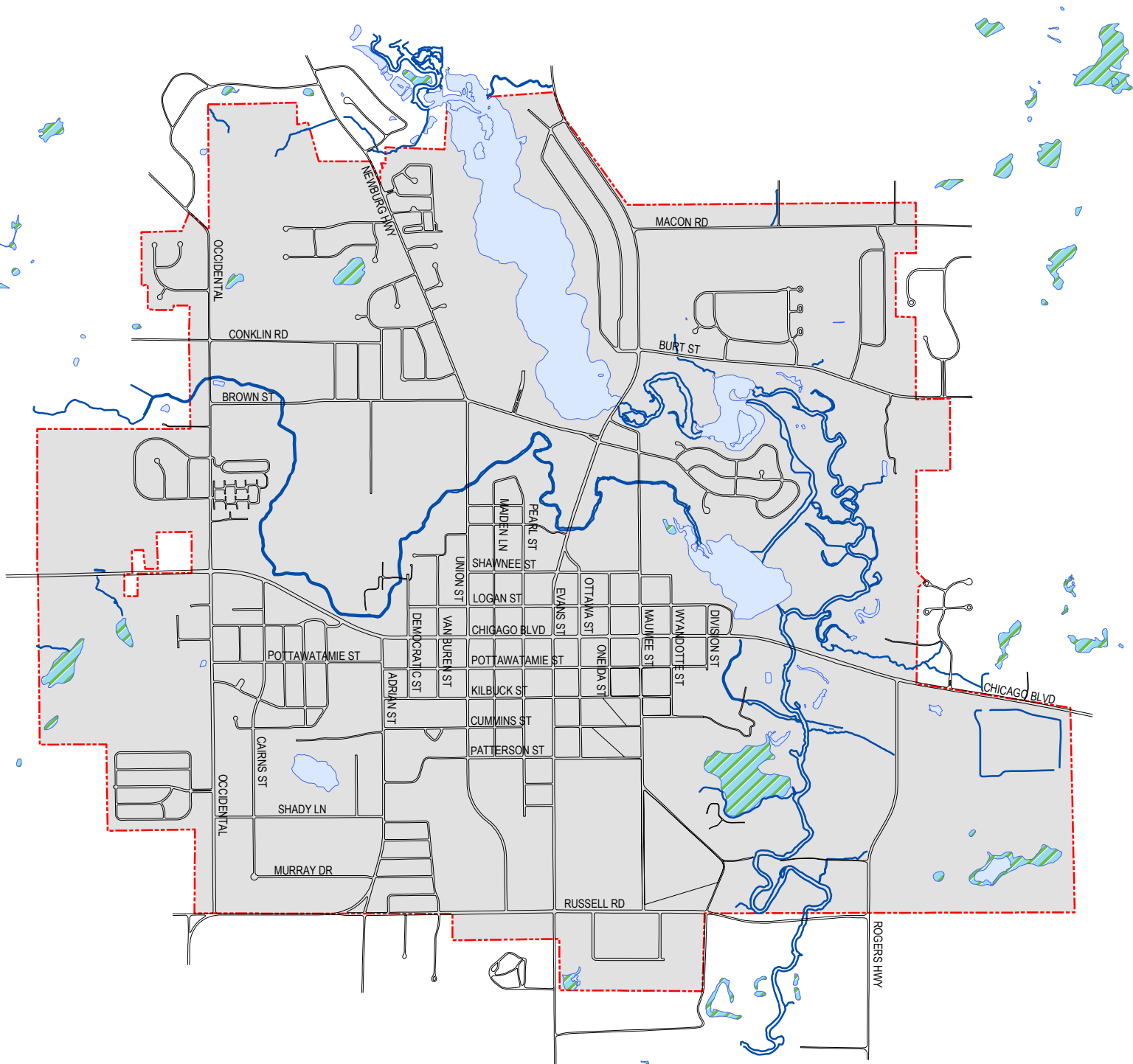
Source: MICHIGAN DNR

WATERSHEDS
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP A4-5

500
 Feet



BIRCHLER ARROYO
ASSOCIATES, INC.



 WETLANDS

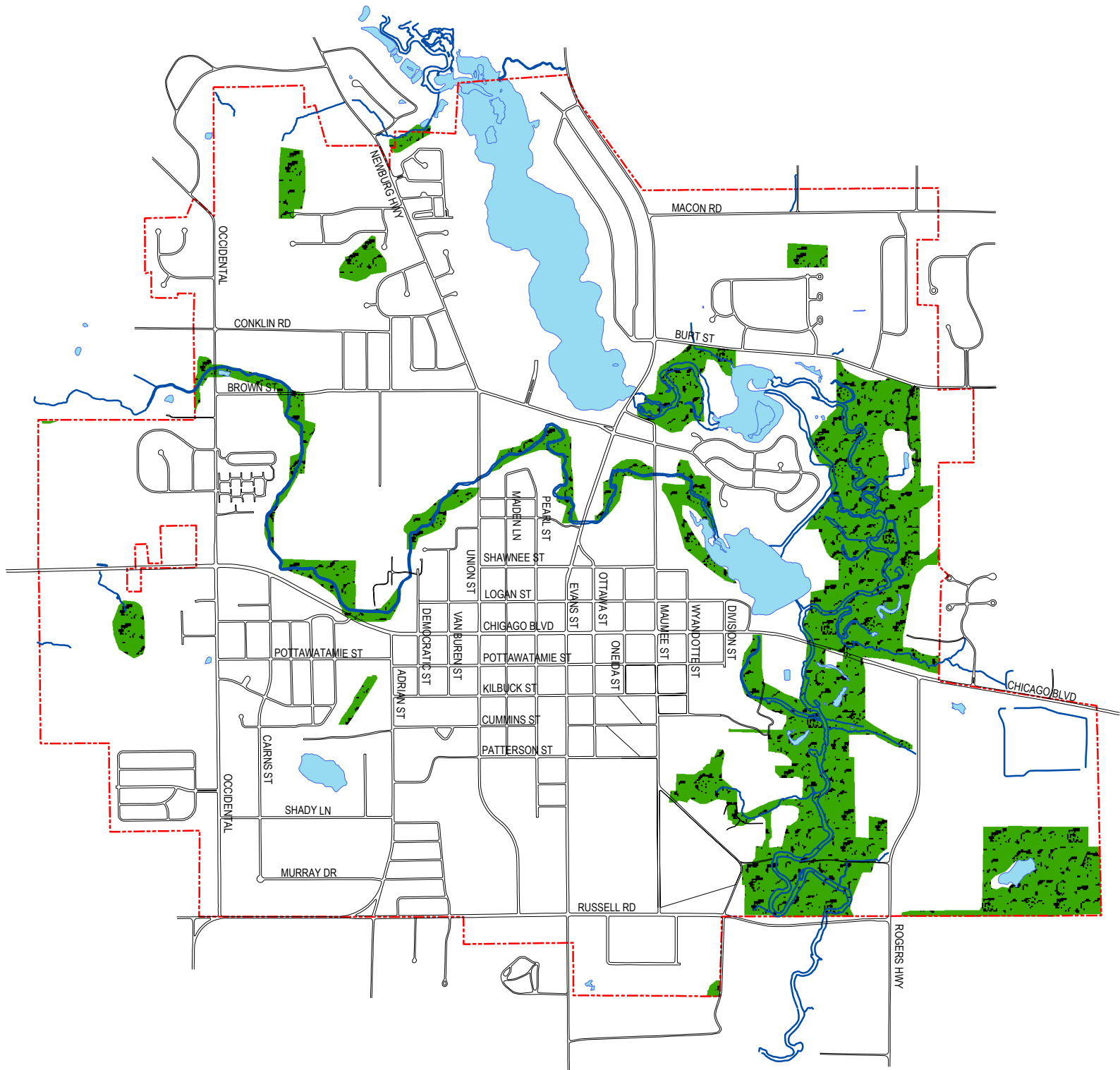
WETLANDS
 CITY OF TECUMSEH
 LENAWEE COUNTY, MICHIGAN
 MAP A4-6

500
 Feet



BIRCHLER ARROYO
 ASSOCIATES, INC.


Source: CITY OF TECUMSEH



 WOODLANDS

Source: MICHIGAN DNR

WOODLANDS
 CITY OF TECUMSEH
 LENA WEE COUNTY, MICHIGAN
 MAP A4-7

500
 Feet



BIRCHLER ARROYO
 ASSOCIATES, INC.

A5

EXISTING TRANSPORTATION CONDITIONS INVENTORY

The function of the road system has a significant impact on the viability of land uses and the overall quality of life in a community.

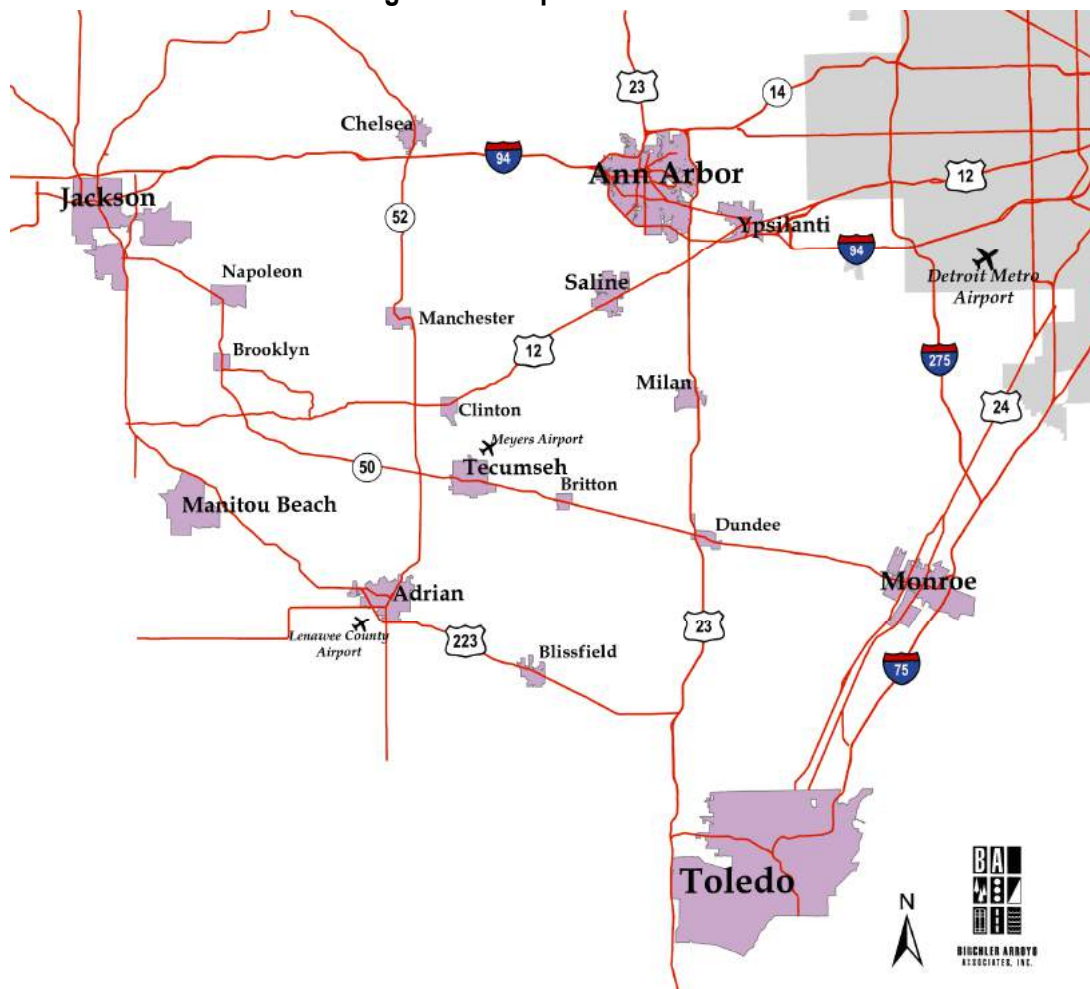


A well known axiom of the planning profession is that land uses and transportation are inextricably connected. A properly designed and maintained road network is necessary to provide access to development; yet the pattern of land uses and number and type of access points onto roads impact the function of the road system. The function of the road system and its ability to move traffic in an efficient and convenient manner can affect the viability of land uses and the overall quality of life in a community. The course of a roadway often determines the paths for public utility lines including water, sanitary sewer, storm sewer, gas, electrical power, and communications. Public services such as police, fire, and emergency rescue rely on the safe and adequate provision of roadways.

REGIONAL LOCATION

The City of Tecumseh is in the northeast corner of Lenawee County near the southern border of Washtenaw County and the western border of Monroe County. State Highway 50 (M 50) runs east/west through Tecumseh, connecting the City to US 23, Britton, Dundee, and Monroe to the east and Brooklyn, Napoleon, and Jackson to the west. State Highway 52 (M 52) runs north/south just west of the City, providing access Adrian to the south and Manchester and Chelsea to the north. The City also has convenient access to US 12 via Tecumseh-Clinton Highway (Evans Street), which connects Tecumseh to Saline, Ann Arbor, Ypsilanti, US 23, and Interstate 94 to the northeast. This regional transportation network is illustrated on Map A5-1 below.

Map A5-1
Regional Transportation Network



FUNCTIONAL CLASSIFICATION

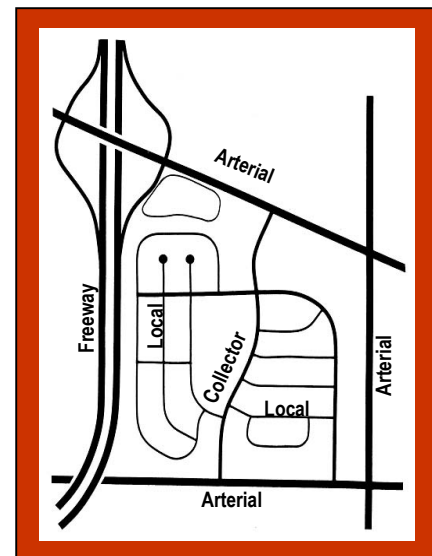
Traffic is concentrated on certain roadways due to the roads' physical condition, level of use, and direction of travel, as well as the overall land-use pattern. About 20 percent of the roads carry approximately 80 percent of vehicle miles traveled¹. In order to set funding priorities for the roads that carry the highest volumes, transportation planners established a street classification system.

Although there is some variation in classification, roadways are typically divided into those that carry through traffic and those that carry local traffic (Figure A5-1). It is desirable to physically separate these two road types as much as possible to eliminate conflicting traffic movements, traffic congestion, delays, and crashes.

¹ Coghlan, Gerald. "Opportunities for Low Volume Roads", *Transportation in the New Millennium*, Transportation Research Board, 1999.

In order to function successfully, the overall traffic circulation system must be carefully integrated. In the City of Tecumseh, the four basic types of roads are arterials, minor arterials, collectors, and local streets. The role of each road classification in providing access and mobility is illustrated in Figure A5-2. Generally, as access increases, mobility decreases – and vice versa. The Existing Functional Classification System Map A5-2 indicates how the roads in the City of Tecumseh are classified by the function they serve as part of the overall road network.

Figure A5-1
Functional Classification of Streets



ARTERIALS

Arterials are the “highest order” of surface streets, and they typically carry high volumes of traffic. Arterials provide travel routes from one city to another, and can traverse one or more states. They are most often used for longer trips, as higher speeds are allowed. When an arterial passes through a more populated area, however, the number of intersections increases and speeds decrease. Arterial roads have a dual function: to provide routes for through traffic while providing access to abutting properties and minor intersecting streets. This can lead to congestion and traffic crashes because of turning vehicles conflicting with through traffic.

MINOR ARTERIALS

Minor arterials serve a similar function as arterials, however, these roads typically carry less traffic.

COLLECTOR STREETS

The intent of a collector street is to collect vehicles from the local streets or rural areas and distribute them to either local destinations or to an arterial. The collector street system serves both land access and through traffic.

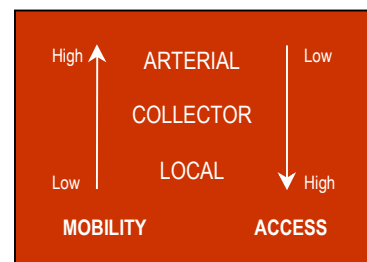
LOCAL STREETS

Providing access to adjacent land is the sole function of local streets. These streets make up a large percentage of total street mileage in urban areas, but they almost always carry a small portion of vehicle miles traveled. The aim of local streets is to provide access to collector streets and through routes, but in such a manner that through traffic is not encouraged to use the local streets as a shortcut route.

TRAFFIC VOLUMES

There are a variety of factors that determine the volume of traffic that travels along a given roadway. The location and length of the roadway, the roadway condition and surface (paved vs. unpaved), and the quantity and type of adjacent land uses are all factors that contribute to the overall traffic on a roadway.

Figure A5-2
Mobility Vs. Access



The Traffic Volumes Map A5-3 displays the traffic volumes along the City's major streets. As the map indicates, the highest volumes are located on the City's two main streets: Chicago Boulevard (M 50) and Evans Street. Occidental also carries a relatively significant volume of traffic south of Chicago Boulevard.

EXISTING ROAD NETWORK

The Existing Road Conditions Map A5-4 identifies the number of lanes on the roadways, traffic signal locations, and locations on the road system that may have a safety concern. The areas on the roadway system that have the greatest safety concerns are the following:

- ❖ Offset intersections
- ❖ Skewed intersections (angles less than ninety degrees or other unusual configurations)

Offset and skewed intersections increase the probability of conflicting traffic movements that can occur, thereby increasing the probability of a crash.

TRAFFIC CRASHES

This section includes a general examination of traffic crash causes, factors that influence traffic safety, and traffic crash data and trends within the City.

TRAFFIC CRASH CAUSES

Roadway design and surface conditions, driver behavior, and weather conditions may all be contributing factors to the cause of a crash. The most frequent cause of traffic crashes is attributed to improper driving. Excessive speed, failure to yield the right-of-way, and following too closely are the principal types of improper driving behavior that lead to crashes. Alcohol and other drugs were reported to be a factor in over one-half of the fatal traffic crashes in the United States.

The impact of weather on traffic crashes may seem significant because a high number of traffic crashes can occur during a short period of time; however, over one-half of total traffic crashes take place when pavement is dry.

The general characteristics of traffic crashes vary significantly in urban and rural areas. Approximately one-quarter of all rural crashes take place at intersections versus about one-half for urban areas. Although more crashes take place in urban areas, crashes in rural areas are more severe; the severity is primarily due to higher rural speeds.

The impact of congestion becomes apparent when examining the relationship between increased traffic volumes and crash frequency. Studies have shown that crash rates increase with increasing volume to a certain point, and then the crash rate drops as congestion and volumes increase. The peak traffic volume in one study was found to be 650 vehicles per hour on California highways; another study of two-lane rural roads showed similar results with a peak traffic volume at about 8,000 vehicles per day.

FACTORS THAT INFLUENCE TRAFFIC SAFETY

Roadway design and safety features also impact crash rates. Several roadway design and safety features are discussed below.

Lane Width

A narrow lane width generally has a greater impact on safety as vehicle speeds increase. One study showed that widening 240 miles of highway from 9-foot wide lanes to 11-foot wide lanes reduced crashes by 21 percent on low-volume roads and 47 percent on high volume roads.

Highway Shoulders

Studies have shown that crash rates decrease with increasing shoulder width. This is primarily due to the fact that motorists traveling on roads with wide shoulders have a stable area to use if they cross the outer pavement edge.

Horizontal Curves

Curves in roadway pavement increase the possibility of traffic crashes. The most frequent crash type is skidding.

Vertical Alignment

A change in pavement elevation can also have an impact on crash rates. Long, steep grades and steep grade/horizontal curve combinations can greatly increase the frequency of crashes.

Intersections

The design, location, number of approaches, traffic controls and vehicular volume of an intersection influence the number and type of crashes that will occur at a given intersection. For example, an intersection with three approaches is generally safer than one with four approaches because of the reduced number of conflicting movements. The sight distance for the motorists approaching the intersection is also a critical variable.

Speed

The speed at which vehicles travel must reflect an appropriate response to existing road and traffic conditions in order to minimize the frequency and severity of crashes. Consequently, high speeds are often safer than slow speeds on roads designed for high speed travel, provided road and weather conditions are good. However, high speeds can both increase the severity of crashes and decrease the frequency of crashes.

Other Factors

Other factors that influence speed and safety include night lighting, railroad crossings, pedestrian crossing, interchanges, and median designs. A comprehensive discussion of all these factors is beyond the scope of this report. However, the general discussion above has been included to provide the reader with some background on the cause of traffic crashes and factors influencing safety before examining crash data within the City of Tecumseh.

TRAFFIC CRASH DATA AND TRENDS

Table A5-1 lists all intersections in the City of Tecumseh at which two or more crashes occurred within the two-year time period of January 2001 through December 2002.

Intersections with four or more crashes during this same time period are illustrated on Map A5-5. As both the table and map illustrate, the intersections with the highest number of crashes are primarily located along the City's most heavily traveled roads, including Chicago Boulevard (M 50), Evans, and Occidental. (It should be noted that crash data for the intersection of Occidental and Russell was collected prior to the installation of a traffic signal at this location in the Fall of 2003.)

Table A5-1
Intersections with 2 or More Crashes
City of Tecumseh, 2001-2002

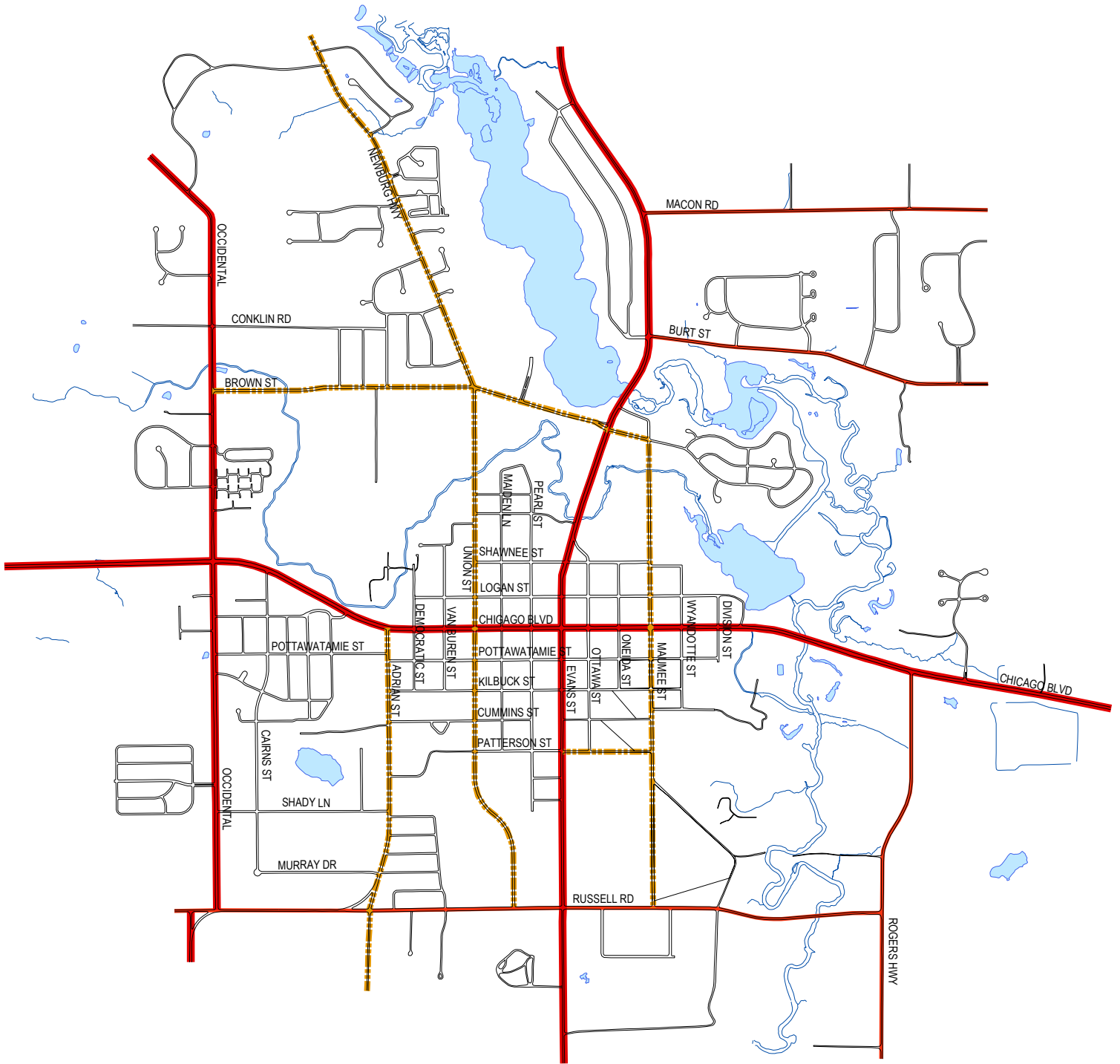
Intersection	Crashes	Intersection	Crashes
M 50 / Occidental	22	Herrick Park / Maumee	3
M 50 / Maumee	19	M 50 / Pearl	3
M 50 / Union	14	Adrian / Pottawatamie	2
Occidental / Russell	11	Bishop Reed / Brown	2
M 50 / Evans	10	M 50 / Eastern	2
M 50 / Cairns	7	Evans / Herrick Park	2
Evans / Russell	7	Evans / Logan	2
Evans / Brown	6	Evans / Pottawatamie	2
M 50 / Wyandotte	6	Evans / Reed Mill	2
M 50 / Adrian	5	Evans / Water	2
Maumee / Russell	5	James / Maumee	2
Brown / Union	4	M 50 / Rogers	2
Evans / Burt	4	M 50 / Maiden Lane	2
Adrian / Russell	3	Occidental / Westhaven	2

Source: Regional II Planning Commission

The intersection crash frequencies listed in Table A5-1 vary primarily with the amount of traffic passing through those intersections. To better assess the actual hazard levels, crash rates – in terms of crashes per million entering vehicles (MEV) – were determined for the nine intersections having both four or more crashes in two years and a means of estimating daily traffic volumes. Relative to a large sample of comparable intersections throughout southeast Michigan, it was found that three Tecumseh intersections have average or above-average crash rates (all others are below average):

- ❖ M 50 / Maumee – 1.26 crashes per MEV (94% of average)
- ❖ Occidental / Russell – 1.15 crashes per MEV (98% of average)
- ❖ M 50 / Occidental – 1.70 crashes per MEV (142% of average)

According to procedures outlined in the *SEMCOG Traffic Safety Manual* (developed for statewide use), only the crash rate for M 50 and Occidental is sufficiently above the sample average to confidently declare it a "high-crash" location. As such, further engineering evaluation of that intersection is warranted to determine the prevailing accident characteristics and appropriate crash countermeasures.

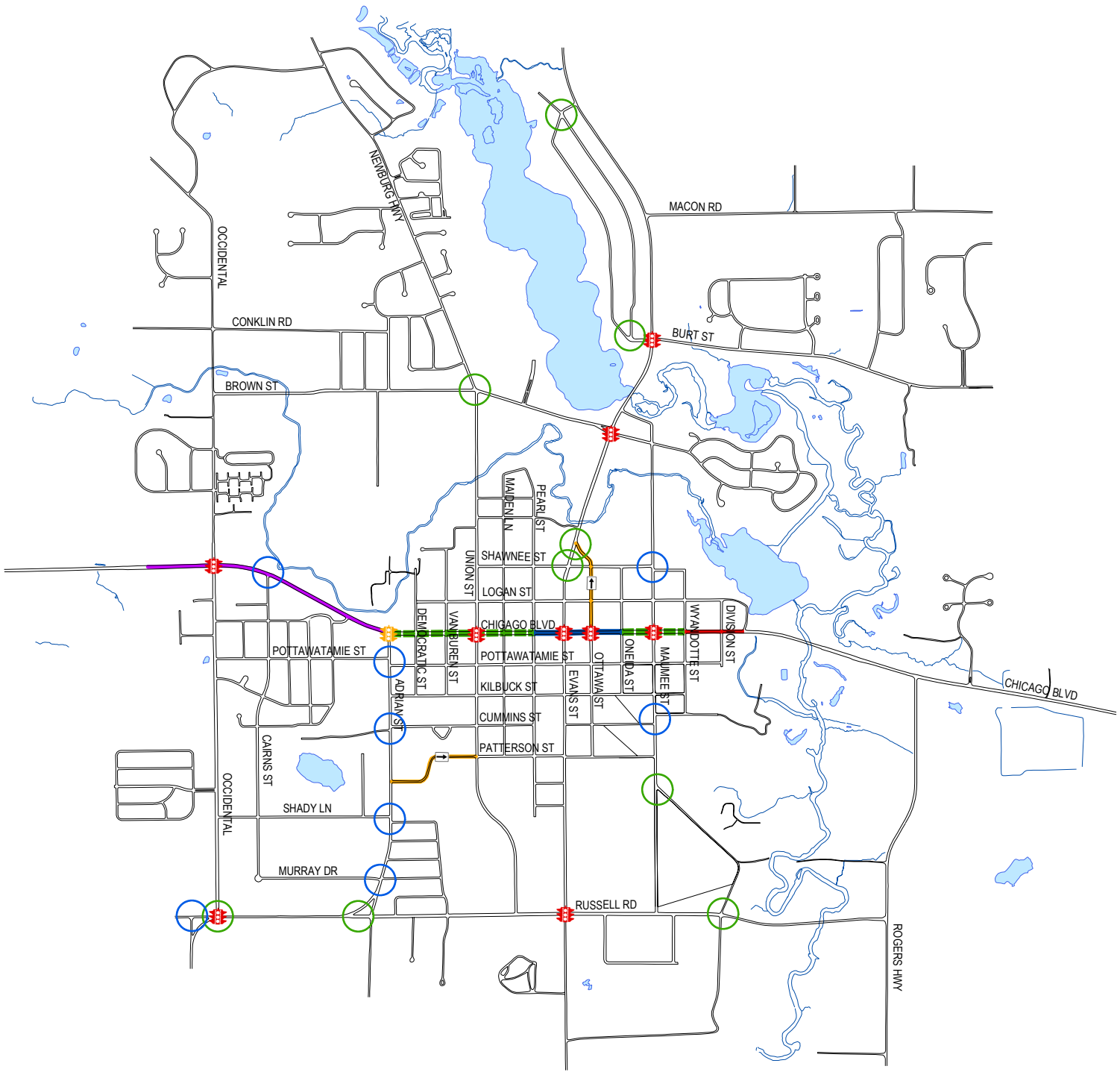












- Arterial
- Minor Arterial
- - - Collector Street
- Local Street

EXISTING FUNCTIONAL CLASSIFICATION
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP A5-2

500
 Feet



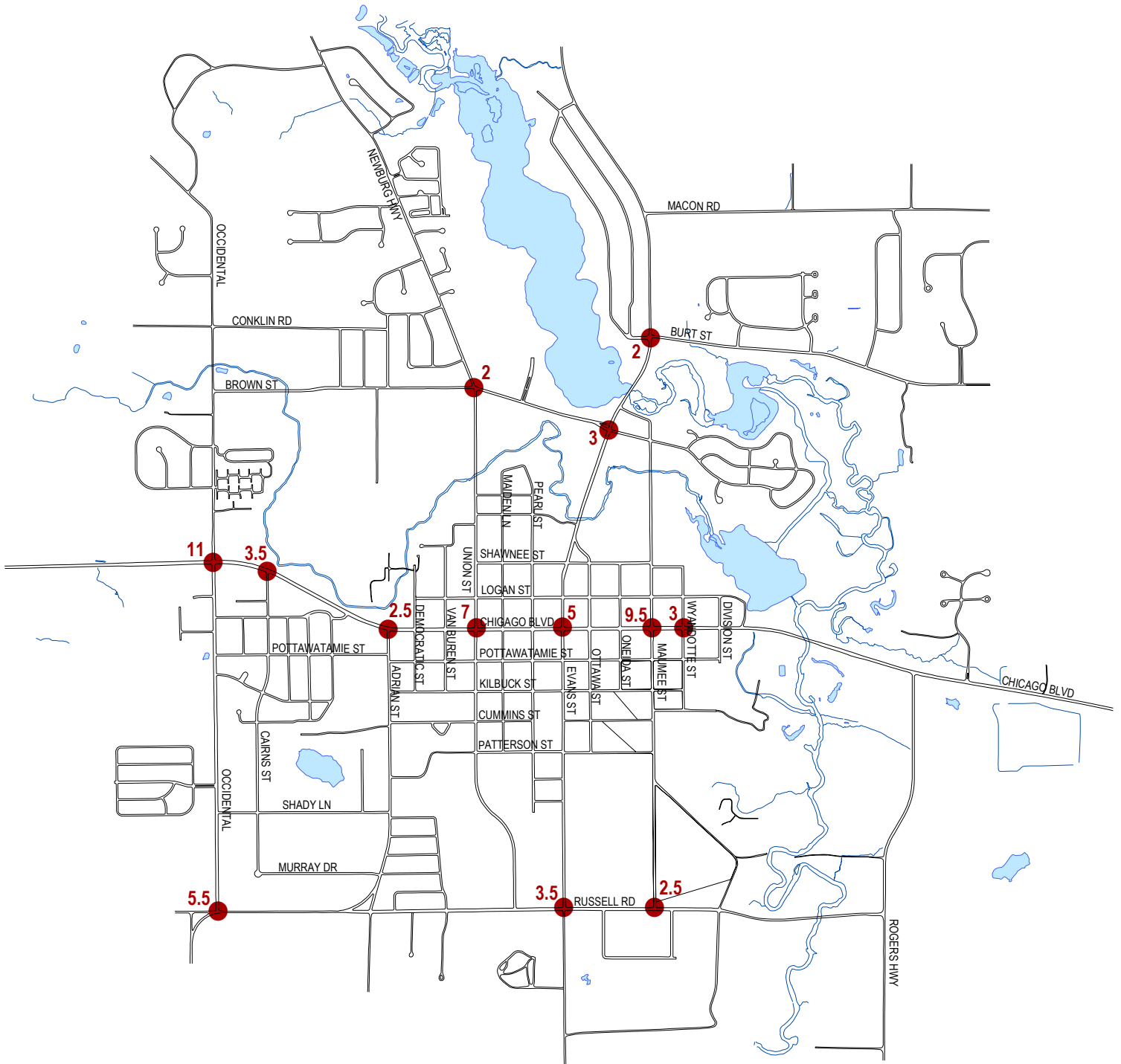


-  Traffic Signal
-  Traffic Signal (Flashing Mode)
-  Skewed Intersection
-  Offset Intersection
-  4 Lane Boulevard
-  4 Lanes
-  3 Lanes (Center Turn Lane)
-  3 Lanes (2 Westbound Lanes)
-  2 Lanes
-  One Way

**EXISTING
ROAD CONDITIONS**
CITY OF TECUMSEH
LENAWEE COUNTY, MICHIGAN
MAP A5-4

500
Feet





- Intersections with 4 or More Crashes in 2-Year Time Period (2001-2002)
- 7 Average Annual Number of Crashes

AVERAGE ANNUAL CRASHES
 CITY OF TECUMSEH
 LENAWEЕ COUNTY, MICHIGAN
 MAP A5-5

500 Feet



A6

VISION SESSION SUMMARY



The Vision Session is a vital opportunity for the Planning Commission to gain the input of residents, other City officials, and community leaders.

On November 11, 2003, over forty people met at the Hayden-Ford Mill Community Center to participate in a Vision Session designed to gather public input on the City of Tecumseh Master Plan. Participants included business owners, residents, Planning Commission and City Council members, public safety officials, and planners.

The participants were divided into six groups to discuss several questions during the two-hour visioning process. The questions were developed to generate a variety of thoughts and stimulate discussion on the future of the City of Tecumseh. The following questions were asked:

- What defines the City of Tecumseh?
- What can the City do to maintain and preserve its community character?
- What needs/desires are there for improved and/or additional City services and community facilities?
- What types of residential development are needed within the community?
- Which areas of the City are more suitable for higher density and multiple family development?
- Need/desire/locations for more commercial outside of downtown?
- Downtown—adequate parking, need for additional uses, other improvements?
- Need/desire/locations for more industrial/research & development/high tech development?

- Need/desire/locations for office uses?
- Adequacy of existing street network?
- Future needs and desires—new roads, roads to be widened, etc.?
- Specific safety concerns?
- Bike paths/sidewalks—desired locations?

Following the small-group discussions, a designated representative of each group presented the consensus of the group's ideas. The comments were recorded on a large tablet for all participants to view. At the end of the evening, each participant was given four dots in order to mark those comments or ideas that the participant thought were most important to pursue for the Master Plan. In this way, a general consensus could be determined for each question posed to the group. Using these group comments, the Vision Statement (which appears in Chapter 4) was developed to reflect the overall consensus of the group. The ultimate purpose of the Vision Statement is to provide significant public input into the formulation of goals, objectives, and policies statements. In that regard, the experience is invaluable to the Planning Commission and City Council, and provides them important guidance.

What defines the City of Tecumseh?

Participants see their city as a vibrant place with a small town feel. They feel that the City is a safe and friendly community with a strong economic development structure, good schools, and important and distinctive historical features.

What can the City do to maintain and preserve its community character?

Several participants think that the City of Tecumseh should acquire more land, so as to have better control over its development. They also feel that it is important for the City to encourage preservation and restoration of existing buildings, and that design standards and zoning ordinances should be strictly enforced.

What needs/desires are there for improved and/or additional City services and community facilities?

Many participants would like to see an expansion of Rails to Trails in the area. They also feel that more recreational opportunities for children and teens are needed, and that the City's many waterways could be emphasized more. Downtown public restrooms, an expansion of City Hall, a fire station at the north end of town, more parks, and public transportation were other ideas that garnered support.

What types of residential development are needed within the community?

A large number of participants feel that there is a great need for phased senior housing in the City. Many participants also support the idea of building condos or brownstones in the downtown. Some participants would like to see more expensive housing being built in the City, while others feel that maintaining quality housing in a variety of price ranges is important.

Which areas of the City are more suitable for higher density and multiple family development?

Several different ideas arose in response to this question, including southeast outside the City limits, northwest, and south near Tecumseh Products. Other participants feel that no such development is necessary for the City.

Need/desire/locations for more commercial outside of downtown?

Many participants feel that the City needs more lodging facilities. Many also agree that the Evans/Brown shopping area could benefit from redevelopment efforts. Others would like to see a grocery at the north and east ends of town. Some residents pointed out that the downtown area should be filled with businesses before the City considers expanding commercial zoning, while others feel that more commercial zoning would be appropriate in the near future.

Downtown—adequate parking, need for additional uses, other improvements?

Participants are satisfied with existing downtown parking; however, some feel that entrances to the City and to City-owned parking lots should be enhanced. The Carnegie Library building is an issue that some people feel needs to be addressed through another Vision Session. Some uses that participants would like to see in the downtown are more retail, more restaurants, and lodging facilities.

Need/desire/locations for more industrial / research & development / high tech development?

Participants think that a partnership between the City and Tecumseh Township to develop an industrial park could be profitable, although others caution that existing industrially-zoned land should be used up first.

Need/desire/locations for office uses?

Participants suggested encouraging office uses to move off of Chicago Boulevard in the downtown area and onto side streets, leaving the main street for retail and restaurants.

Adequacy of existing street network? Future needs and desires—new roads, roads to be widened, etc.? Specific safety concerns?

In general, residents are not interested in expanding the street network or widening roads. They do have some specific concerns about intersections that might benefit from left turn lanes or traffic lights. They think that one-way streets should revert to two-way. One item on which most participants agree is the necessity of a truck route to keep industrial traffic out of the downtown and away from residential areas.

Bike paths/sidewalks—desired locations?

There is overwhelming support for more bike paths in the City, with several specific recommendations for where sidewalks should be extended and bike paths should be built.

COMMENTS FROM VISION SESSION PARTICIPANT GROUPS

Note: Check marks indicate that more than one group contributed the same comment. Number of dots indicates how many dots were placed next to the comment at the end of the Vision Session.

COMMUNITY CHARACTER

8 DOTS

Rails to trails √√

6 DOTS

Teen center and recreation areas

4 DOTS

Truck route

3 DOTS

City should acquire land √

Encourage preservation and restoration √√

Strong economic development department

2 DOTS

Indoor recreation for children √√

Development of waterways √√√

1 DOT

Enforce design standards √√√

Grants to restore buildings and homes

Support hospital

Control growth

Enforce zoning √

Recognize people who do things right

Improved use of community center

Land for industrial park √

Senior housing

North side utilities/services

Outdoor pool

Amphitheater

Combat interstate

Proactive, not reactive planning

0 DOTS

Continue "Promotion of Past"

Encourage community pride

Don't stagnate

Recognize value of what we have

Support police/fire departments

Maintain infrastructure

Convert historic homes back to single-family dwellings

Promote existing seasonal events

Attract teens and keep them busy

Quality leaders

Involve new generations in civic pride and

development

Education

Promote community volunteerism

Public transportation √

Men's and especially women's clothing stores

Community college satellite

Dog park

More seasonal events especially winter

Expand City Hall √

Spring cleanup

Community arts program

Improve cemetery

More parks √

West end improvements

Create incentives for improvements

Planning sessions vital

Include adjoining townships in planning and services

(police/fire), will help keep taxes low

Downtown restrooms √

Street sweeping

Fire station at north end of town √

Continually protect rivers/greenways

Unique shops

Understated signage

Small-town festivals/parades

Planned activities for youth

Vision Session Summary

DEFINES TECUMSEH

3 DOTS

Strong economic development
Sense of safety ✓

2 DOTS

Schools

1 DOT

Historical features ✓✓✓✓✓
Small town feel ✓✓✓✓✓
Well-kept ✓✓
Strong sense of community ✓
Progressive

0 DOTS

Feels vibrant ✓✓
Downtown architecture ✓
Industrial base ✓
Quality restaurants
Proximity to larger cities ✓
Friendly ✓✓✓
Conservative
No interstate connection
Distinctive neighborhoods
Excellent public services ✓
Library and civic auditorium
Cooperative citizens
Well-rounded variety of businesses
Civic pride

RESIDENTIAL

15 DOTS

Progressive senior housing like Grand Court in Adrian
✓✓✓

7 DOTS

Condos/brownstones in downtown

2 DOTS

More affluent housing ✓
Rental property inspections

1 DOT

Maintain quality housing in variety of price ranges
Less dense development

0 DOTS

Additional condos ✓
New single family houses ✓
Starter homes \$125-150,000 or less ✓
Senior developments
Phased senior housing ✓
Income-based housing
Upper-story residential in downtown ✓
Redevelopment & infill on currently underutilized land
Areas for more dense development:
 Southeast outside City limits
 Not applicable to Tecumseh, maybe on edge of town
 Northwest ✓
 South by Tecumseh products
Designated resident parking in downtown
Convert older homes back to SF
Sensitivity to historic homes coming on the market
Anticipate changes in uses of grandfathered commercial property in residential areas
Trend: population push coming from Ann Arbor
Housing too expensive
Taxes too high
No effects from Cabela's yet

Vision Session Summary

COMMERCIAL

8 DOTS

Lodging

3 DOTS

Evans/Brown shopping area needs help

2 DOTS

North side grocery

Resolution to Carnegie library building—hold a vision session

Use marketing study

1 DOT

Grocery store/farmer's market at east end of town (old Napa building)

Entrances to city and parking lots need to be enhanced

Incentives for office uses to move out of downtown

More commercial zoning

Logan/Evans needs to be developed ✓

Develop office/service off the main streets ✓

More retail

Additional restaurants

Truck route ✓

0 DOTS

Area of office/residential mix

Encourage medical/dental near hospital but mediate effects on neighborhood

Aesthetics ✓

Office/service in underutilized areas

North end for office (plaza)

N. Evans office

West end streetscape

Maintain status quo

Designate area for office to keep them in town

Utilize 2nd stories for apartments/galleries ✓

Shared office/retail

Fill downtown before expanding ✓✓

Proactively encourage businesses we don't have

We already have good parking ✓✓

Bicycle parking

Downtown grocery store

Wayfinding signs

Flexibility in zoning

INDUSTRIAL

2 DOTS

City/townships share hi-tech industrial park

Use unused industrial facilities ✓

1 DOT

Community college satellite

0 DOTS

Be sensitive to truck issues ✓

May need to expand into township/425 agreement ✓

Continue to work with Lenawee Tomorrow

East end / Eton farm

Have own industrial park

TRAFFIC & CIRCULATION

6 DOTS

More bike paths ✓✓✓

LH turn lane at Maumee and Occidental

5 DOTS

Keep trucks out of town ✓

3 DOTS

Light at driveway by Doug & Ted's ✓

2 DOTS

Protect boulevard from redevelopment ✓

Use Russell Rd as a truck route ✓

Reinstate sidewalk and driveway approach paving program

1 DOT

Get rid of one-way on Ottawa

L turn light at Occidental and Chicago

No asphalt paths along residential streets

Extend boulevard sidewalks to west end ✓✓

0 DOTS

Heavy traffic at school entrance ✓

Lack of curb cut definition

Connect subdivisions better

Don't widen existing streets

Beautify entrances

Street tree planting

Existing street network is good ✓

Widen Mohawk, curb and gutter

Fix fences downtown

Address cul-de-sac design standards

Lane widths on W. Chicago Boulevard

Maiden Lane too narrow

Bike path along Occidental

Path needed along Evans by high school

Paths needed outside main sidewalk system

Connect parks/schools/downtown/other trails

Pedestrian signals at west end

Access management at west end

Sidewalks near Stacy Mansion ✓

RESOLUTION OF ADOPTION

City of Tecumseh Comprehensive Plan

By City of Tecumseh Planning Commission

WHEREAS, the City of Tecumseh Planning Commission has the responsibility and is empowered by the Municipal Planning Act No. 285 of 1931, as amended, to make and adopt a Comprehensive Plan for the physical development of the City and to amend the Plan as needed from time-to-time, and

WHEREAS, the Tecumseh City Council created the Planning Commission for the purposes stated in the Municipal Planning Act No. 285 of 1931, as amended, and

WHEREAS, the City of Tecumseh has retained a professional planning consultant to assist the Planning Commission with the technical studies necessary to make a new Comprehensive Plan for the City of Tecumseh, and

WHEREAS, the City of Tecumseh Planning Commission has held a public hearing on its proposed new Comprehensive Plan for the City on December 7, 2004 at the Tecumseh City Hall, and

WHEREAS, the Planning Commission finds that the new Comprehensive Plan is necessary for the continued development and the appropriate redevelopment of the physical areas of the City,

NOW THEREFORE BE IT RESOLVED that the City of Tecumseh Planning Commission hereby adopts this Comprehensive Plan for the City of Tecumseh, along with the text, maps, charts, graphs, and other materials contained in the Plan, and

BE IT FURTHER ORDERED, that an attested copy of the Comprehensive Plan shall be certified to the Tecumseh City Council, the Lenawee County Planning Commission, and the Region II Planning Commission.

Motion by Commissioner Gonser . Supported by Commissioner Rhora .

AYES: Commissioners Gentner, Smith, Van Buren, and Fox.

NAYS: None

ABSENT: Commissioners Harsh, McBride, and Bidwell.

RESOLUTION DECLARED ADOPTED THIS 7 th DAY OF December, 2004.



Robert C. Fox, Chairperson
City of Tecumseh Planning Commission

RESOLUTION OF ADOPTION
City of Tecumseh Comprehensive Plan
By Tecumseh City Council

WHEREAS, the Tecumseh City Council may adopt a Comprehensive Plan for the physical development of the City as empowered by the Municipal Planning Act No. 285 of 1931, as amended, and

WHEREAS, the Tecumseh City Council created the Planning Commission for the purposes stated in the Municipal Planning Act No. 285 of 1931, as amended, and

WHEREAS, the City of Tecumseh has retained a professional planning consultant to assist the Planning Commission with the technical studies necessary to make a new Comprehensive Plan for the City of Tecumseh, and

WHEREAS, the City of Tecumseh Planning Commission has held a public hearing on its proposed new Comprehensive Plan for the City on December 7, 2004 at the Tecumseh City Hall, and

WHEREAS, the Planning Commission adopted the new Comprehensive Plan for the City on December 7, 2004 and transmitted the Plan to the City Council for their approval,

NOW THEREFORE BE IT RESOLVED that the Tecumseh City Council hereby adopts this Comprehensive Plan for the City of Tecumseh, along with the text, maps, charts, graphs, and other descriptive materials contained in the Plan.

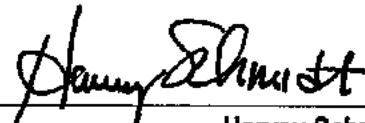
Motion by Deming Supported by Wright

AYES: Schmidt, VanAlstine, Wright, Baker, Deming, Housekeeper and Naugle.

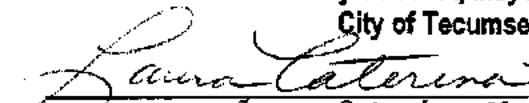
NAYS: None.

ABSENT: None.

RESOLUTION DECLARED ADOPTED THIS 4thTH **DAY OF** January, 2005.



Harvey Schmidt, Mayor
City of Tecumseh



Laura Caterina, Clerk
City of Tecumseh

Master Plan Meeting
City of Tecumseh Planning Commission
December 7, 2004
7:00 p.m.

Chairperson Fox called the meeting to order at 7:01 p.m.

Chairperson Fox led the pledge of allegiance.

Roll Call: Commissioners Gentner, Gonser, Rhora, VanBuren and Chairperson Fox. (Smith arrived at 7:22).

Absent: J. Harsh, V. McBride, and P. Bidwell
A quorum was established.

Others present: D. Malmquist, B. Raymond, and M. Johnson.

Public Comment: Items on the Agenda
None

Master Plan

Public Hearing opened at 7:07

Rod Arroyo, Birchler & Arroyo, explained the process in which the planning commission has for taken to present the draft form of the master plan at this time. He stated the city's last master plan was adopted in 1990. The new state statute mandates adoption of a city's master plan every ten years and to be revisited after 5 years. Birchler & Arroyo and the planning commission have been working together on the plan for the past 18 months.
Public Hearing closed @ 7:22.

Motion Gonser to adopt the master plan as submitted and recommends to forward to city council for adoption **Seconded** Rhora.

Roll Call: Gentner-yes, Smith-yes, Van Buren-yes, Rhora-yes, Gonser-yes, and Chairperson Fox -yes. **Motion Carried unanimously 6-0.**

Public Comment: Items not on the agenda
None

Fox stated the timing was right to begin reviewing the zoning ordinance.

Van Buren thanked the commissioners for their friendships and working relationships on the planning commission for the past 25 years. VanBurens resignation will be in effect after tonight's meeting.

Meeting adjourned at 7:33
Recorded by Michelle Johnson

July 22, 2003

Lenawee County Planning Commission
c/o Tim Anderson, Senior Planner
Region II Planning Commission
Jackson County Tower Building
120 W. Michigan Ave.—16th Floor
Jackson, MI 49201



BIRCHLER ARROYO
ASSOCIATES, INC.

RE: City of Tecumseh Master Plan Preparation Notice

To Whom It May Concern:

Pursuant to the requirements of Section 125.7a(2) of the Municipal Planning Act as amended, this notification is to inform you of the City of Tecumseh's intent to update its Master Plan. A copy of the draft plan will be distributed to you for your review and comment in advance of the City's public hearing on the plan as specified in this act. The plan's scope is as follows:

1. Existing Conditions Analysis including demographic, existing land use, regional setting, and community facilities updates; existing transportation conditions; and natural features inventory
2. Public Input component including a Community Vision Program and the development of a Vision Statement
3. Development of Goals, Objectives, and Strategies
4. Land Use Plan
5. Thoroughfare Plan
6. Housing Plan
7. Economic Development Plan
8. Downtown Tecumseh Plan
9. Capital Improvements/Infrastructure Plan
10. Natural Resources/Environmental Protection Plan

We welcome you to follow the progress of the plan. Updates and drafts will be posted at <http://www.birchlarroyo.com/tecumseh.htm>. If you would like to receive any future notices regarding the City of Tecumseh Master Plan process electronically, please e-mail your request along with your e-mail address to rbessey@birchlarroyo.com or send your request to Brad Raymond, Development Services Director, 309 E. Chicago Blvd., Tecumseh, MI 49286.

Please feel free to contact us if you have any questions. We look forward to your input as we prepare this update to the City's Master Plan.

Sincerely,
BIRCHLER ARROYO ASSOCIATES, INC.

Rodney L. Arroyo, AICP
Vice-President

Rebecca D. Bessey, AICP
Associate Planner

cc: Frank Crosby, Tecumseh City Manager; Brad Raymond, Development Services Director

July 22, 2003

Tecumseh Township Planning Commission
c/o Pat Lamb
5764 Milwaukee Rd.
Tecumseh, MI 49286



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July 22, 2003

Raisin Township Planning Commission
5525 Occidental Highway
Tecumseh, MI 49286



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Vice-President

Rebecca D. Bessey, AICP
Associate Planner

cc: Frank Crosby, Tecumseh City Manager; Brad Raymond, Development Services Director

July 22, 2003

Region II Planning Commission
c/o Charles Reisdorf, Executive Director
Jackson County Tower Building
120 Michigan Ave.—16th Floor
Jackson, MI 49201



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Rodney L. Arroyo, AICP
Vice-President

Rebecca D. Bessey, AICP
Associate Planner

cc: Frank Crosby, Tecumseh City Manager; Brad Raymond, Development Services Director

CITY OF



July 20, 2004

LENAWEE COUNTY PLANNING COMMISSION
C/O TIM ANDERSON, SENIOR PLANNER
REGION II PLANNING COMMISSION
JACKSON COUNTY TOWER BUILDING
120 W. MICHIGAN AVE.—16TH FLOOR
JACKSON, MI 49201

Dear Mr. Anderson,

On behalf of the City of Tecumseh Planning Commission and as required by Section 125.37b of the Municipal Planning Act as amended, please find enclosed a copy of the draft City of Tecumseh Comprehensive Plan. Pursuant to the requirements of this legislation, we are requesting your review and comments on this proposed new comprehensive plan prior to its public hearing and adoption.

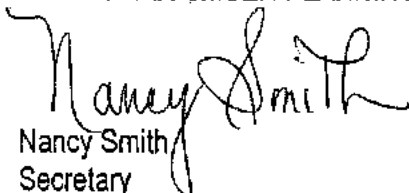
As required by Section 125.37b, copies of the draft plan have also been distributed to the following:

- Raisin Township Planning Commission
- Tecumseh Township Planning Commission

We have attached copies of the letters mailed to the above communities for your records. Please submit comments to **City of Tecumseh Planning Commission (c/o Brad Raymond, Development Services Director, 309 E. Chicago Blvd., Tecumseh, MI 49286)** no later than 95 days after you receive this notice. Thank you in advance for your cooperation and timely response in reviewing this draft plan. Comments offered during this review process will be taken into account in adjusting and refining the plan.

In the event that you have any questions on the enclosed plan, please feel free to contact **Brad Raymond, City of Tecumseh Development Services Director, at (517) 423-2107.**

Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION


Nancy Smith
Secretary

Home of Tecumseh Products Company

P.O. Box 396 • 309 East Chicago Boulevard • Tecumseh, Michigan 49286 • 517/423-2107 • FAX 517/423-3610

♻️ Recycled Paper



July 20, 2004

TECUMSEH TOWNSHIP PLANNING COMMISSION
C/O PAT LAMB
5764 MILWAUKEE RD.
TECUMSEH, MI 49286

Dear Planning Commissioners,

On behalf of the City of Tecumseh Planning Commission and as required by Section 125.37b of the Municipal Planning Act as amended, please find enclosed a copy of the draft City of Tecumseh Comprehensive Plan. Pursuant to the requirements of this legislation, we are requesting your review and comments on this proposed new comprehensive plan prior to its public hearing and adoption.

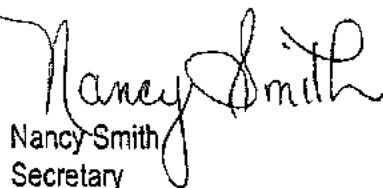
The amended Planning Act provides surrounding communities and other respondents with 65 days to provide comments. The Act further directs respondents to concurrently submit copies of their comments to the Lenawee County Planning Commission, which shares a statutory responsibility to coordinate the review of local plans.

To facilitate this review process, we have prepared a sample response form for your comments. You may use this form, if convenient, or submit your comments in letter form, if desired. Please submit comments to **City of Tecumseh Planning Commission (c/o Brad Raymond, Development Services Director, 309 E. Chicago Blvd., Tecumseh, MI 49286)** no later than 65 days after you receive this notice. In addition, please submit a copy of your comments to the **Lenawee County Planning Commission (c/o Tim Anderson, Region II Planning Commission, Jackson County Tower Building, 120 W. Michigan Ave.—16th Floor, Jackson, MI 49201)**.

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Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION



Nancy Smith
Secretary



July 20, 2004

RAISIN TOWNSHIP PLANNING COMMISSION
5525 OCCIDENTAL HIGHWAY
TECUMSEH, MI 49286

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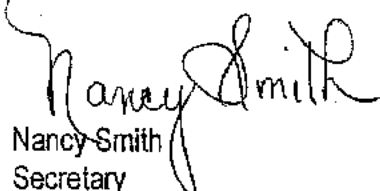
The amended Planning Act provides surrounding communities and other respondents with 65 days to provide comments. The Act further directs respondents to concurrently submit copies of their comments to the Lenawee County Planning Commission, which shares a statutory responsibility to coordinate the review of local plans.

To facilitate this review process, we have prepared a sample response form for your comments. You may use this form, if convenient, or submit your comments in letter form, if desired. Please submit comments to **City of Tecumseh Planning Commission (c/o Brad Raymond, Development Services Director, 309 E. Chicago Blvd., Tecumseh, MI 49286)** no later than 65 days after you receive this notice. In addition, please submit a copy of your comments to the **Lenawee County Planning Commission (c/o Tim Anderson, Region II Planning Commission, Jackson County Tower Building, 120 W. Michigan Ave.—16th Floor, Jackson, MI 49201).**

Thank you in advance for your cooperation and timely response in reviewing this draft plan. Comments offered during this review process will be taken into account in adjusting and refining the plan.

In the event that you have any questions on the enclosed plan, please feel free to contact Brad Raymond, City of Tecumseh Development Services Director, at (517) 423-2107.

Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION


Nancy Smith
Secretary



FILE COPY

October 28, 2004

LENAWEE COUNTY PLANNING COMMISSION
C/O TIM ANDERSON, SENIOR PLANNER
REGION II PLANNING COMMISSION
JACKSON COUNTY TOWER BUILDING
120 W. MICHIGAN AVE.—16TH FLOOR
JACKSON, MI 49201

Dear Mr. Anderson,

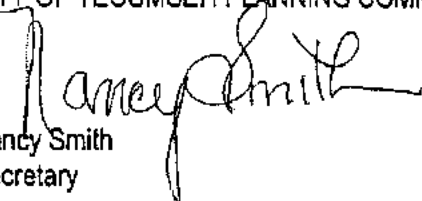
On behalf of the City of Tecumseh Planning Commission and as required by Section 125.38 of the Municipal Planning Act as amended, please be advised that the City of Tecumseh Planning Commission will hold a public hearing on December 7, 2004 at 7:00 p.m. in the City Council Chambers, City Hall, 309 E. Chicago Boulevard, Tecumseh, MI 49286. The purpose of the hearing is to receive public comments on a proposal by the Planning Commission to adopt an updated Comprehensive Plan for the City of Tecumseh.

The updated Comprehensive Plan includes text, charts, tables, graphs, illustrations and maps that describe the Planning Commission's proposal for the long-range future development of the community. The updated Comprehensive Plan has been prepared under the authority vested in the Planning Commission by the Municipal Planning Act, Michigan Public Act 285 of 1931, as amended. As a part of the Comprehensive Plan process a Community open House and Future Visioning meeting with City residents was held to obtain comments regarding the future development of the City.

A complete draft of the proposed Comprehensive Plan may be inspected prior to the hearing at the City of Tecumseh Department of Development Services, 309 E. Chicago Boulevard. The draft Plan can also be viewed online at www.birchleraroyo.com/Clients/tecumseh.htm. Written comments may be sent to the Planning Commission at 309 E. Chicago Boulevard, Tecumseh, MI 49286 prior to the hearing. Oral comments will be taken during the public hearing.

In the event that you have any questions on the proposed plan, please feel free to contact Brad Raymond, City of Tecumseh Development Services Director, at (517) 423-2107.

Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION



Nancy Smith
Secretary



FILE COPY

October 28, 2004

TECUMSEH TOWNSHIP PLANNING COMMISSION
C/O PAT LAMB
5764 MILWAUKEE RD.
TECUMSEH, MI 49286

Dear Planning Commissioners,

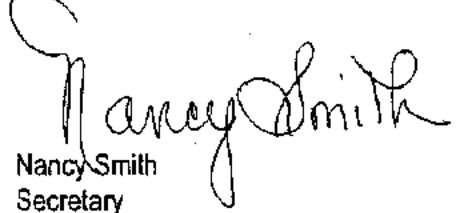
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Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION


Nancy Smith
Secretary



FILE COPY

October 28, 2004

RAISIN TOWNSHIP PLANNING COMMISSION
5525 OCCIDENTAL HIGHWAY
TECUMSEH, MI 49286

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Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION


Nancy Smith
Secretary



February 4, 2005

LENAWEE COUNTY PLANNING COMMISSION
C/O TIM ANDERSON, SENIOR PLANNER
REGION II PLANNING COMMISSION
JACKSON COUNTY TOWER BUILDING
120 W. MICHIGAN AVE.—16TH FLOOR
JACKSON, MI 49201

Dear Mr. Anderson,

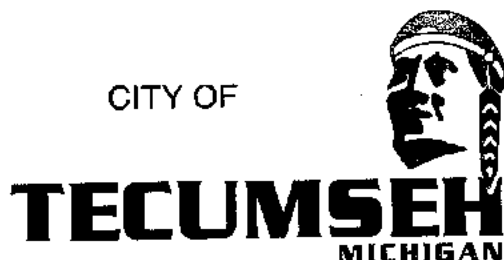
As you may recall, your review and comments on the proposed City of Tecumseh Comprehensive Plan were requested in July of 2004 as required by Section 125.37b of the Municipal Planning Act as amended. After the mandatory review period expired, the Planning Commission held a public hearing on the proposed plan. Subsequently, the Comprehensive Plan was approved by the City of Tecumseh Planning Commission on December 7, 2004, and the City Council on January 4, 2005.

Since no changes were made to the City of Tecumseh Comprehensive Plan document since it was distributed for review, please regard the draft copy that was previously transmitted to you as the final adopted version of the City of Tecumseh Comprehensive Plan.

In the event that you have any questions regarding the City of Tecumseh Comprehensive Plan, please feel free to contact **Brad Raymond, City of Tecumseh Development Services Director, at (517) 423-2107.**

Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION


Nancy Smith
Secretary



February 4, 2005

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C/O PAT LAMB
5764 MILWAUKEE RD.
TECUMSEH, MI 49286

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Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION

Nancy Smith
Secretary



February 4, 2005

RAISIN TOWNSHIP PLANNING COMMISSION
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TECUMSEH, MI 49286

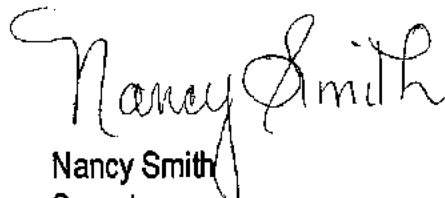
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Sincerely,
CITY OF TECUMSEH PLANNING COMMISSION


Nancy Smith
Secretary

Lenawee County Planning Commission



Members

James Tipton,
Chairman
Ralph Tillotson,
Vice-chairman

Karol Bolton
Keith Dersham
Howard Keller
Becky Liedel
William Saunders

Staff

Timothy Anderson

October 26, 2004

Ms. Nancy Smith, Secretary
City of Tecumseh Planning Commission
309 E. Chicago Blvd., P.O. Box 396
Tecumseh, MI 49286

Dear Ms. Smith:

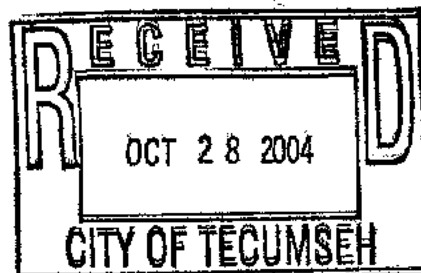
At their October 21, 2004 meeting, the Lenawee County Planning Commission reviewed the final draft of the City of Tecumseh Master Plan. The LCPC found no inconsistencies with the Lenawee County Comprehensive Land Use Plan.

The LCPC congratulated the City of Tecumseh Planning Commission on its planning effort. Special mention was made of the public participation process and focus on senior citizen housing. Further information can be found in the enclosed minutes.

If you have any questions, please feel free to give me a call at (517) 768-6703.

Sincerely,

Timothy Anderson, Secretary
Lenawee County Planning Commission



MINUTES
LENAWEE COUNTY PLANNING COMMISSION
Lenawee County Courthouse - Committee Room
Adrian, Michigan
Thursday, October 21, 2004

Members Present: Karol Bolton, Lenawee County Commission
Keith Dersham, City of Adrian
Bill Saunders, Dover Township
Jim Tipton, Blissfield Township

Members Absent: Howard Keller, LISD
Becky Liedel, Madison Charter Township
Ralph Tillotson, Lenawee County Commission

Others Present: Tim Anderson, Secretary
Two Raisin Township residents

ITEM 1 APPROVAL OF MINUTES

Comr. Saunders made the motion to approve the minutes of the September 16, 2004 meeting.
Comr. Bolton seconded the motion which passed unanimously.

ITEM 2 APPROVAL OF AGENDA

Mr. Anderson suggested the addition of a rezoning in Raisin Township and PA 116 agreement from Dover Township.

Comr. Saunders made the motion to approve the agenda as revised. Comr. Dersham seconded the motion which passed unanimously.

ITEM 3 CONSIDERATION OF REZONINGS IN RAISIN CHARTER TOWNSHIP

a. Russell Road

Mr. Anderson presented a staff recommendation to approve the rezoning of six parcels on the south side of Russell Road between Occidental and Green highways in the northwest 1/4 of Section 4 in Raisin Township from C-1, Local Service Commercial to C-2, General Service Commercial. The staff recommended approval of the request.

The members discussed the property that is not included in the rezoning. Mr. Anderson said that this property owner did not want to be included in the rezoning because he did not need C-2 zoning and was not interested in paying the application fee.

MINUTES
LCPC
October 21, 2004

Comr. Dersham stated he believes that the parcel should be added to the others in order to create a contiguous zoned area.

Comr. Saunders made the motion to recommend denial of the rezoning. Comr. Dersham seconded the motion which passed by the following roll call vote: Yeas - Saunders, Dersham and Tipton; Nays - Bolton; Absent - Tillotson, Liedel and Keller.

b. Pocklington Road

Mr. Anderson presented a staff recommendation to deny the rezoning of 1.52 acres on the south side of Pocklington Road between Billmyer and Ridge highways in the northwest 1/4 of Section 13 in Raisin Charter Township from A-1, Agriculture to C-1, Local Service Commercial. The purpose of the rezoning is to bring an illegal non-conforming use into compliance with the zoning ordinance. The Raisin Township Planning Commission recommended denial of the rezoning.

The members suggested that the rezoning is spot zoning. The rezoning is against the township and county land use plans, there is no commercial zoning in the area, the area to be rezoned is small, and there is no commercial zoning or uses in the area.

Comr. Saunders made the motion to recommend denial of the rezoning. Comr. Dersham seconded the motion which passed unanimously.

ITEM 4 REVIEW AND COMMENTS ON TECUMSEH MASTER PLAN

The members reviewed the draft master plan from the City of Tecumseh. The members agreed that the city planning commission and their planning consultants did an excellent job in developing the plan. Comr. Bolton expressed appreciation for the special attention that the city gave to senior citizen housing and the public participation process.

Mr. Anderson was asked to draft a letter of reply to the city.

ITEM 5 COMMENTS ON PROPOSED ONSTED SEWER SYSTEM IMPROVEMENTS

The members reviewed sewer system improvements to the Village of Onsted sewer system for consistency with the Lenawee County Comprehensive Land Use Plan. The review was requested by the Region 2 Community Action Agency in Hillsdale who is applying for funds on behalf of the village.

Mr. Anderson presented a staff review which found the improvements consistent with the goals and policies of the county plan. Commission members concurred.

MINUTES
LCPC
October 21, 2004

Comr. Dersham made a motion to find the sewer system improvements consistent with the Lenawee County Comprehensive Land Use Plan. Comr. Bolton seconded the motion which passed unanimously.

ITEM 6 **CONSIDERATION OF PA 116 FARMLAND AGREEMENTS**

The Commission reviewed the following P.A. 116 farmland agreement applications. Two were from Riga Township and one was from Dover Township.

a. Porter (Riga Township)

The members reviewed two applications submitted by Bruce and Pamela Porter, 8625 Horton Road in Blissfield. The applications called for the enrollment of 20 acres (\$12,000/20 acres = \$600/acre) for a 10-year term in Section 31 of Riga Township. A separate application called for the enrollment of an adjacent 74 acres on the west side of Berkey Highway.

Comr. Saunders made the motion to recommend approval of the agreements. Comr. Bolton seconded the motion which passed unanimously.

b. Valerio (Dover Township)

The members reviewed an application submitted by Joseph and Terry Valerio, 5995 Morey Highway in Clayton. The application called for the enrollment of 58 acres at the northeast corner of Haley Road and Morey Highway in Section 30 of Dover Township. The term of the agreement was not specified in the application.

Because the term of the agreement was not specified, Comr. Saunders made the motion to table review of the application. Comr. Bolton seconded the motion which passed unanimously.

ITEM 7 **DISCUSSION ON THE ACTIVITIES OF THE SOLID WASTE**
COORDINATING COMMITTEE

Comr. Dersham said that the movie has been shown but he did not get a chance to see it. The premiere will be at a later date.

ITEM 8 **OTHER BUSINESS**

Comr. Bolton updated the members on progress on the jail. Also, Comr. Bolton informed the members that there will be a public hearing on the farmland preservation ordinance on October 28.

There being no other business the meeting was adjourned at 8:05 p.m.

Tim Anderson, Secretary



**BIRCHLER ARROYO
ASSOCIATES, INC.**

PLANNING COMMUNITIES & TRANSPORTATION SYSTEMS

**28021 SOUTHFIELD ROAD
LATHRUP VILLAGE, MI 48078
(248) 423-1776
birchlararroyo@birchlararroyo.com**